

South Yorkshire Pensions Authority

262 High Holborn | Appendix H

Construction Management Plan

30th April 2013

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- H Construction Management Plan - Prepared by RPM**
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- L Daylight & Sunlight Assessment - Prepared by Gordon Ingram Associates

262 High Holborn, London



Construction Management Plan 15th April 2013

Revision A – First Issue

Document Control

Job Title : 262-267 High Holborn

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-	02/04/13	First Draft	NB	JR	JR
A	15/04/13	First Issue Logistics plan and text in Section 3.3 updated to reflect need for basement back propping on Holborn Place.	NB	JR	JR

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1.0 Introduction

This document is intended to outline the proposed methods of demolition and construction of the 262-267 High Holborn development.

With the proximity of the site to neighbouring businesses and the adjacent Chancery Court Hotel in mind, this document explores the environmental considerations associated with the proposed works and outlines appropriate measures that will be implemented to minimise their impact whilst ensuring that the project is completed in a timely manner.

2.0 Location and Access to the site

The site is located on the south side of High Holborn between the junctions with Kingsway and the Grays Inn Road; there is access to the west side of the site via Holborn place which is included in the ownership of the site. This will be the main vehicle access to the site for deliveries.

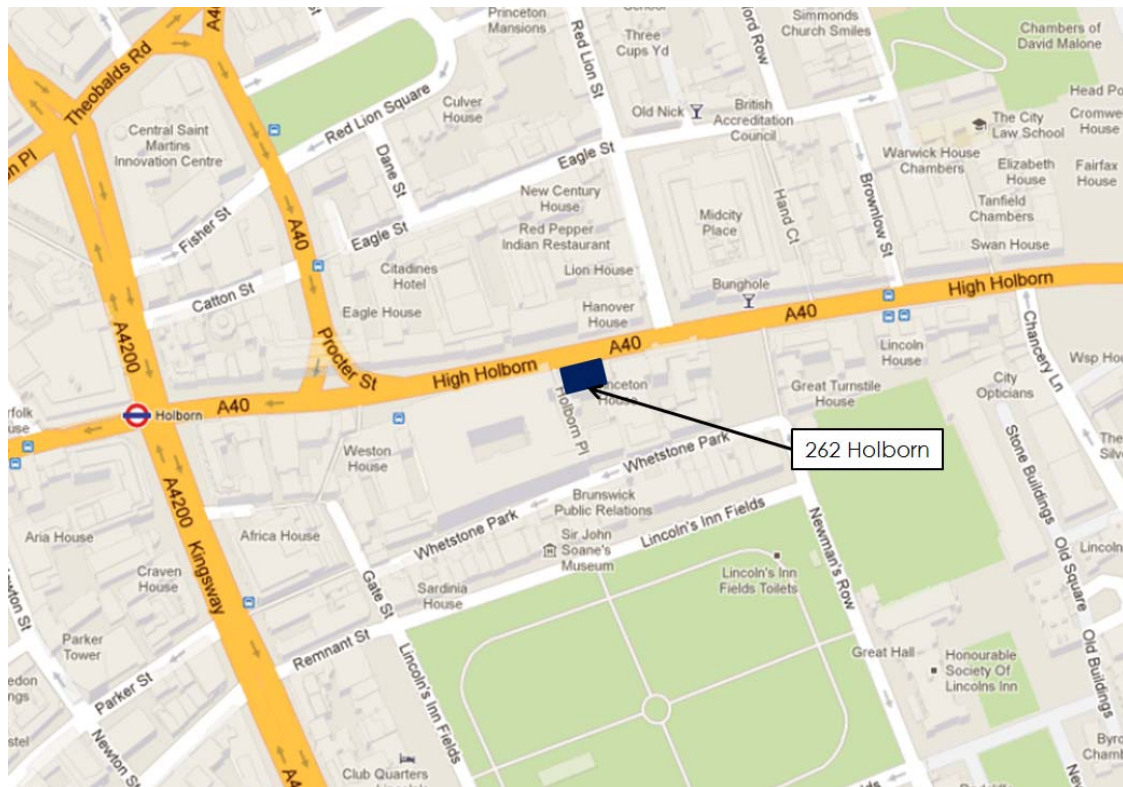


Figure 1: Plan indicating site location.

2.1 Site access

Access to the site for construction traffic entering London using the A40 from either the East or the West will be via Proctor Street and High Holborn with vehicles exiting the site having the option of heading North up Southampton Row or heading South via Kingsway.

It is envisaged, subject to agreement with London Borough of Camden (LBC); that the majority of large deliveries will be made from High Holborn directly to a scaffold gantry / lifting beam arrangement located at First Floor level with smaller, hand / pallet deliveries being taken in at Holborn Place and processed here. There is also an opportunity to use the rear access via Whetstone Park for smaller 'white van' deliveries to relieve pressure on the side access of Holborn Place at peak times.

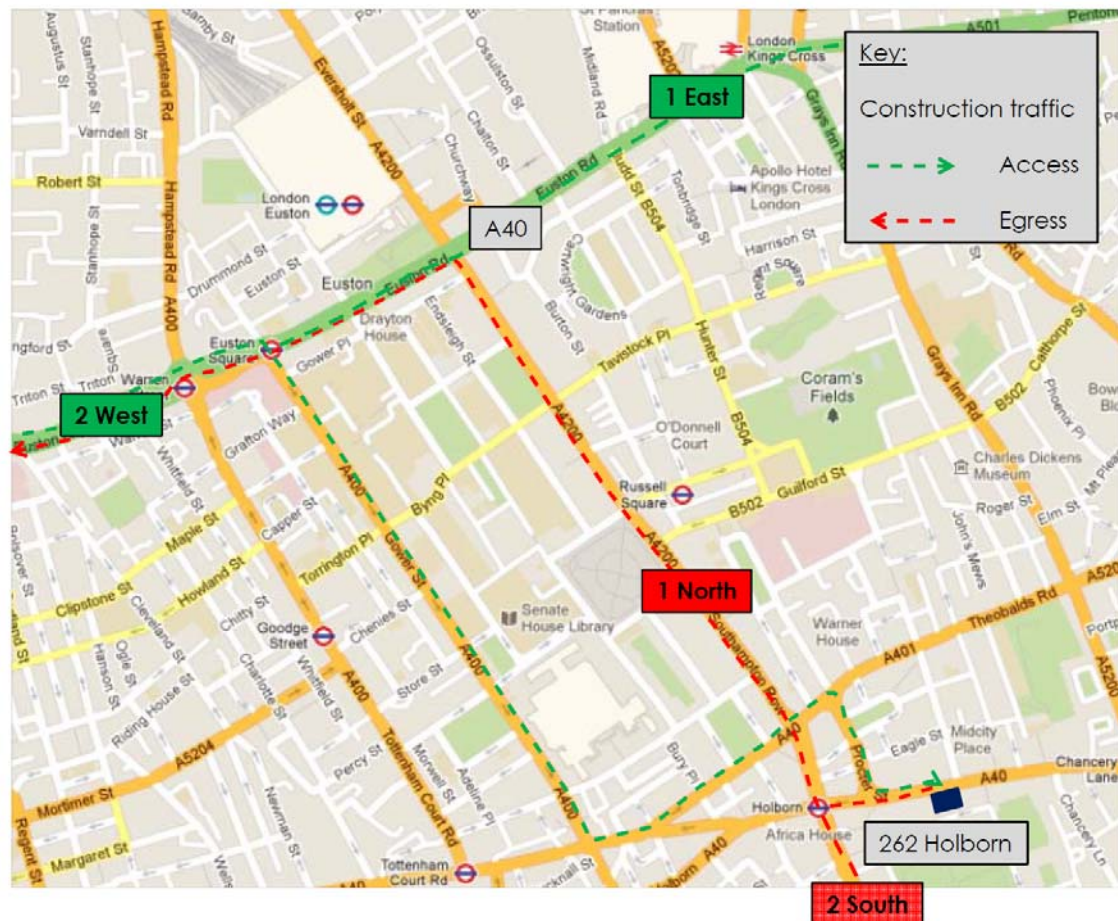


Figure 2: Plan indicating construction vehicle access / egress routes from the A40.



Figure 3: Junction with Proctor Street and High Holborn.

The Project office accommodation is likely to be located within the building footprint with staff, operatives & visitors using a separate secure access through the hoarding at street level directly from High Holborn.

This will allow the staff and operatives to access the site through a secure location without mixing with construction traffic. The access gates at the Holborn Place / High Holborn provide sufficient width to allow suitable vehicles to access only and this would be the case for the duration of the works.

The entrance will need to be managed and access strictly policed; at present this is under the temporary control of the contractor carrying out the refurbishment works to the adjacent Chancery Court Hotel; however upon commencement of the Project control of this area will revert to the control of the Development team's Contractor and it is proposed that deliveries required to the Hotel works would be required to use the delivery booking system to be applied on this Project. (Refer to Sections 3.3 & 3.4)

At specific times of the Project construction access may change to Whetstone Park to the rear of the building to relive pressure on the other delivery points.



Figure 3: Access from High Holborn into Holborn Place.



Figure 4: Exit from Holborn Place with traffic turning left to join direction of traffic.

3.0 Site Setup and Logistics

Two logistics plans have been included within this report for further review; the first indicating the Strip out/Demolition Phase and the second illustrating arrangement during the Main Construction Phase. (Refer to Section 3.6)

Whilst this has been prepared to ensure the Project can be completed as quickly as possible it is has been prepared to ensure that the general public, neighbours, construction traffic and pedestrians can co-exist safely with any disruption kept to an absolute minimum.

3.1 Logistics and Neighbourhood Liaison manager

Once appointed the successful Contractor will provide a Logistics and Neighbourhood Liaison Manager (LM) to manage and co-ordinate the material access / egress to the site through the operation of an electronic vehicle / material booking in system. It will also be their responsibility to manage and update the Contractors Construction Traffic Management Plan in conjunction with the Project Construction Health and Safety Plan. Both documents will be reviewed, updated and distributed as necessary on a monthly basis.

As part of the role, regular contact will be maintained with the local businesses and log the details and responses to any complaints received by the neighbours or any other affected third parties.

Minimum requirements regarding the cleanliness of the site boundary will be maintained. The areas adjoining the site and used for access to cross the public highway will be kept clean and free from obstructions. Where there is a need to run cables, hoses or any other potential hazard for the users of the footway, suitable ramping together with appropriate signage must be employed.

These external areas will be the responsibility of the banksmen and outside of their role of policing deliveries to and from the site they will be on hand to offer help and assistance to vulnerable users of the footway.

The booking in system will make allowance of peak use of the highway and the bus routes on High Holborn. Trained banksmen will be used for ALL deliveries and the calibre of contractors selected to tender will be well versed with providing this level of service as a minimum requirement.

All deliveries crossing the footway and entering the site will be managed into the site using trained banksmen who will maintain the flow of the footway, particularly during peak times. Suitable well maintained 'Chapter 8' barriers will be used to segregate the public from construction traffic movements wherever necessary.

It is noted that the on-going works to the Chancery Court Hotel will need close co-ordination between contractors on both Project to ensure the two can operate successfully, the LM will take a lead in this process with the deliveries for the CCH being run through the Projects delivery booking system; this is further detailed in Sections 3.3 & 3.4.

3.2 Condition surveys

As soon as the Contractor is appointed a series of photographic condition surveys will be carried out during the lead in period before the start on site date.

These will take place to the following areas;

- Holborn Place access route and external wall of the adjacent Chancery Court Hotel from High Holborn
- High Holborn footway prior to erection of external scaffolding.

These surveys will be issued to the relevant interested parties for agreement and review at the completion of the Project to assess any remedial works that may be required.

3.3 Site Security, Access / egress & traffic management

3.3.1 Site Security

With the security of the Project, the adjoining building connected at roof level and other adjacent properties in mind it is proposed the Project be protected by security guards during normal working hours who will maintain the High Holborn and Holborn Place access points; outside normal working hours the site will be manned with two guards patrolling (TBC).

To maintain the security at roof level roof in both directions there will need to be a temporary physical demarcation formed during the construction works; this will be part of the security patrol.

During normal working hours the Holborn Place vehicle access point will be manned by trained banksmen who will receive booked in deliveries to the site and manage the construction traffic interface with public footway using temporary 'Chapter 8' barriers deployed to prevent pedestrians crossing during vehicle movements. The main gates will be opened to allow vehicle to arrive and leave but will remain closed at all other times.

3.3.2 Access / Egress

All deliveries made to the site will be subject to an electronic delivery booking system managed by the contractor's Logistics and Neighbour Liaison Manager.

The system will apply to all vehicle movements to the site and ensure that material deliveries are rationalised to reduce vehicle movements to the site generally.

To ensure bottle necks and waiting vehicles are avoided a system will be implemented to ensure that each delivery calls into the site to confirm their available booking slot 15 minutes before their planned arrival time; if there is not an available slot for the next 30 mins they will be asked to return at a later available time slot.

Two main access / delivery routes have been identified for large to medium sized deliveries within Section 2.1 but there is a third option for smaller deliveries that can access the rear site at the junction with Holborn Place off Whetstone Place, via Newman's Row; these vehicles will have the option of exiting using Whetstone Place or if the route is clear, through Holborn Place onto High Holborn.

Any deliveries not booked into this system, or arriving outside the permitted working hours, will not be permitted onto site and will be turned away. Waiting in the local residential streets will not be permitted.

A separate pedestrian access for construction staff and will be set up to provide a secure access from High Holborn via an electronic photo card system to access / exit from the site.

There will be no parking for construction vehicles or operatives on site.

3.4 Delivery / vehicle management

3.4.1 General deliveries

All general 'day to day' vehicular deliveries will be made to High Holborn using the route detailed in Section 2.1, booked in using the computerised delivery booking system and where possible taken into the confines of the site. Once at the site the delivery will be taken under the control of a fully trained and certified banksman who will deal with the delivery to and from the site, including managing the pedestrian crossing point into Holborn Place.

The vehicle movements will be managed and monitored by the Logistics Manager. Waste removal and muck away activities will be limited to off peak hours to avoid clashes with critical deliveries.

Larger deliveries of materials that cannot negotiate the Holborn Place access will use the proposed external gantry lifting arrangement for vertical transportation to the appropriate floor using an external materials hoist.

3.4.2 Muck away and demolition arisings

Waste will be removed via the Holborn Place access into skips and compacting lorries depending on the phase of the Project. These will be sited in the Holborn Place courtyard, refer Section 3.6 Strip out/demolition phase logistics plan.

As the work progresses beyond the demolition and sub-structure phases the volume of deliveries will reduce to lighter vehicles, this will be assisted through the rationalising of deliveries to the site.

3.4.3 Concrete deliveries

Due to the high level of traffic movements no concrete deliveries are planned to take place off High Holborn. These will instead be made via Holborn Place, however due to the extent of the Chancery Court Hotel basement it is possible that an element of back propping to the Holborn Place route may be required to ensure that the section of slab that extends below the road is adequately supported for the duration of the works.

Concrete deliveries will be directed into Holborn place and pumped from this location. Vehicles involved in this operation will use the vehicle routes detailed in Section 2.1.

It is the intention to liaise with the local traffic police and LBC's Environmental / Highways Department to discuss the proposed vehicle delivery / loading manoeuvres. Any recommendations will be immediately implemented and incorporated into this CMP.

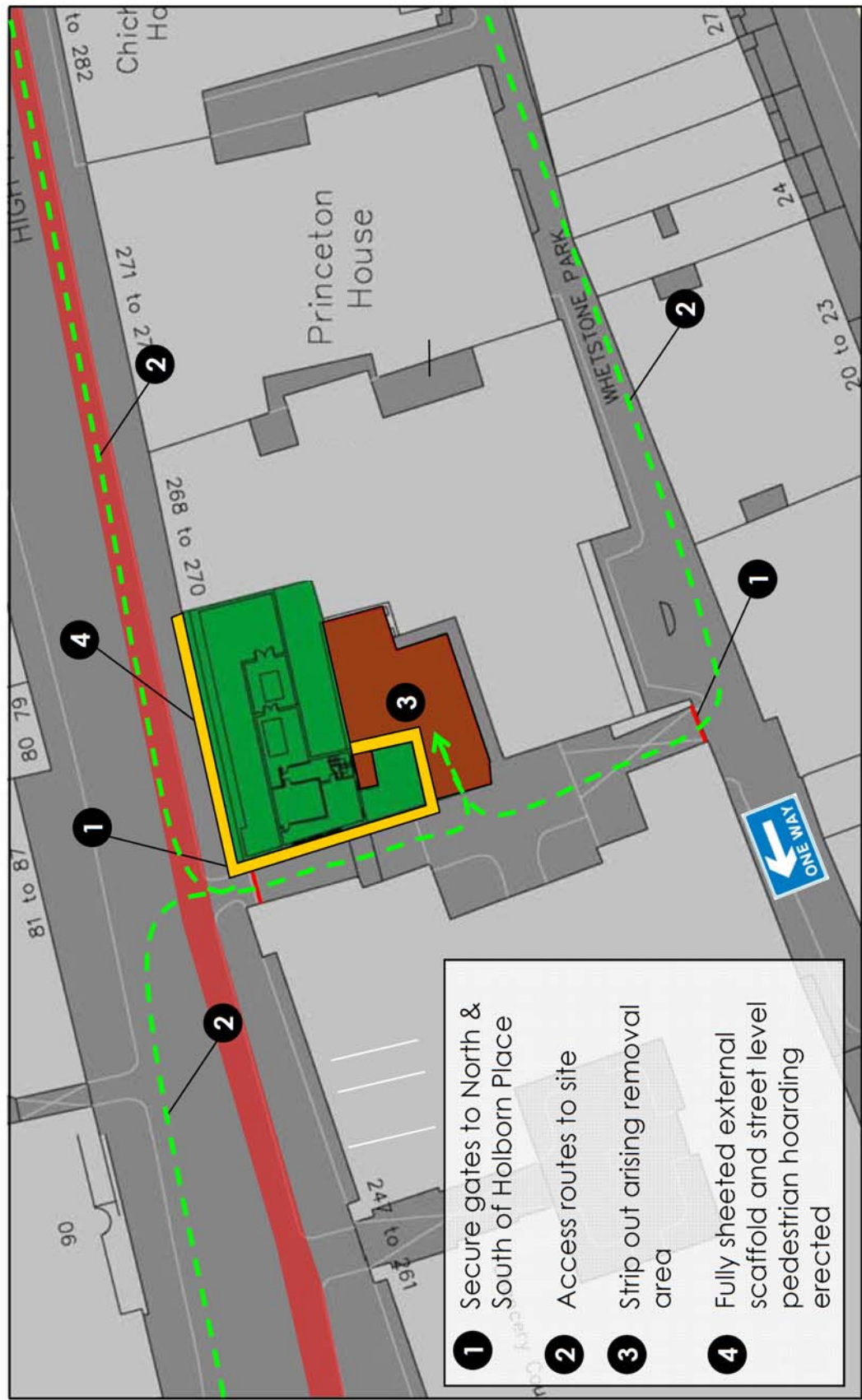
At this stage we do not envisage the need for suspensions / relocation of parking bays, the bus stop adjacent or pedestrian crossing to allow the construction related equipment, structures or activities taking place on the highway to facilitate the development but if this becomes necessary we will liaise with LBC's Transport and technical Department to ensure that appropriate authorisations and /or licences are obtained.

3.5 Incoming services

It is possible that new services may be required to facilitate the development but at this stage this cannot be established until the services design has been developed further.

3.6 Site Logistics Plans

- a) Strip out / Demolition Phase
- b) Construction Phase



Strip out / Demolition phase logistics plan



Construction phase logistics plan

4.0 Details of the works & proposed methods

4.1 Construction sequence

The works can be divided into the following simple elements;

- Site set up, security and welfare facilities.
- Enabling works.
- Strip out, temporary works and demolition.
- Crane erection for structural works.
- Erect structure to rear and upper floor extensions.
- External cladding to rear and upper floor extensions.
- Internal finishes, fixtures and fittings.
- External works.

4.1.1 Site set up and welfare facilities

Due to the external constraints of demolition and structural work of the site coupled with overall footprint the site, the contractors accommodation and welfare facilities will need to be established within the footprint of the building; probably located at either ground or basement level.

4.1.2 Enabling, strip out and demolition works

4.1.2.1 Enabling works

Prior to works starting on site the contractor accommodation and welfare facilities will be located in the basement and an external scaffold erected to the High Holborn and Holborn Place elevations; by agreement with LBC the High Holborn scaffold accommodating a material hoist and continuous unloading gantry at first floor level; this will be serviced by a jib crane initially and by the tower crane during its period of operation.

4.1.2.2 Strip out works

Prior to the physical strip out works commencing the asbestos containing materials within the building shall be removed by an ARCA approved contractor. Once the appropriate clearance notices have been issued the strip out works will take place. Depending on the nature and extent of the ACM's there may be an opportunity isolate these areas whilst allowing the strip out works to commence safely; this will be established once the asbestos register has been reviewed.

The strip out package will comprise works to remove the existing finishes to expose the structure; this package will ideally include the removal of all non-loadbearing

walls and risers where possible with appropriate edge protection installed to suit.

Soft strip arisings will be removed on a floor by floor basis using one of the redundant lift shafts as a chute to ground level with material cleared through a temporary opening in the external wall at ground floor level into bulk skips located in the Holborn Place courtyard.

4.1.2.3 Demolition

The demolition to the rear courtyard and levels 7-8 will be carried out using a combination of hand and mechanical techniques with constant 'damping down' taking place to ensure that noise and dust is kept to a minimum; together with the use of screens erected to the perimeter of the works to shield the neighbours from noise breakout.

Where appropriate diamond saw cutting will take place to slab edges and columns to further reduce noise and dust whilst speeding up the process and leaving the retained structure in a controlled state for future connection works to take place.

4.1.3 Construction of extension and roof

Tower Crane

To facilitate the construction of the new structure and in particular the reinforced concrete core; a luffing jib tower crane will be required. By agreement with LBC Highways Dept. this would be erected and dismantled from High Holborn with the mast being located within the floor plate area (Grids 3-4 / D-E), carefully located to ensure it avoids the chiller and generator spaces within the 7th floor plant area.

The tower crane will remain in place for the construction of the rear courtyard extension steel frame and the new Level 7-8 steel / timber framed construction. The new floor construction to levels 1-7 will take place around the crane mast together with additional openings below ground to allow the mast base temporary foundations required. On removal of the crane these openings will be in-filled, roof down as soon as possible.

Concrete core

The main lift / service core is being formed from reinforced concrete, this will be constructed using conventional floor to floor shutters ahead of the surrounding steel frame. The concrete will be pumped from Holborn Place using either lorry mounted or trailer pump (depending on which stage the perimeter works are at).

Steel frame extension

The steel framed courtyard extension will be constructed using the tower crane which will be fed with material deliveries from the High Holborn and Holborn Place. As the frame is erected the composite metal deck floors will be constructed; any shear stud fixing will be carried out within the prescribed 'noisy periods' only.

Once the frame and floor decks have been completed the external cladding will be installed using the tower crane to fix the units.

Levels 7-8 Construction

To allow the concrete core to be constructed utilising the majority of the tower crane allocation it is proposed that a mini crane will be used to construct the steel / timber frame at levels 7 to 8 which will work independently but be serviced with deliveries using the tower crane. The mini crane will be lifted into place once the tower crane has been completed (at this stage all demolition should be completed and steel erection can commence during the construction of the concrete core in the courtyard).

The tower crane will then remove the mini crane from the roof.

The external cladding at these levels will be installed using perimeter lifting beam once the main glass and framing deliveries have been made to the roof by the tower crane.

4.1.4 Internal services, core works and externals

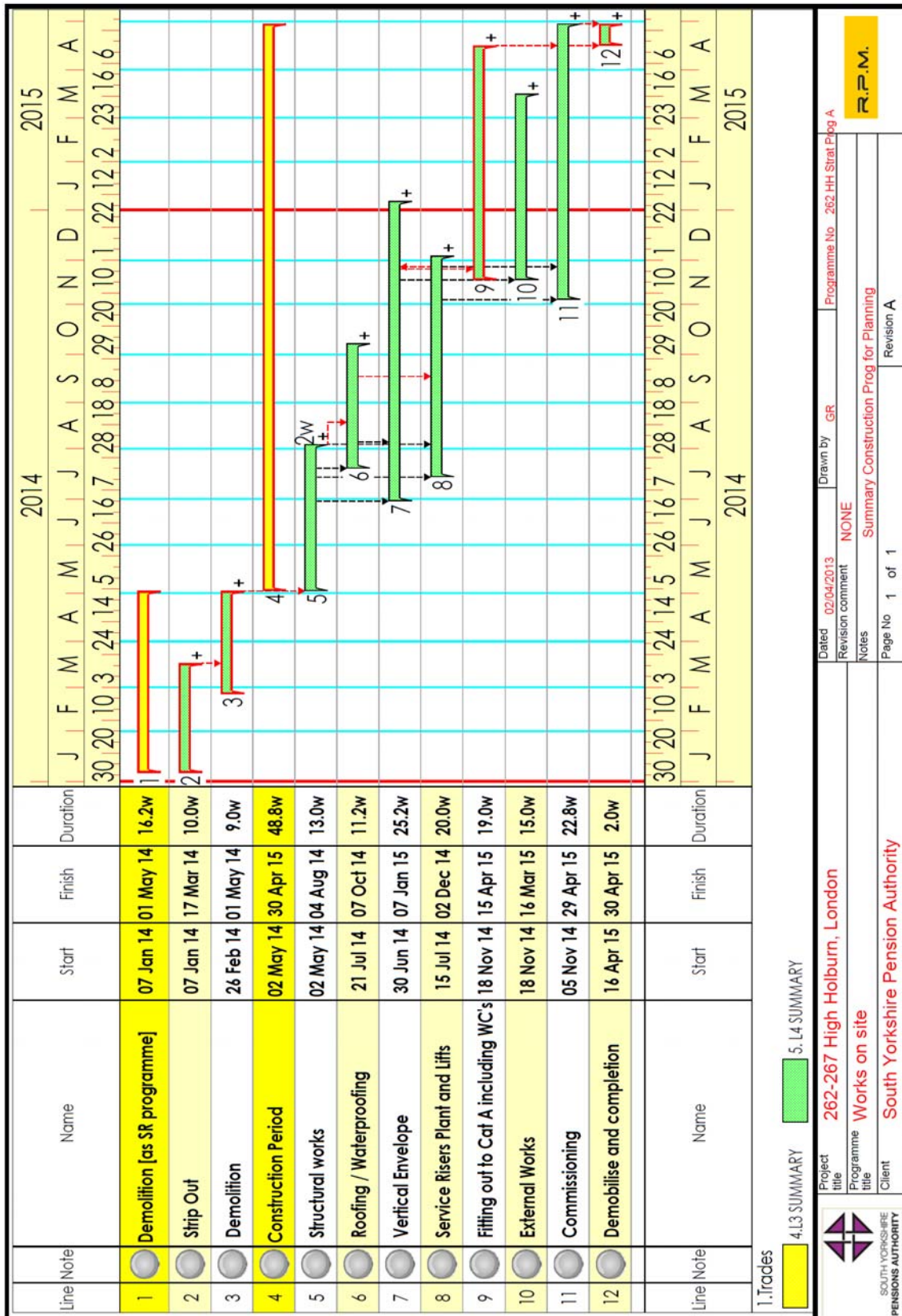
On completion of the external walls to the courtyard extension the fit out will progress in this area and like up with the other works that have been able to advance on the remainder of the building.

The internal fit out works will be serviced from both High Holborn and Holborn place with the latter being used to completion when the external material hoist has been removed.

The hoist will be removed once the majority of the internal fit out works have been completed to enable deliveries to the floor plates to be made; beyond this point vertical material / waste distribution will be using the goods lift.

The external street level hoarding shall remain in place so that the works to the ground floor units can be completed, at which point the hoarding will be removed and remedial works to the carriageway way carried out behind secure short term temporary Heras type fencing with Chapter 8 barriers to compliment as required.

5.0 Outline Programme



6.0 Environmental considerations during Construction

6.1 Site Working Hours

The proposed hours of work will be agreed with the Local Authority. LBC guidelines for noisy works dictate that the following hours should be adhered to;

- 08.00 - 18:00 hours (Monday to Friday)
- 08.00 - 13.00 hours (Saturday)
- No Sunday working unless by prior arrangement with LBC.

Noisy operations are not proposed to take place outside these times.

As noted in Section 4.1.2, and as part of the pre-construction process it is proposed that a voluntary Section 61 would be entered into through agreement with LBC to manage the extent of noisy work being carried out; with 'noisy' working being broken up into '2 hours on / 2 hours off' approach; starting 8am-10am and so on.

6.2 Considerate Constructors Scheme

The Project will be undertaken in accordance with the Considerate Constructor Scheme with targets set for minimum performance.

Prior to commencing on site, the Contractor will be required to register the Project under the Considerate Constructor Scheme with a minimum target score of 4 in each section of the Schemes site code for Considerate Practice.

As part of the CSS scheme regular inspections will be carried out and subsequent reports will be distributed as part of the contractor's monthly report.

6.3 Noise, vibration and dust

All works will employ the 'Best Practicable Means' as referred to in BS 5228-1:2009, to minimise the effects of noise and vibration that can be practicably applied at reasonable cost. For example, using muncher attachments to excavators, in lieu of pneumatic breaker and utilising separation cut lines to minimise vibration transfer. Where appropriate, the following measures to minimise noise and vibration levels will be adopted:

- Using modern, quiet and well-maintained equipment;
- Using low impact techniques, such as munchers and bored or hydraulically-jacked piling rigs;
- Using electrically powered equipment (mains or super silenced generators);
- Use of screws and drills rather than nails;
- Careful material handling such as lowering rather than dropping items;
- Isolating the deconstruction works from sensitive neighbours, in order to minimise the transfer of vibration and structure borne noise;
- Avoidance of unnecessary noise such as engines idling between operations, shouting, loud radios or excessive revving of engines by effective site management;

- The use of radios on site. Shouting, swearing, singing; sitting outside the site is not to be permitted at any time.

The distance between noise and vibration sources and sensitive neighbours will be maximised and the sound path obstructed, where practical, by considerate siting of stationary plant and loading/unloading areas, erecting hoarding scaffolding during demolition, leaving building facades and boundary walls intact as long as possible and boarding/bricking up windows; use of existing non-sensitive structures as shields.

The suitability of specific noise limits is highly dependent upon the individual situation. The factors to be considered include the characteristics of the potentially affected neighbours, baseline ambient noise levels and the nature and duration of the works.

6.4 Waste Minimisation and Management

Procedures shall be developed to ensure compliance with the Site Waste Management Plans Regulations (2008), good practice requirements for waste management are applied across the Project to all contractors.

The Contractor will be responsible for managing their own waste and ensuring that they and all sub-contractors measure monitor and record the disposal of their waste in accordance with the construction phase SWMP.

Initiatives to reduce waste streams will be incorporated where possible and recycling and segregation of waste material on site will be encouraged with a view to achieving the additional BREEAM points available.

6.5 Neighbour and Community Liaison

As part of the process the team will meet with the LBC Environmental Health and Highways representatives to fine tune the methods of working and measures to minimise disruption. As part of this liaison, regular meetings will be held to ensure they are kept informed of the progress and particular any complaints received as a result of the works.

To mitigate complaints the Contractor will identify any residential properties and other sensitive occupiers in close proximity to the site, prior to commencing work.

The Contractor will appoint a responsible Logistics and Neighbourhood Liaison Manager to liaise with the council, local residents, businesses and other interested parties to keep them informed of progress on site and forthcoming activities which may affect them.

Prior to starting work, all occupiers in the vicinity of the site will be informed of the start date, the duration and nature of the project, the principal stages of the project and contact names and numbers of appropriate personnel via a mailed newsletter.

Further newsletters notifying neighbours of progress and forthcoming activities, particularly those which may cause disturbance, access difficulties and the like, are to be hand delivered to all adjoining occupiers and other neighbouring occupiers who may be affected by the works, on a monthly basis throughout the duration of the works.

A 'display board' will be erected outside the site, which will identify key personnel, contact addresses and telephone numbers as well as a full copy of the planning consents relating to the works.

The board will also carry the monthly newsletters advising the general public of progress and future activities on a month by month basis.

Appendices

Appendix 1 – Construction plant & Equipment

Project specific construction plant to be inserted where applicable