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Draft

# CMP Trinity

Construction Management Plan

3 Trinity Close, Willoughby Rd

London . NW3 1SD

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By Kamala Foster

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In association with  
**Building Doctors Ltd**

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Reference TC/BD/ 001

13th March 2013 / REV 00

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## 1.0 Introduction

The CMP provides a system and procedures to address and manage potential impacts on the local amenities, environment and Construction and Health and Safety.

To ensure that the all parties involve are aware that that Camden council is very concern regarding any disruption and it implications to the surrounding area.

Therefore the CPM is prepared not only to cover the construction management on site but into account the impacts on the local amenities and environment.

This could be achieved in a nut shell by proper planning from the very beginning by:

- Working to gather with the local community
- Ensuring that the contractor who is being appointed to work on this project is an accredited contractor( by checking the tract record of the contractor ),
- Ensuring contractors are conscious and sensitive towards the local community
- Contractor who will be appointed is either registered with Considerate Contractor Scheme or will have to registered with CCS ( tender requirement).

This CMP provides a system and procedures to address and manage potential impacts associated with the part demolition, reconstruction ,refurbishment and construction of new basement encapsulating not only the construction phases but also health and safety , local amenities and environment impact of the surrounding.

In future this CMP must be read in conjunction with the design access statement, Basement Impact Assessment report by the engineer, planning conditions and, Site Waste Management Plan (SWMP), Preconstruction (By CDM) in future with the, construction details and drawings by consultant, and Construction Health and Safety Plan by (contractor )and and the company's Environmental Policy of the contractor.

CMP outlines the key steps to be taken by all site personnel and their sub contractors to manage the environmental hazards and risks associated with the project and to effectively minimise the potential for environmental harm. All and sub-contractor personnel engaged on the Project will be required to comply fully with the requirements of this CEMP in order to limit the potential for environmental harm and regulatory non-compliance.

## **A** Brief Description of The Site, Surrounding Area and Development Proposals for which The CMP Applies.

This CMP is submitted on behalf of (Sheriar Bradbury) to Camden Council to the Planning Department of Planning for their perusal and approval for No 3 Trinity Close, Willoughby Street London, NW31SD.

The proposed site falls under the Conservation Area. The proposal consists of renovation of one of the existing house which was formerly a part of conversion of 3 houses of former church of Trinity Close.

It was squared off all courtyard facing windows, and added incongruously shaped dormers facing Old Brewery Mews. A Few original church features survived such as The Old Brewery buttresses and the Willoughby Street gable with retained Gothic pointed stone tracery windows. No original church signage remains.

The current proposal is to enhance the frontage of the existing property and reconfiguring the internal living space. To increase the living space, a basement has been introduced with light well in view of natural day light. The front smooth curve renovation with new stone mullion on brick base replaces the current frontage which is a boxy. The changes have created unison with the whole building. The existing 1980's dormer is retained. Conservation Lumen LR6/7 roof lights has been proposed by the architect on the Old Brewery mews side and the Trinity Close Courtyard.

All new conservation skylights are flush with the roof tiles and located in accordance with Camden's design guide and mostly concealed behind the ex. raised courtyard parapet. No development is planned above the ridge line.



# A.2 Project Description

The proposal consist of minor demolition, refurbishment of the existing building with a construction of new basement and light well. Detailed scope involved in this project.

1. Two weeks be will given to the contractor to prepare the Construction Management Plan include
  - Project Organisation chart
  - Site Organisation Chart / Detail Project Programme
  - Construction Health and Safety Plan
  - Plan for application of permits, suspension, road closure & Utilities
  - Write to neighbours to notifying the activity,
  - Method statement for immediate works
  - Set up a complaints box / Temporary supplies / Welfare facilities
2. Hording and securing the site.
3. Part Demolition of existing structure and the job progress for each stage.
4. Underpinning to the neighbour's property
5. Upgrading utilities if required but most unlikely.
6. Sheet piling or kingpost construction based on contractors MS
7. Excavating for the basement
8. Temporary supports as the work progress
9. Drainage
10. Construction basement slab and wall
11. Casting of ground floor slab
12. Place the permanent structure and temporary support prior to striping the next floor in view of stability of the existing structures and safety of the workers during construction.
13. Then striping out the first floor joist, repeat the above process followed by repetition of the work above all floors till all structural element are placed. (Please take note the striping out, the procedure could work from top to bottom- based on the construction method of the contractor.)
14. Replace roof truss with rafter as per the drawings
15. External scaffolding to erected based on the project requirements
16. Roof covering including skylight to be installed
17. Windows to be installed
18. All Mechanical & Electrical to be carried based on the first and second fix requirements
19. Internal wall & Plasterboard / Plastering /skim / Render
20. Kitchen Installation
21. Tiling /wooden flooring/carpet
22. Painting and decoration
23. Making good of the surrounding Including reinstatement of pavement which are affected by the construction
24. De-mobilisation

## **B** Start and End Dates For Each Phase of Construction.

The Projected time line for the above work involve are as per the following;

- Planning Approval and Construction Detail Design  
15/03/2013 -14/09/2013 (6 months)
- Tender and Appointing Contractor -  
16/9/2013 - 11/10/2013 (4wks)
- Project Planning Period contractors upon being appointed  
14/10/2013 - 25/10/2013 (2wk)
- Mobilisation and Site Set up-  
28/10/2013- 1/11/2013 (1-wks)
- Part demolition & Construction of Basement  
4/11/2012-16/05/2014 (28wks – incl. Christmas holiday)
- Strip out Reconstruction & Refurbishment Works  
19/05/2014-14/112012-(26 wks)
- Testing and demobilisation  
17/11/2014-28/11/2014 (2 wks)
- Defect Liability period  
29/11/2014- 28/11/2015 (52 wks)

## **C** Proposed Hours In Which Vehicles Will Arrive And Depart and Site Preparation

A standard letter will be sent to all sub contractors and suppliers requesting all deliveries must be made between the hours of 09.30am till 14.30pm latest thus avoiding congestion of school runs and rush hour but due to the working hours is in between 8 am – 6 pm at times delivery could be within this hours .

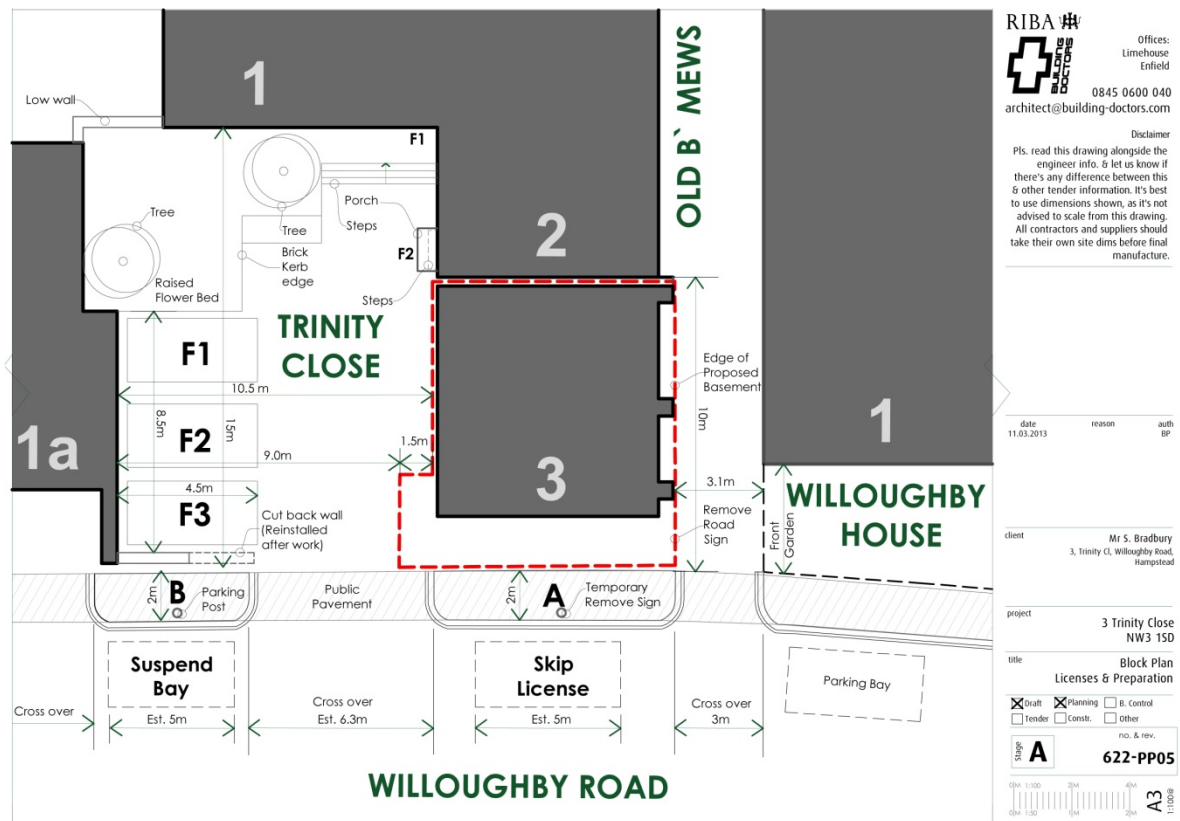
However the contractor will try to avoid all rush hours. Deliveries to the site must be pre booked via the contractor's site manager / foreman on site so that these can be scheduled in advance.

Any major deliveries or disruption will be NOTIFIED to the local communities and also contractor to ensure it is delivered at a NON PICK TIME.

Any parking bay suspension , Post removal, road closure, and sign relocation will be applied well in advance to Camden council and can only be done by their approval.

The contractor to work closely with the highways department as well to ensure all traffic rules are adhere too.

Apart from that any major activities will be notified to the community by the monthly leaf lets.





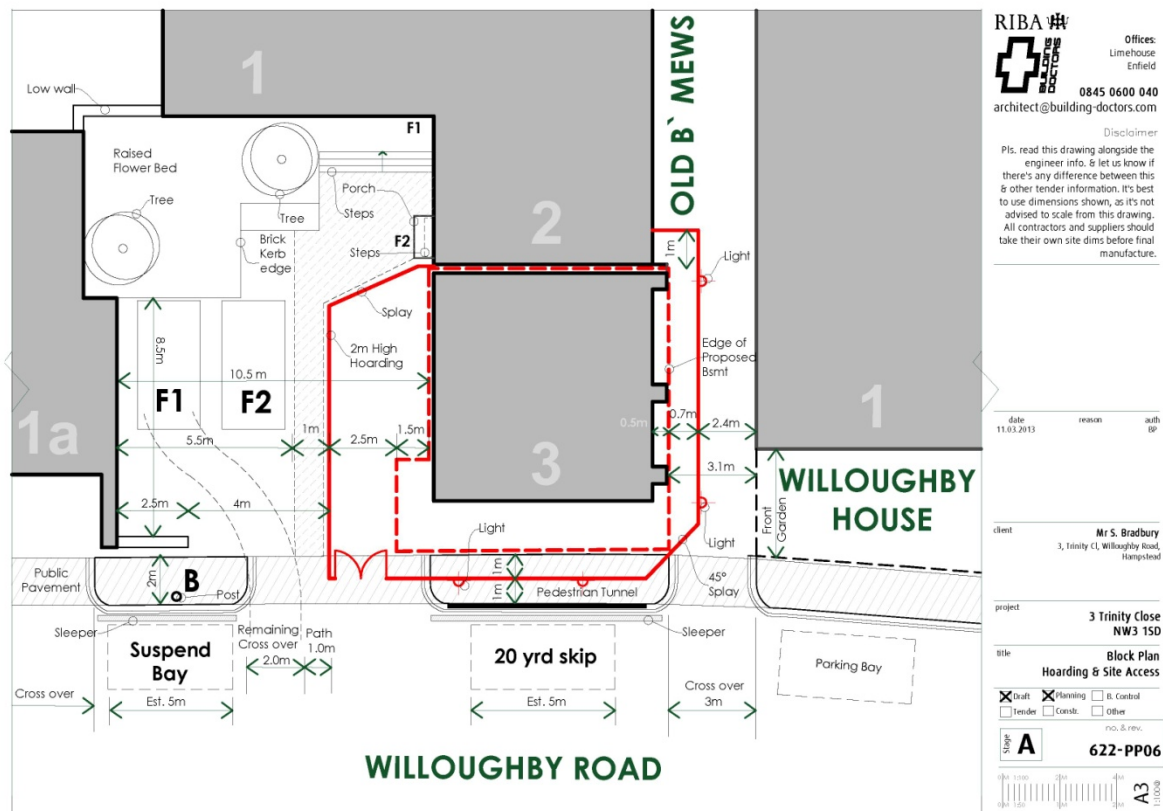
# D Access Arrangements For Vehicles

The access to the site is via Willoughby Road. The entrance to the site is through the site gate integrated in to the hoarding as indicated on drawing number 622- P006. Currently the access to site will used a part of the existing entrance.

All Deliveries/visitors to site must schedule prior to arrival to site.

The banks men will ensure that traffic flow on roads is maintained at all times.

Traffic management will be in place always be in place to direct the traffic and pedestrian.



All time the road in front of the site will be kept clean by sweeping or jet washing based on the road condition.

# E Proposed Routes For Vehicles Between The Site And TLRN

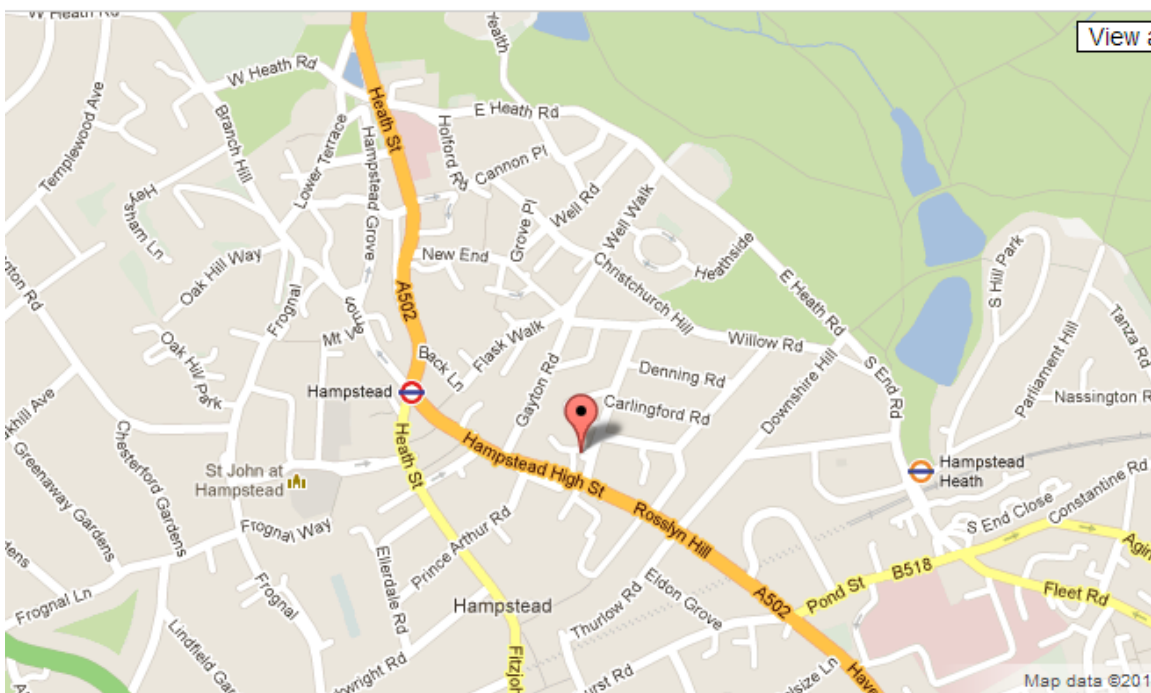
Eventhough the closes access to 3 Trinity Road is Trough Willoughby road via Hamstead High Street, the material will mainly be brought E Heath Road , S End road or Downshire Hill, then follow Willow Road and finally into Willoughby road instead of A502,Hamstead High Street as we are aware that Hampstead High street is a very busy . All Deliveries/visitors to site must also be announced prior to arrival to site.

During construction if we find the road is not that busy as it has been inteded to deliver during of peak we will deliver through hampstead High Street which will be inline of minimising the disruption to the local community.

A strict delivery procedure will be implemented to ensure that the Willoughby Road are not overrun with site and delivery vehicles.  
Loading and off loading must be avoided during school drop off and pick times.

Maps will also be issued to the sub contractors to indicate direction of approach to the site and leaving route.

All vehicles on site should not to be left idling unnecessary creating unwanted and unnecessary pollution to the area.



## **F** Size Of Vehicles, Frequency, Time They Require Access To The Site For Each Phase Of Construction

Size of vehicle will be based on the activity on site as per the following:

During the construction of basement the following vehicle will be used :-

- Skip lorries. These will include roll on/roll off skips for strip out, part demolition works and excavation for basement 8.5m in length and 2.480m in width
- Standard 8 yard skips for general waste approx size 7m in length and 2.480m in width.
- Ready mix concrete lorries Approx. size 8.25m length and 2.480 in width.
- Flat bed delivery vehicles for delivery of various building material will be use Approx. size 8.5m in length and 2.48m in width
- Articulated vehicle may be used but only and only if necessary
- Otherwise a rigid base 8 wheeler or small lorries where ever possible will be used to avoid any congestion and any disruption to the locals.

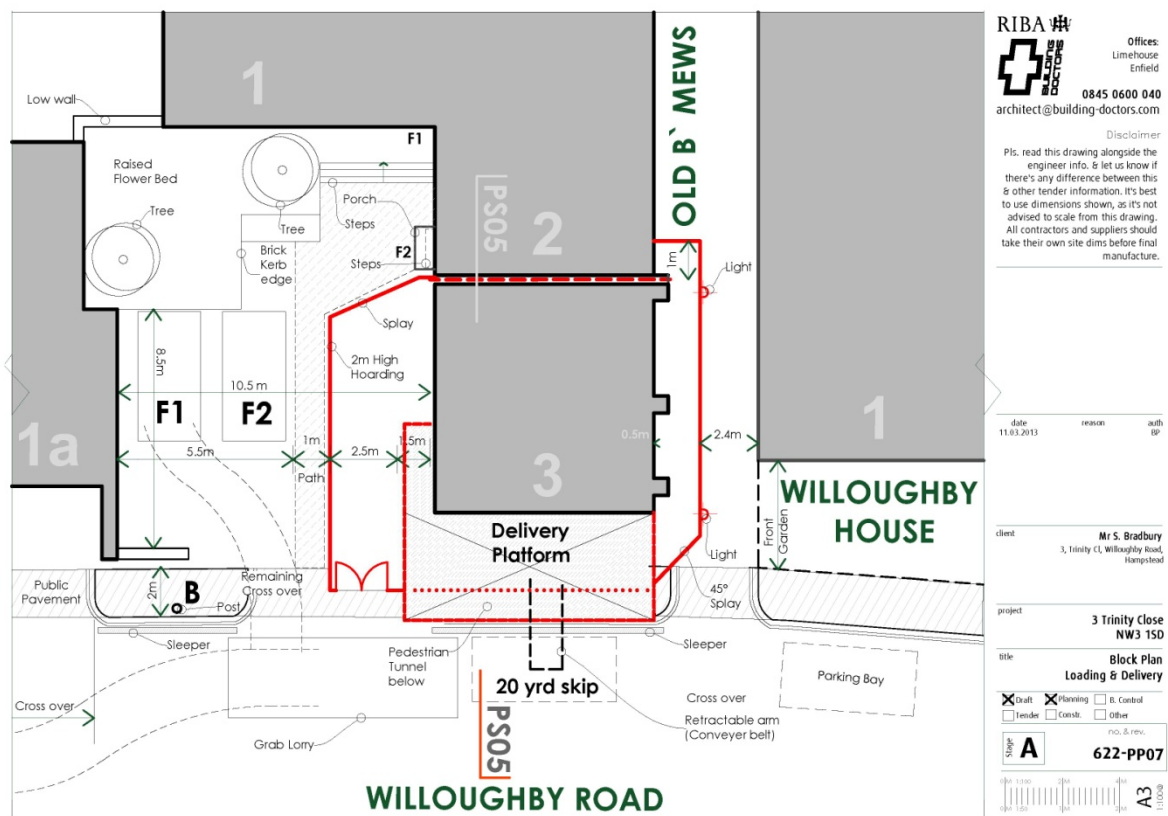
The frequency will be based on the activity it could vary from 2 lorries to 10 lorries per day

- During strip out and part demolition: 3-5 lorries per day
- During excavation and muck away : 5-8 lorries per day
- During concreting : 1-8 lorries per day
- During general building it may vary between 2 – 5 lorries per day
- They will be days where deliveries will not be required

The approx. frequency of the lorries to site will be kept informed to the local community and the council via monthly leaflets.

## G Swept path drawings for any tight maneuvers on vehicle routes to the site.

The drawing below indicates the vehicle movements.



## H Details (Including Accurate Scaled Drawings) Of Any Highway Works Necessary To Enable Construction To Take Place.

The entrance to the site is an existing drop kerb as indicated in the drawing. Therefore this project does not require any cross over's.

In the event any part of the entrance or kerb been damage then the contractor will reinstate to its original condition. Sleepers have been placed at kerb edges to prevent damage beside trucks.

## **Parking and Loading Arrangement of Vehicles and Delivery of Materials and Plant to the Site.**

Most loading and unloading will be done within the private space that is available within the horded area or by negotiating with the neighbours to use the parking space when they are not in use.

The contractor to work together with the local community and minimise disruption .

The parking bay right & in front of this unit will be suspended in the event if necessary for example during basement excavation. Refer the drawing below. Safe Provision i.e. walkway under the scaffolding will be made for the pedestrian to walk..

Traffic management will be in place always be in place to direct the traffic and pedestrian.

Loading and off loading must be avoided during school drop off and pick times.

Maps will also be issued to the sub contractors to indicate direction of approach to the site and leaving route. Vehicles will pull into the site for unloading wherever possible.

## **Details of Proposed Parking Bays Suspensions and Temporary Traffic Management Order**

A discussion will be held with the highway department with regards to the suspension of the parking bay and the traffic management order for this project.

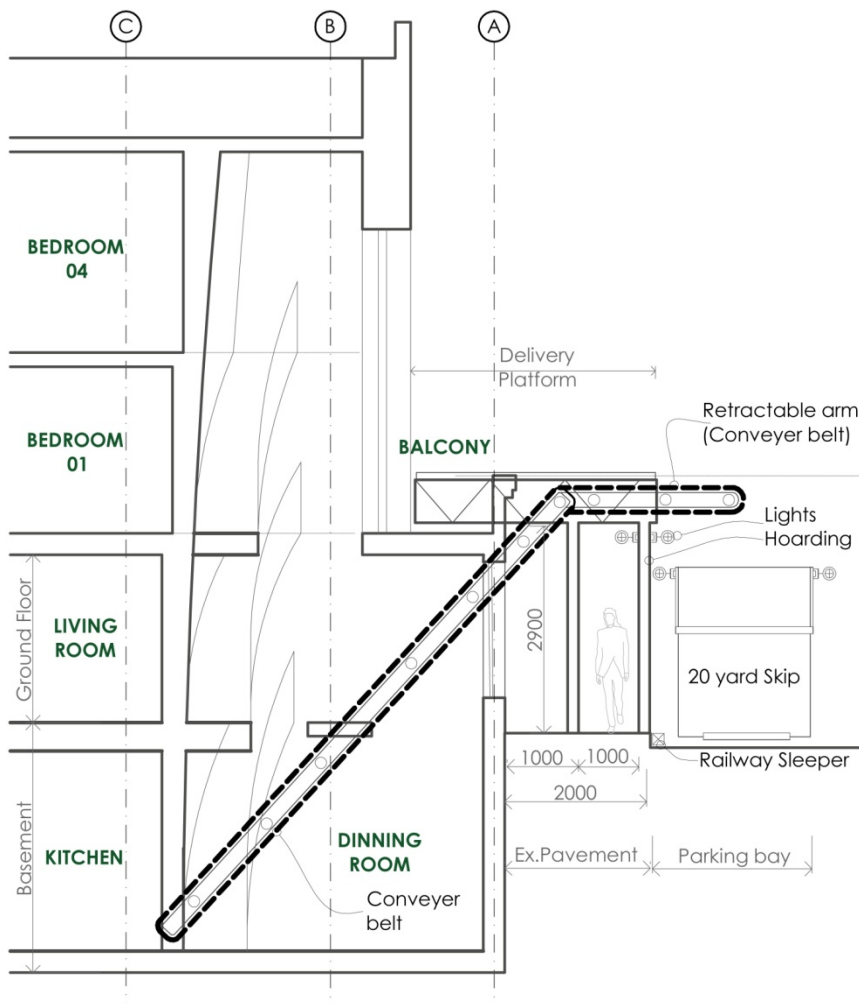
Please see below, the drawing has indicated the bays that required to be suspended. The bays will only be suspended as and when required.

The traffic marshal and an assistance will be in control of directing traffic with the help of signs and cones if required

# K Proposed overhang of the Public Highway

A discussion and permission will be requested from the highway section of Camden council. It is intended the quickest and the safest way to remove the muck away from the site for this particular project is by using conveyor with minimum disruption to the local. The period of construction is reduced.

A scaffolding gantry will be placed over the current foot path in front of the project. The width of the pavement is 2 m. The scaffolding will be enclosed with a hoarding and we will ensure that a minimum of 1.0m of clear footpath remains open to the public. The platform above is used to support the conveyor to transport the excavated material for the basement in to the skip which is placed on the suspended parking bay.



Offices:  
Limehouse  
Enfield

0845 0600 040  
architect@building-doctors.com

## Disclaimer

Pls. read this drawing alongside the engineer info. & let us know if there's any difference between this & other tender information. It's best to use dimensions shown, as it's not advised to scale from this drawing. All contractors and suppliers should take their own site dims before final manufacture.

date	reason	auth
11.03.2013		NC

client Mr S. Bradbury  
3, Trinity Cl, Willoughby Road,  
Hampstead

project 3 Trinity Close  
NW3 1SD

title Proposed Loading Deck &  
Basement Extraction

<input checked="" type="checkbox"/> Draft	<input checked="" type="checkbox"/> Planning	<input type="checkbox"/> B. Control
<input type="checkbox"/> Tender	<input type="checkbox"/> Constr.	<input type="checkbox"/> Other

no. & rev.  
622- P505

Scale A3  
0/1M 1:100 2/1M 1:50 3/1M 1:20 4/1M 1:10 5/1M 1:50 6/1M 1:20 7/1M 1:10 8/1M 1:50 9/1M 1:20 10/1M 1:10 11/1M 1:50 12/1M 1:20 13/1M 1:10 14/1M 1:50 15/1M 1:20 16/1M 1:10 17/1M 1:50 18/1M 1:20 19/1M 1:10 20/1M 1:50 21/1M 1:20 22/1M 1:10 23/1M 1:50 24/1M 1:20 25/1M 1:10 26/1M 1:50 27/1M 1:20 28/1M 1:10 29/1M 1:50 30/1M 1:20 31/1M 1:10 32/1M 1:50 33/1M 1:20 34/1M 1:10 35/1M 1:50 36/1M 1:20 37/1M 1:10 38/1M 1:50 39/1M 1:20 40/1M 1:10 41/1M 1:50 42/1M 1:20 43/1M 1:10 44/1M 1:50 45/1M 1:20 46/1M 1:10 47/1M 1:50 48/1M 1:20 49/1M 1:10 50/1M 1:50 51/1M 1:20 52/1M 1:10 53/1M 1:50 54/1M 1:20 55/1M 1:10 56/1M 1:50 57/1M 1:20 58/1M 1:10 59/1M 1:50 60/1M 1:20 61/1M 1:10 62/1M 1:50 63/1M 1:20 64/1M 1:10 65/1M 1:50 66/1M 1:20 67/1M 1:10 68/1M 1:50 69/1M 1:20 70/1M 1:10 71/1M 1:50 72/1M 1:20 73/1M 1:10 74/1M 1:50 75/1M 1:20 76/1M 1:10 77/1M 1:50 78/1M 1:20 79/1M 1:10 80/1M 1:50 81/1M 1:20 82/1M 1:10 83/1M 1:50 84/1M 1:20 85/1M 1:10 86/1M 1:50 87/1M 1:20 88/1M 1:10 89/1M 1:50 90/1M 1:20 91/1M 1:10 92/1M 1:50 93/1M 1:20 94/1M 1:10 95/1M 1:50 96/1M 1:20 97/1M 1:10 98/1M 1:50 99/1M 1:20 100/1M 1:10

## **L** Details of hoarding required or any other occupation of the public highway

The complete perimeter boundary shall be secured with a secure stable hoarding to prevent unauthorized access to the site and protect the surrounding area to a degree from noise, dust etc. The security hoarding shall be subject to all the relevant signage and warning displays. The site will be secured.

All visitors, subcontractors to site must report to check point A and undergo a site induction carried out by the site manager, explaining site rules and conduct briefing/ tool box talk on surrounding and environmental control measures.

## **M** Details of how pedestrian and cyclist safety will be maintained, including any proposed alternative routes (if necessary), and any Banksman arrangements.

When vehicles are entering or leaving the site, these will be supervised by our road Banks men. Where vehicle are unloading in Willoughby Road, this will be supervised by either banks men.

The general public/pedestrians will have right of way along the pathways that Has been identified and where the scaffold gantry is available.

If the pavement area are being used for the construction purpose then the at which time the cones will be placed to quadrant off the pedestrian and cyclist ,

The pedestrian and the cyclist will be requested to use the pavement on the opposite side of the road.

The appointed Logistic's Manager will also ensure that the external perimeter of the site is regularly patrolled (twice a day) to ensure that any debris is kept clear of the pavements.



## **N** Details Of How Traffic Associated With The Development Will Be Managed In Order To Reduce Congestion.

As the site is very close to Hampstead underground tube station, all workers are encourage to take the tube to come to site or any other public transport or by cycle .

Their tools can be dropped off for by the company vehicle.

As the road is only dual carriage way and Hampstead High Street is very busy, the logistic of the delivery have to be planned carefully. The deliveries all scheduled based on the site activities and called off very precisely.

The delivery to site will be booked in using a pre-plan delivery schedule logs to avoid congestion to the road. Should a lorry/vehicle arrive that has not been booked in, that lorry will be turned away

## **O** Details Of Any Other Measures Designed To Reduce The Impact Of Associated Traffic (Such As The Use Of Construction Material Consolidation Centres)

The congestion of transport in any construction project is due to unorganised delivery.

Therefore it is important to have a detailed project programme includes all element involved in the project and when is it required . This programme to be reviewed from time to time not only to ensure the project runs on time but for ordering or material as well. This will play a major in reducing the congestion on the road due to

- unplan delivery
- last minute ordering
- Ordering in small quantities as and when required.

Apart from that using a construction consolidation centre to reduce the number of deliveries to sites and operating materials ordering from sites on a just-in-time (JIT) basis can improve :

- construction project efficiency in terms of traffic congestion ,
- time spent receiving deliveries
- and the management of materials on-site.
- It can also improve safety as the construction site
- Reduce cost on delivery
- will be less congested with materials.



## **P** Details Of How Any Significant Amounts Of Dirt Or Dust That May Be Spread Onto The Public Highway Will Be Cleaned Or Prevented.

The control of dust will be the site manager's responsibility and in accordance to the company's environmental policy. This must be monitored and supervised constantly and treated with high priority due to the effects on the local and immediate environment as well as the individual operators involved.

All activities which are likely to create excessive dust shall be closely monitored and appropriate control measures will be in place identified by a risk assessment and relevant COSHH assessment.

All practicable measures must be in place prior to the activity proceeding. As before the assessment will be carried out using health and safety hierarchy guidelines to avoid, by using other means, reduce, by limiting the hazard, (dampen area with water) and by isolation, to segregate and contain the activity so it limits the hazards to the many.

The men will patrol the road minimum twice a day to based on the activity on site. The road will be

- swept from time to time to keep it clean
- debris or rubbish from construction seen on the road will be picked up immediately
- Jet wash if required
- Road sweepers to be brought in if / as necessary
- Scaffolding will be coved with monoflex.

## **Q** Consultation Evidence

Evidence and details of consultation on a draft CMP with local residents, businesses, local groups (e.g. residents/tenants and business associations) and Ward Councillors. Details should include who was consulted, how the consultation was conducted and the comments received in response to the consultation. In response to the comments received, the CMP should then be amended where appropriate and where not appropriate a reason why will need to be provided. The revised CMP should also include a list of all the comments received. You are advised to check your proposed approach to consultation with the Council before carrying it out.

**This has not been carried out yet**



## R Construction Working Group

Details of any Construction Working Group that will be set up, addressing the concerns of surrounding residents, as well as contact details for the person responsible for community liaison on behalf of the developer, and how these contact details will be advertised to the community.

As mentioned earlier the contractor will have to be a registered contractor with CCS or would have to register for this particular project.

The contractor's construction team on site will appoint a person to address the concerns of the surrounding residents. His contact detail will be circulated in the news letter as well as will be advertise on the hording.

All complaints whether received by telephone call to the office or verbally on site shall be treated with the utmost urgency and in a most efficient and polite manner.

Whichever way the complaint is made, a log must be made of the complaint and entered in the complaints log held at Contractor's Head Office, so the relevant person/s can act immediately on the issue, whilst at the same time being monitored by the person appointed on site.

All efforts practicably possible must be taken to deal with the complaint to a satisfactory degree and a follow up must be made by the contracts manager to ensure the complaint has been dealt with in a satisfactory manner.

The client will appoint a representative as a liaison office, whom all the local residents can get in touch with if there is any issues which is not being address by the contractors. The contact detail will be advertised in the similar manner as above.

In addition a monthly news letter will provide information briefly describing the activities , deliveries and any pre-warn of any major disruptions.

As considerate contractor scheme and Camden's considerate contractor's manual applies in this project, the chances of **not considering the neighbourhood and the surrounding would be very unlikely**

## **S** Details of any schemes such as the “Considerate Contractors Scheme”

([www.consideratecontractorsscheme.org.uk](http://www.consideratecontractorsscheme.org.uk)) that the project will be signed up to. Contractors will also be required to follow the “Guide for Contractors Working in Camden” also referred to as “Camden’s Considerate Contractor’s Manual”

As the contractor has not been appointed, currently un able to provide with any detail but it will be stipulated that one of the condition for the contractor to be appointed as a scheme The contractor should ;

- has to register the site with Considerate contractors scheme
  - sign up with to follow the Guide for Contractors Working in Camden - Camden’s Considerate Contractors manual
- and the contractor has to abide the procedure.

## **T** Amelioration and monitoring

Ameliorate, Monitor and measures over construction traffic including procedures for notifying the owners and or occupiers of the residences and businesses in the locality in advance of major operations delivery schedules and amendments to normal traffic arrangements (if any).

Notify by leaflets and any emergency will be notified immediately by the site personnel within the immediate vicinity .

Any complains to be address as soon as possible.

## **U** Details of other construction sites in the local area and how your CMP takes into consideration the cumulative effects of construction local to your site.

Not aware of any other construction taking place in the surrounding area therefore the cumulative effects of local construction has not been considered in this Construction management plan.



## **V** Any other relevant information with regard to traffic and transport.

As part of environmental approach the contractor should source resources from local area provided that specification, requirements and costs are met which in turn minimises the cost of transportation. The contractor should implement his company environmental policies on site as well.

The contractor should provide the SWMP as part of the CPM whereby it would have impact the transportation congestion.

## **W** The CMP should also include the following statement

This CMP should form the basis of a CMP that the Council will seeks to secure via a S106 agreement.

Please liase with:

Marshall Levine  
Marshall F. Levine & Associates  
81 Haverstock Hill  
London NW3 4SL

Web:  
[www.marshalllevineassociates.com](http://www.marshalllevineassociates.com)  
Email: [info@marshalllevineassociates.com](mailto:info@marshalllevineassociates.com)  
Mob: 07710132716  
Tel. :0207 586 7149  
Fax :0207 586 7149