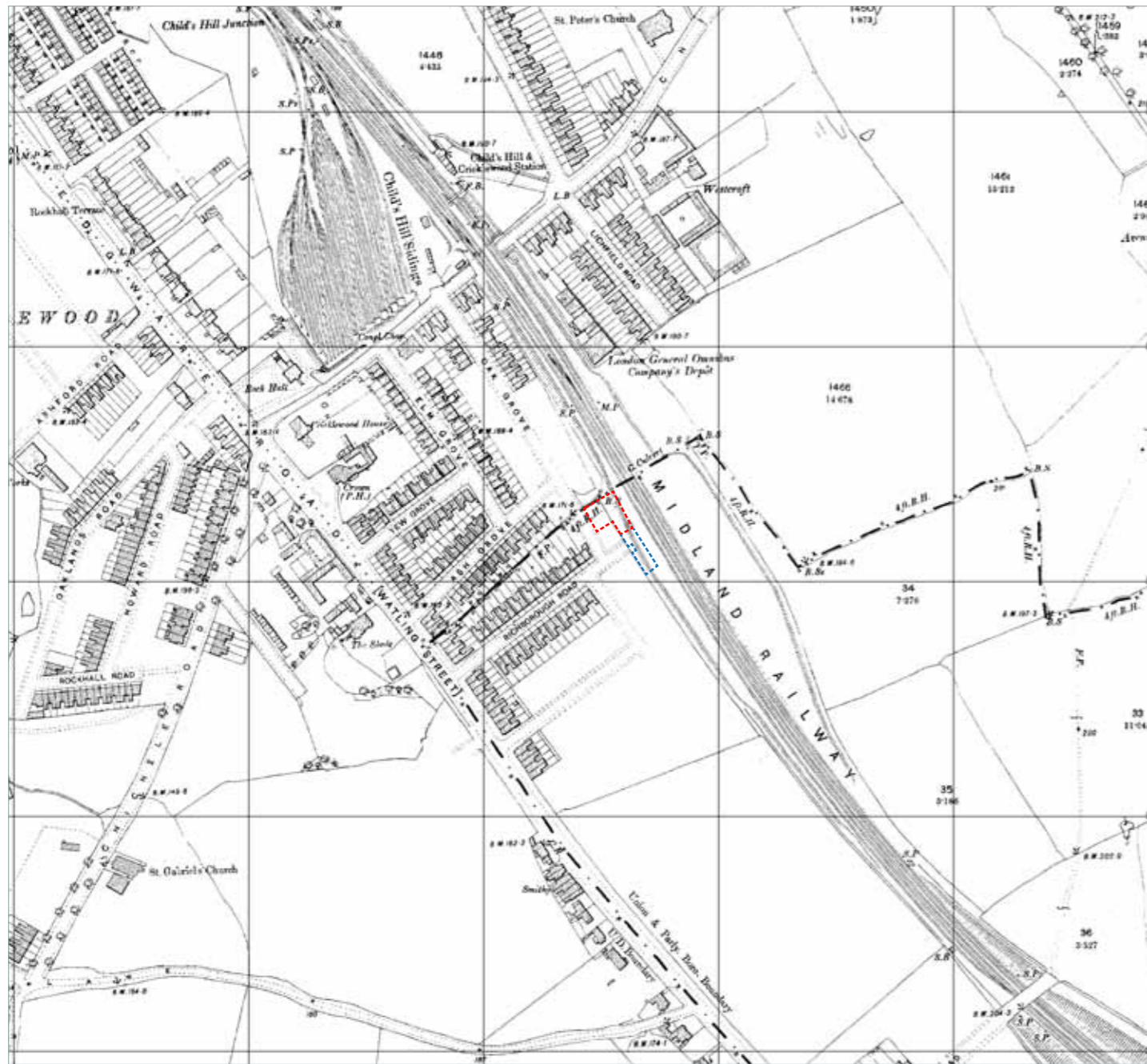


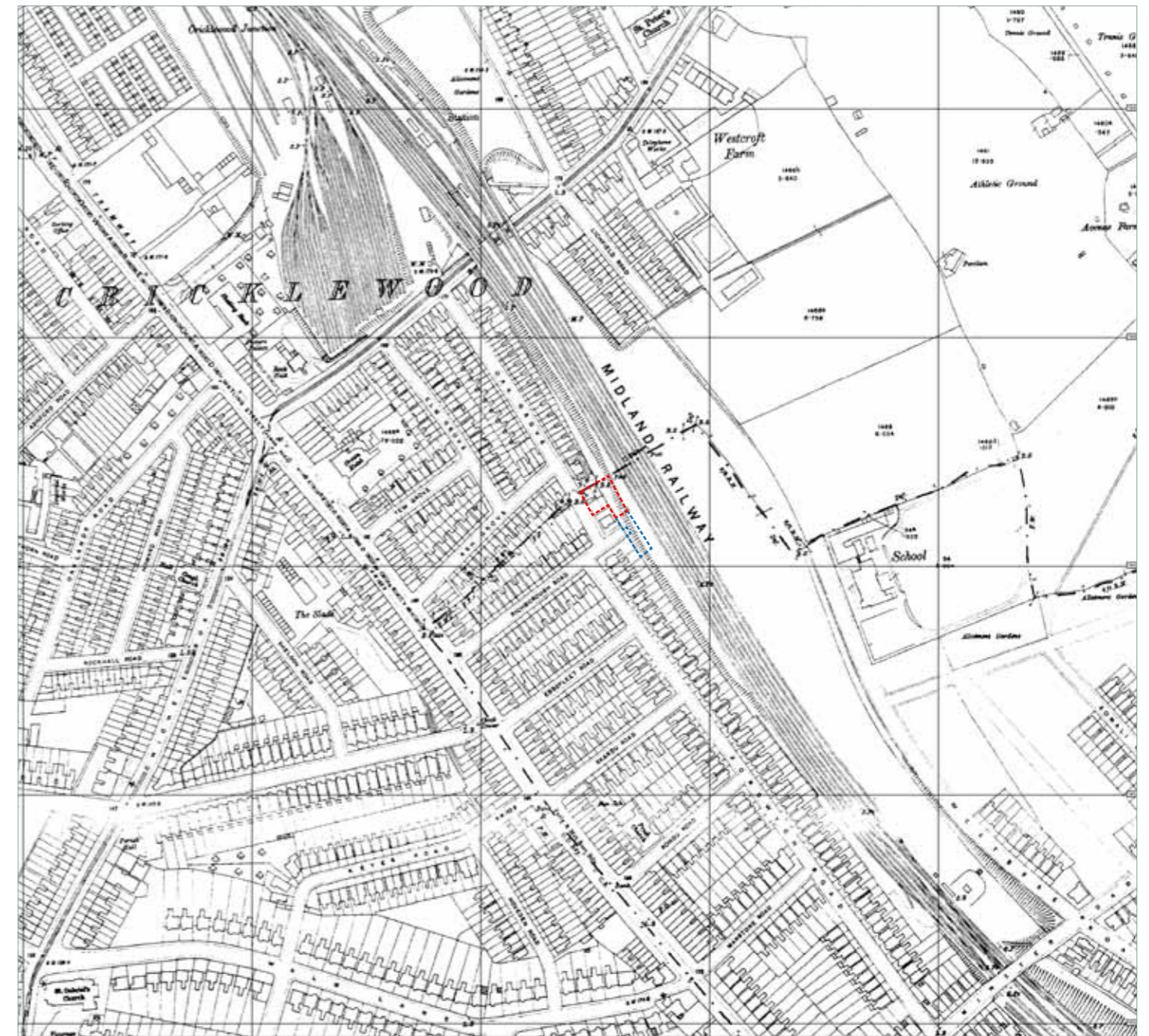
# HISTORY OF THE SITE



## 1896

The above map shows the eastern side of what is now Cricklewood Broadway beginning to develop. Ash Grove and Richborough Road are fully built and the northern end of Oak Grove is developed already with the remainder of the road alignment mapped out.

The map also indicates that the alignment of the current London Borough boundaries dates back to that time, with the development site being located immediately south of the boundary.



## 1915

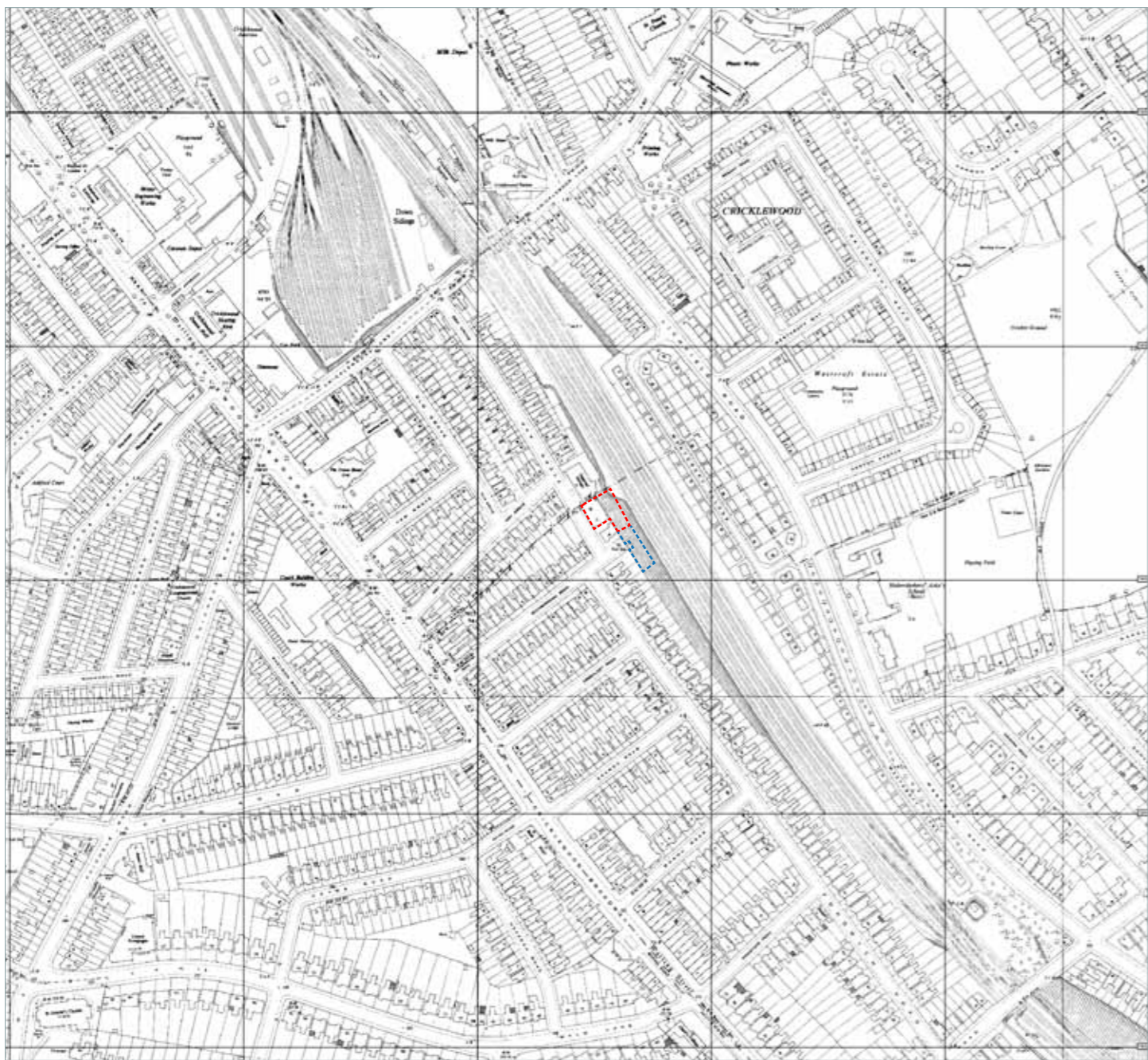
The area between Cricklewood Broadway and the railway line is now fully developed and laid out in the street grid that still exists to today.

The Parish boundary defines the northern edge of the site, which now appears to be broken down into two plots.

Whilst 59 Oak Grove appears to be occupied by a couple of smaller, non-residential buildings, it is not entirely clear from the map whether there is a building on 61 Oak Grove.

A footpath is indicated along the Parish boundary, between 55 and 59 Oak Grove.





1954

By now 55 Oak Grove has been redeveloped as the garage.

The development site (59-61 Oak Grove) contains one building. This contained a wig factory (light industrial use).



2013

The above Ordnance Survey map shows the site vacant and the fragmented nature of the urban form to the proposed site and its immediate context in strong contrast to the well defined street pattern surrounding it.



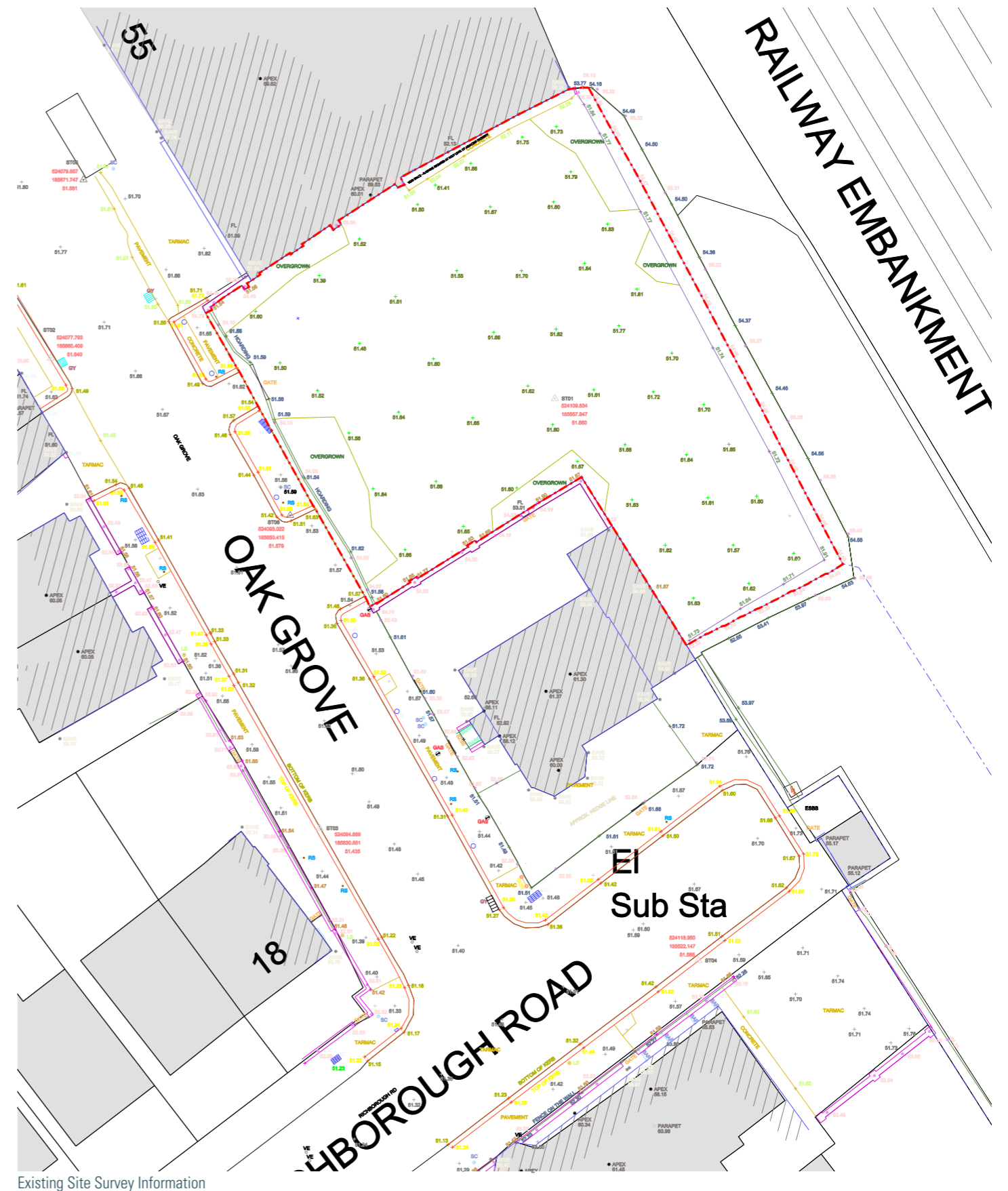
# EVALUATION

## CONSTRAINTS

- Compact site of 764 sqm / 0.08 ha
- Irregular L-shaped site measuring around 35 m north to south with a predominant depth of 28 m reducing to 12 m at the southern end.
- Railway line at the top of an embankment to the north east
- Existing trees along this boundary are part of a SNCI forming an ecological corridor and buffering the proposed development from the impact of the railway.
- The site is broadly level sitting at around 51m AOD with the exception of the embankment which forms the eastern boundary where it rises by around 2.5m with a 2m fence to the top of the embankment.
- There is some overlooking from the rear windows of number 19 Richborough Road to the south approximately 6m from the southern boundary.
- Number 58 and 60 Oak Grove are a minimum of 13m from the site measured to their bay windows with a street tree to their frontage providing a layer of screening.

## OPPORTUNITIES

- Re-establish the predominant building line of Oak Grove and repairs the broken urban grain
- Create street frontage that animates this part of Oak Grove enhancing the security and outlook of adjacent homes - particularly 58 and 60
- Provide a more sustainable use for a currently under-used site in form of housing to meet borough's demands
- New affordable homes for key workers from local community
- Pocket's preferred arrangements and business model is suitable for a site of this size
- No trees on the site itself, opportunity to enhance the area through improved landscaping
- East-West frontage supports a double loaded corridor design
- Predominant height of surrounding properties provides context for the scheme
- No additional parking is required for the scheme (due to conditions on lease from Pocket)
- The site is well located for easy access to buses and tube



Existing Site Survey Information



# VERNACULAR

## LOCAL BUILDING EXAMPLES

The predominant pattern of built form is Victorian with the majority of development emerging with the railway.

The nearby Cricklewood High Street offers examples of the greatest architectural expression and scale. As illustrated these are predominantly of a red brick with a rich texture of relief, detail and ornamentation. A strong contrast is typically set up between vertical expression of brick work and horizontal banding in detail.

The more immediate context is of a very consistent form with Victorian semi detached and terraced properties all featuring bay windows and echoing the rich detail of the high street. As is typical although they all are based on a similar model there is a wide degree of variations within this framework including increased expression of windows through the addition of surrounds and painted reveals, articulated parapet walls (reducing the effect of roofs), varying fenestration for the sliding sashes, brick corbelling and horizontal bands contrasted by a more general vertical expression and emphasis.



One of the local buildings on the high street features a red brick design with vertical



Another more typical example from the high street shows again a strong grided arrangement with games played between horizontal and vertical expressions but with a definite emphasis on the vertical most clearly expressed by the windows



The Crown is a particularly distinctive local building with a strongly expressed colonnade to the base



As can be seen from this view of the high street the overall impression is one of uniformity and a predominance of red terracotta coloured brick



Ebbsfleet Road is typical of the surrounding stock with generous fenestration, expressed entrances and rich detail and texture to the facade



As shown by this photo from Richborough Road the red brick stock, mortar and detailing all vary



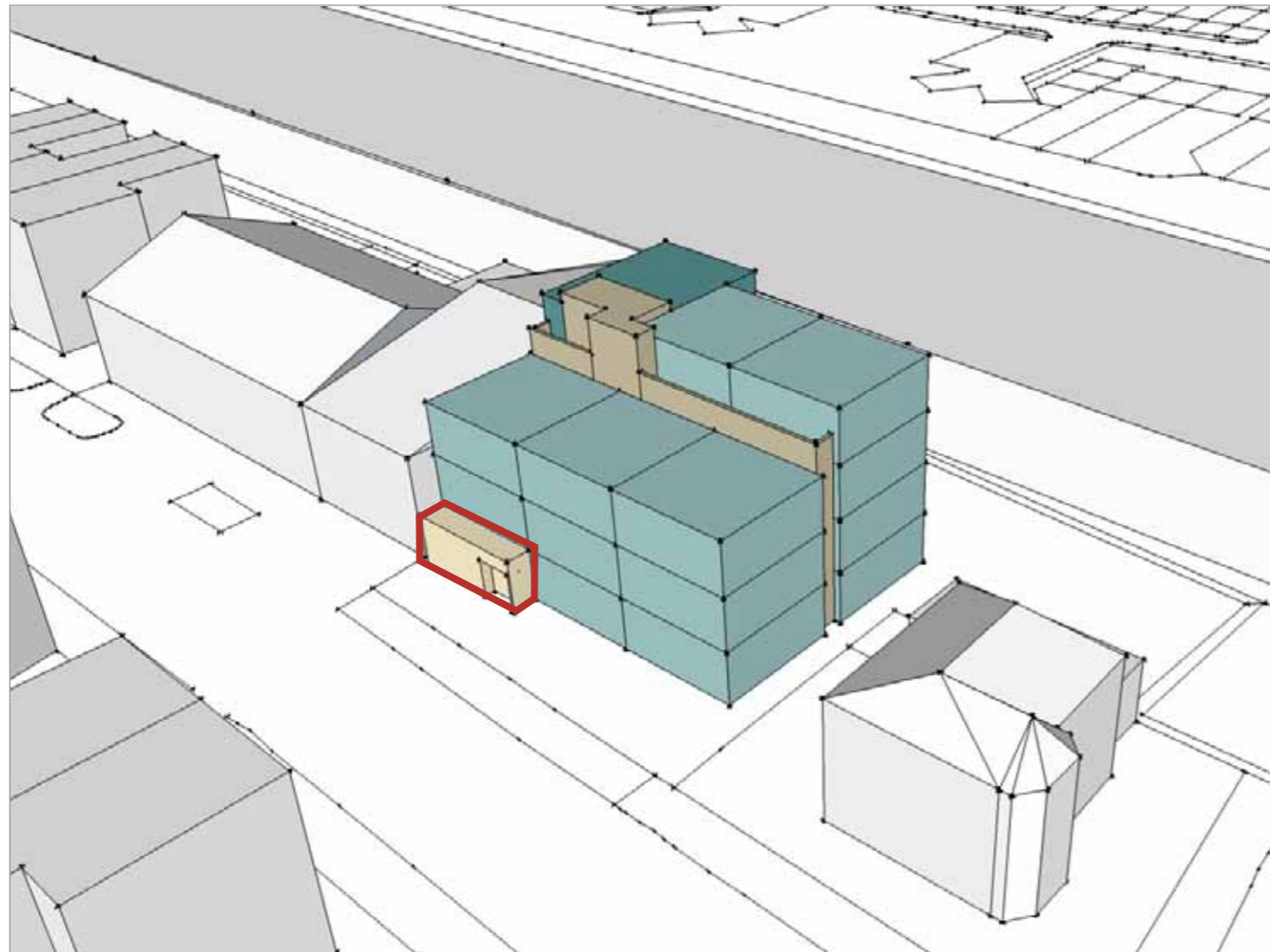
The bay windows dominate the facade with larger areas of brick work broken down by banding and other brick detailing



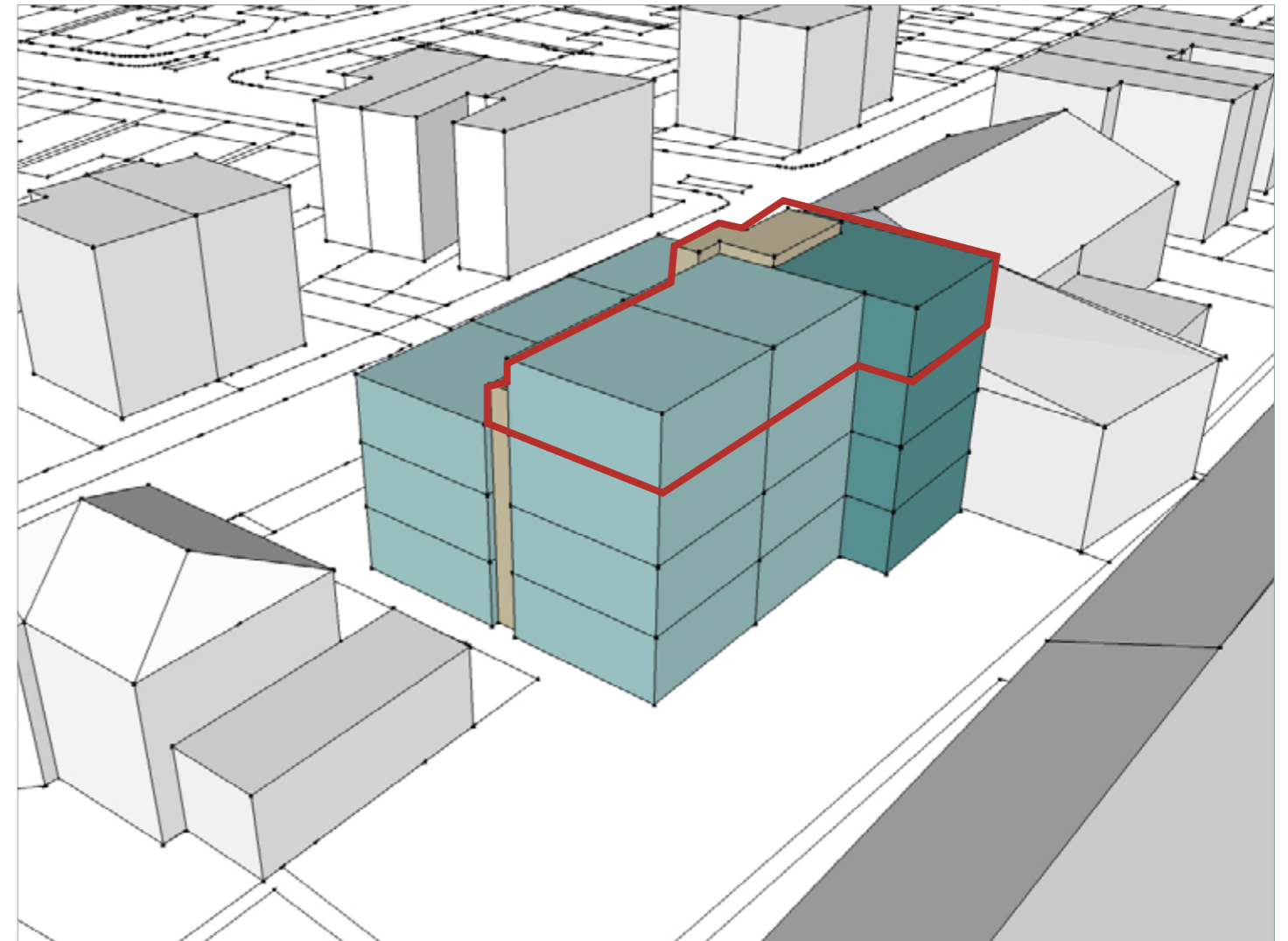
# PRELIMINARY CAPACITY & DESIGN STUDIES

A number of options were explored with Camden planning officers and suggestions made based on preliminary design work. These included:

- Maintain only three storeys to match the surrounding building heights - omit the fourth floor to the rear.
- A strong hierarchy to the elevation with an emphasis on the ground floor and the impact of the roof minimised to echo the context
- Following the predominant building line along Oak Grove closely
- A set back to the second floor to ensure it was legible as a roof extension
- Ensuring that any expression that related to the bay windows of the area was sympathetic without creating a pastiche
- Ensuring a clean expression to the Oak Grove frontage by avoiding any projections from the main building line

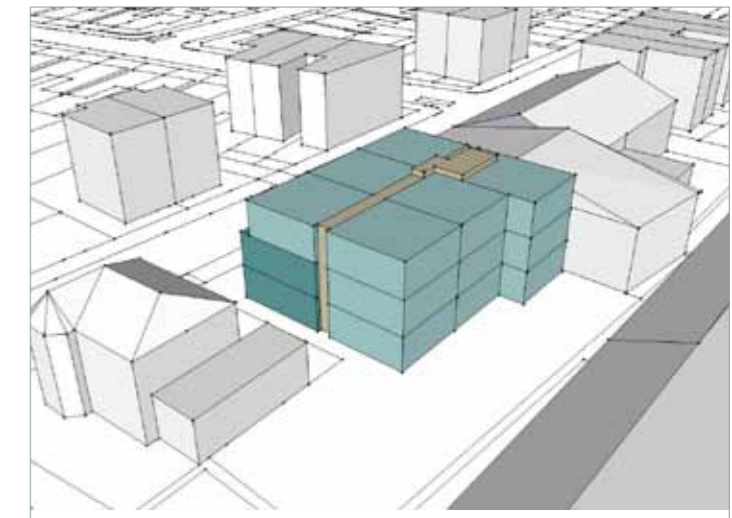
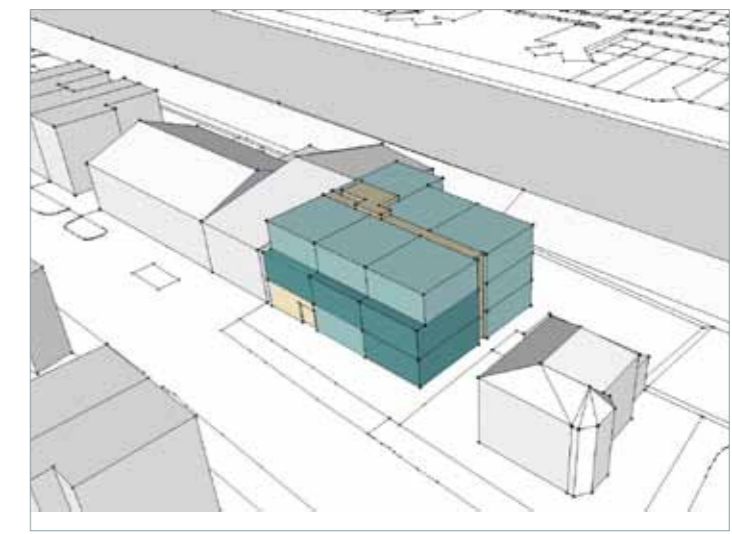
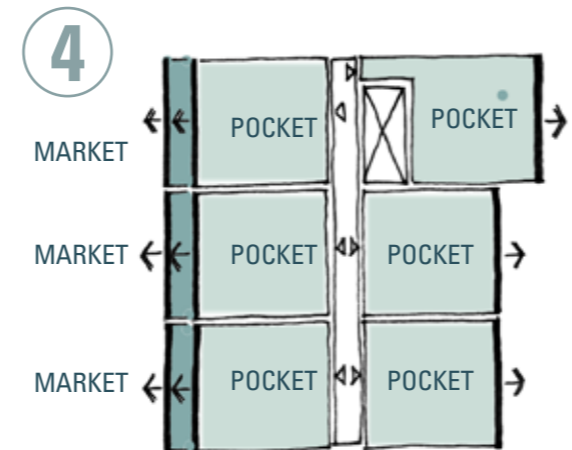
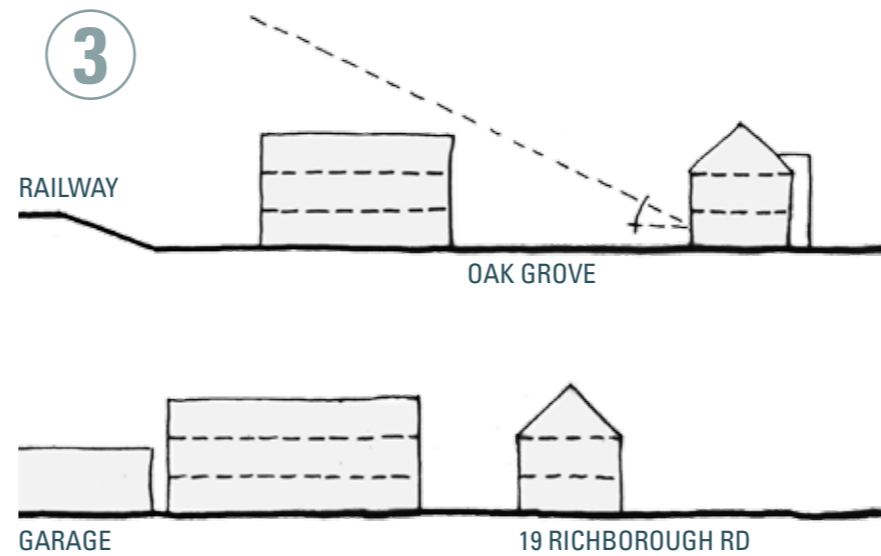


BIRDSEYE VIEW OF PRELIMINARY OPTION FROM THE SOUTH SHOWING PROJECTION TO THE FRONTAGE WHICH WAS AGREED FOR OMISSION FROM THE FINAL DESIGN



BIRDSEYE VIEW FROM THE EAST SHOWING FOUR STOREYS WHICH WERE REDUCED TO THREE STOREYS IN DISCUSSION WITH OFFICERS

# MASSING STUDY AND CONCEPT



## 1 CONTINUOUS ACTIVE FRONTAGE

The southern end of Oak Grove between Ash Grove and Richborough Road currently lacks continuous active frontage. Whilst the garage opens out onto Oak Grove it does not provide any passive surveillance outside of working hours. 38 Ash Grove as well as 18 and 19 Richborough Road have their main elevations towards their respective roads. Only 58-60 Oak Grove overlook this part of the street with habitable room windows.

The proposal seeks to address this by extending the building line southwards and providing active frontage and residential uses that reflect the surrounding uses and offer passive surveillance onto the street.

## 2 RELATIONSHIP TO RAILWAY EMBANKMENT

The proposed building picks up the back building line from its context north along Oak Grove. It is set back the same distance from the embankment.

The remainder of the site will provide amenity space for the new homes, enhancing both landscape and vegetation to the southern part of the site.

## 3 SCALE AND MASSING

To provide a continuous frontage along Oak Grove three storeys are proposed facing the street, relating to the height of the adjacent 19 Richborough Road. The top floor is expressed as part of the roof.

## 4 ARRANGEMENT OF HOMES

On a typical floor six flats will be arranged along a central corridor; three addressing Oak Grove and three the communal gardens and railway line beyond. The 4 market sale one bed flats are located on the ground (1) and first floors (3) of the scheme; all face onto the street.



# PROPOSAL

Based on the massing illustrated previously the proposed building will accommodate 13 one bedroom Pocket flats and 4 market sale one bedroom flats.

The ground floor comprises of the entrance area as well as five flats.

Bin storage and plant rooms are located to the front of the building, easily accessible for collection / servicing but discretely positioned to ensure that they do not negatively impact on the arrival experience.

Secure cycle parking is provided in the rear area, with one space provided per flat.

A communal garden at the back of the development will provide the amenity space for the development.

The first and second floor accommodate six flats each along a communal corridor.



Ground floor



# 01



First Floor

# 02



Second Floor

# R



Roof plan



# LANDSCAPE

As part of the development two different garden spaces are provided.

The first is to the front providing visual amenity and breaking and softening the frontage. The communal rear courtyard is layered back to the railway. A planted bed is located along the eastern boundary which allows the SNCI to be complemented by new planting along this boundary. A paved surface is preferred for a robust finish in areas that are highly trafficked. Privacy is provided to the ground floor flats through the mediating structure of a small privacy terrace.



1.0 Hard Landscape			
Item	Element	Suggested Manufacturer/ Supplier	Range / Size / Colour
ST1	Concrete block paving	Marshalls Tel: 0845 3020 600 www.marshalls.co.uk  or similar approved	Type: Concrete block paving Range: Tegula cobbles Size: 100 x 100 x 60mm thickness Finish: tumbled Colour: Pennant grey Laying: Stack bond
ST2	Timber decking  To private terraces and gardens	To architect's specification	Type: Hardwood timber decking. Smooth boards

2.0 Kerbs and Edging			
Item	Element	Suggested Manufacturer/ Supplier	Range / Size / Colour
E1	Concrete edging to paths	Marshalls Tel: 0845 3020600 <a href="http://www.marshalls.co.uk">www.marshalls.co.uk</a> or similar approved	Type: PCC Flat Top Edging Size: 50 x 150 x 914mm

3.0 Boundaries, Walls and Steps			
Item	Element	Suggested Manufacturer/ Supplier	Range / Size / Colour
B1	1.0m high steel railings  Front boundary	Contractor's choice	Type: Flat top, vertical bar railings. Size: 1.0m high. 50x8mm flat steel bar uprights and 50x50x3mm RHS support posts Finish: Polyester powder coated in RAL colour to match building fenestration and ironmongery.
B2	1.8m high timber privacy panels  Rear deck dividers	Contractor's choice	Type: 1800mm Hardwood Timber 'Hit and Miss' fence  Boards: 70x19mm pressure treated softwood set with Posts: 75 x 75mm timber posts Finish: Natural
RW	Bespoke timber sleeper retaining wall. Approx 450mm high  Rear communal areas	UK Sleepers, UK Timber Ltd Tel: 01536 267107 www.uksleepers.co.uk  or similar approved	Type: New Untreated Oak Sleepers Size: 1800 x 250 x 125 cut to accommodate wall height, set vertically Colour: natural

4.0 Site fittings, Furniture and Features			
Item	Element	Suggested Manufacturer/ Supplier	Range / Size / Colour
F1	Bespoke timber sleeper edging	Woodscape sales@woodscape Tel: 01254 685185  Or similar	Type: Multi-slatted hardwood; Timbers Sections: 125 x250 x500mm lengths set perpendicular to supporting wall Finish: Natural, planed and sanded Other requirements: bespoke fixing into top of sleeper wall. Sleepers to match retaining walls



# THE POCKET HOME



Each home is the result of Pocket's extensive period of prototyping and testing to deliver a practical home within as economical a footprint as possible. The design is the subject of a continuous feedback loop from each new development. Pocket have already completed one development in Camden at Weedington Road, Geoff Marsh Court.

One of the most important attributes in a home this size is balancing the principle living spaces to service zones, namely bathrooms and kitchens. As can be seen from the plan a smaller kitchen with independent washing machine space and a shower room off the hall allow these important spaces to be maximised.

Similarly storage needs to be carefully considered; built-in storage both off the hall and in the bedroom allow everything to be easily accommodated. To address officer comments a deeper unit has been introduced to allow a full set back to upper floors - these are typically market units with one Pocket affordable type.

## ACCESS STATEMENT

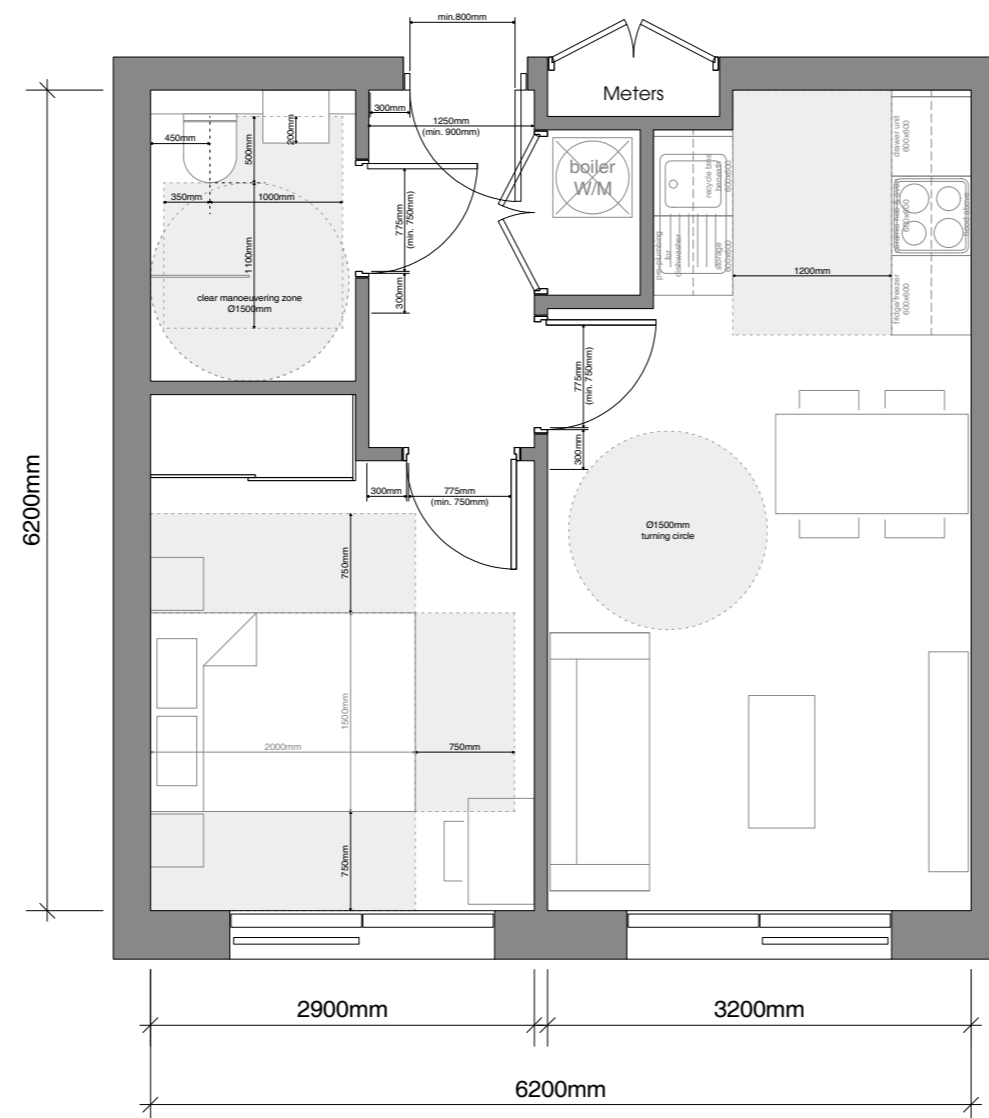
The design seeks to be inclusive. As noted previously the site is easily accessed by public transport. It will be possible for vehicles to drop off in front of the building for visitors and the proposed scheme has been designed to facilitate step free access to the building.

The design and layout of the service area and refuse store has been carefully considered to facilitate easy access and collection from the road serving Oak Grove.

Internal circulation spaces are provided in accordance with Part M with doors and circulation space carefully arranged to ensure ease of access. The lighting and floor finishes of common areas will ensure they are easy for all to negotiate.

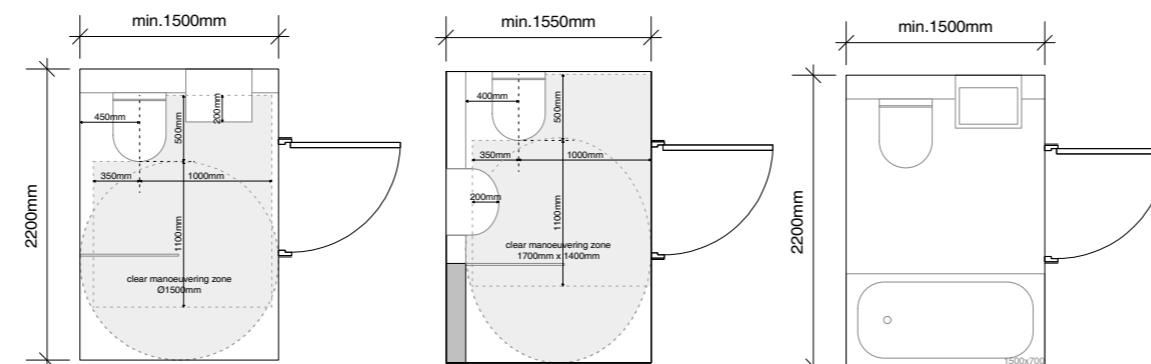
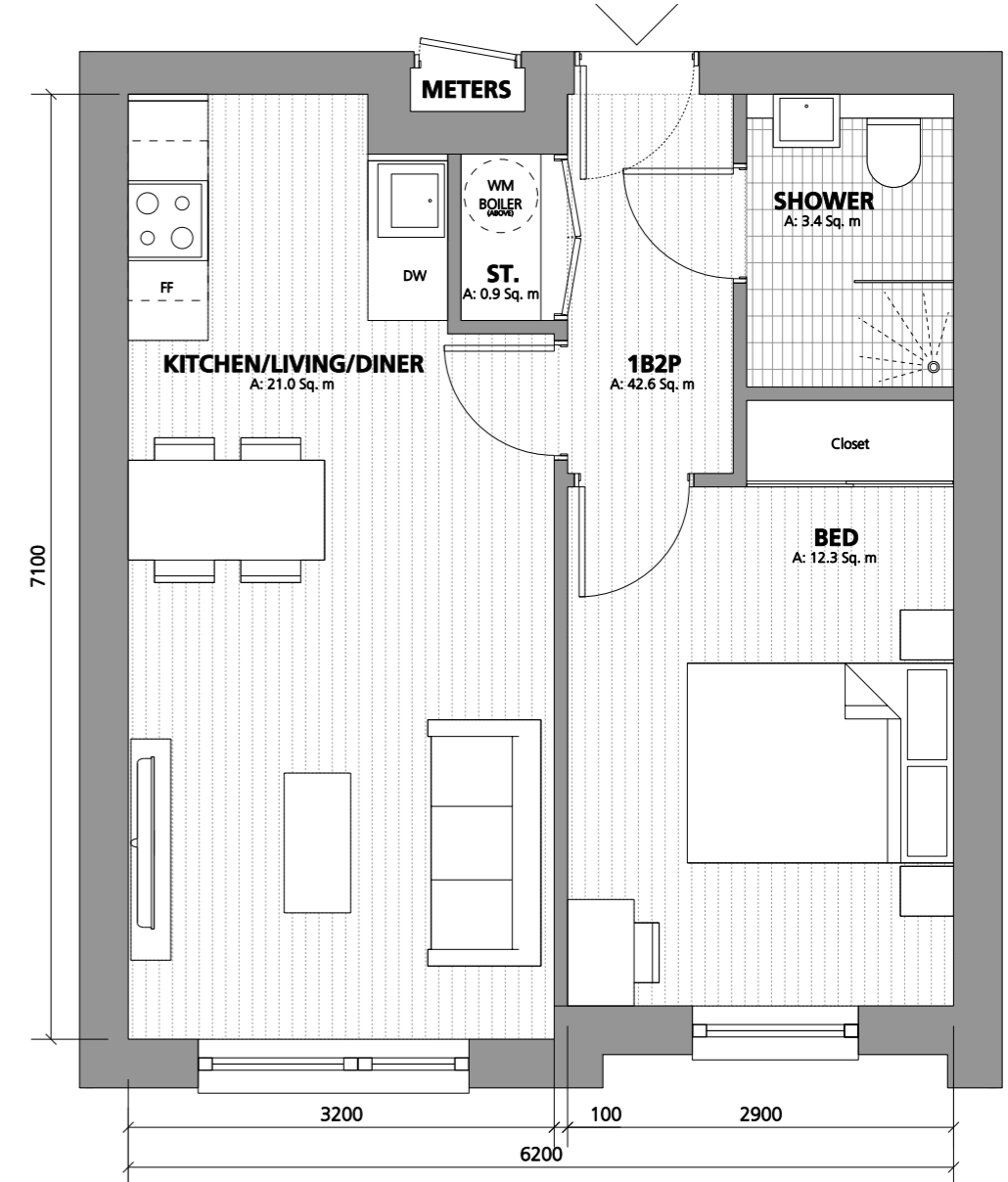
Pocket's flat layout are Lifetime Homes compliant. (Please refer to the typical plan with Lifetime homes compliance overlay).

POCKET 'STANDARD' HOME WITH LIFETIME HOMES OVERLAY



Typical plan of a Pocket home (including overlay to demonstrate Lifetime Home compliance)

ENLARGED PLAN TO ALLOW TOP FLOOR SETBACK



variation of wetroom layout

potential bathroom conversion in future

Development of wetrooms for Pocket home to achieve Lifetime Homes compliance



# LONG ELEVATIONS



## OAK GROVE ELEVATION (WEST)

As illustrated above the street elevation is principally expressed with a red brick to match the context complimented by a neutral cladding panel to the top floor.

The primary expression in the facade is the rhythm of bays that articulate the living rooms and echo the bay windows which predominate in the area. A detailed study of one of these is included overleaf. Within these a subtle hierarchy is proposed of reducing window openings going up the building.

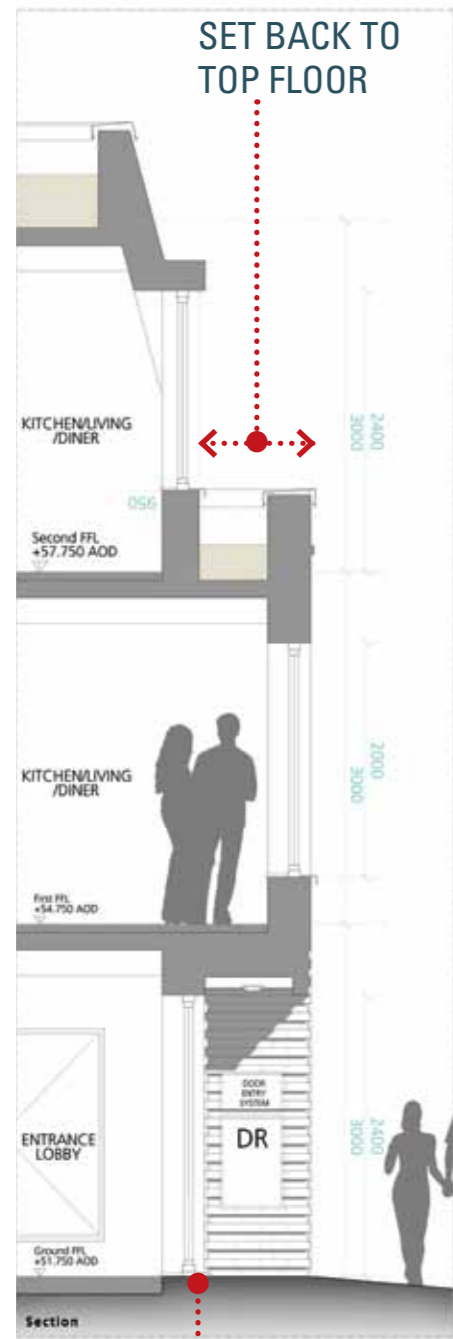
The top floor features a metal clad dormer in dark grey to reduce the building's visual impact.

ARTISTS IMPRESSION OF THE PROPOSED ELEVATION FROM OAK GROVE

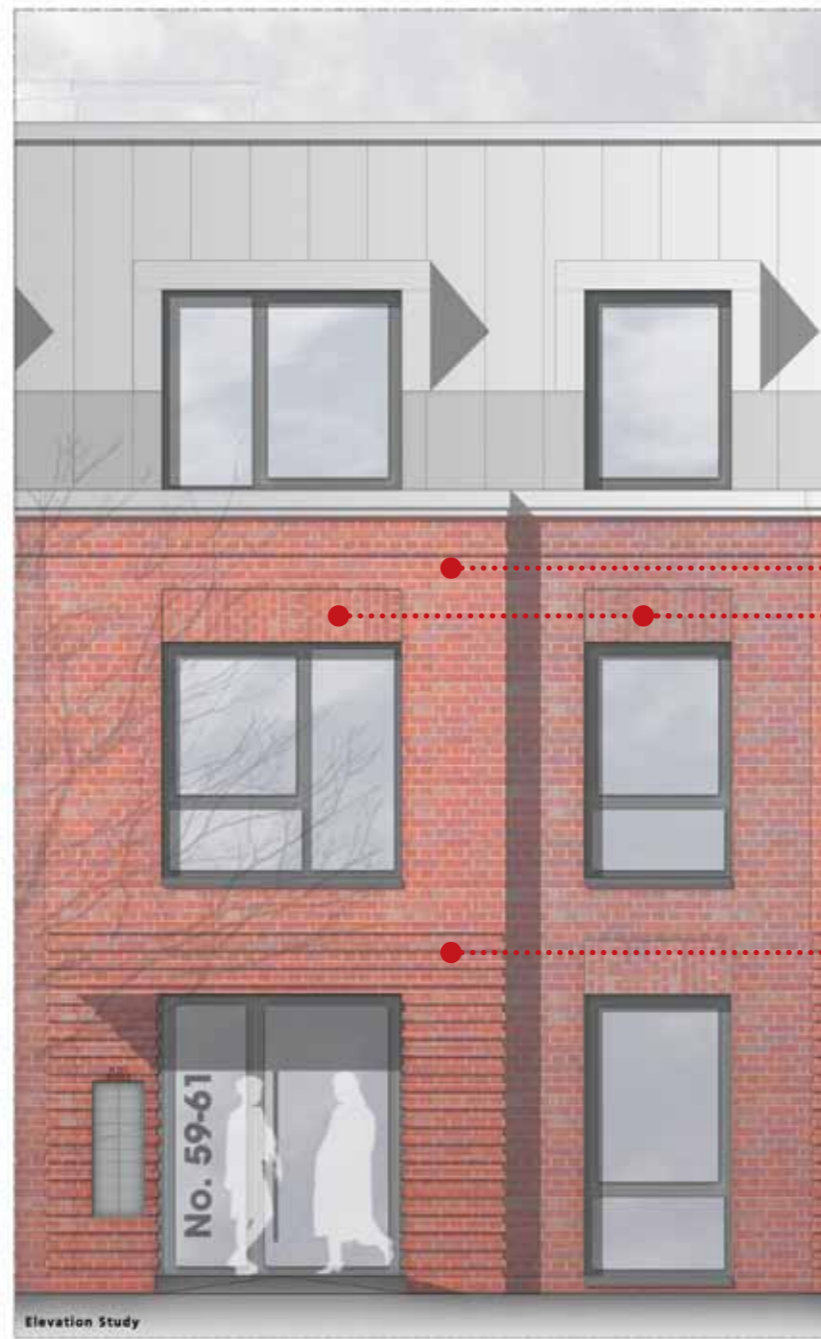




# DETAILED STUDY

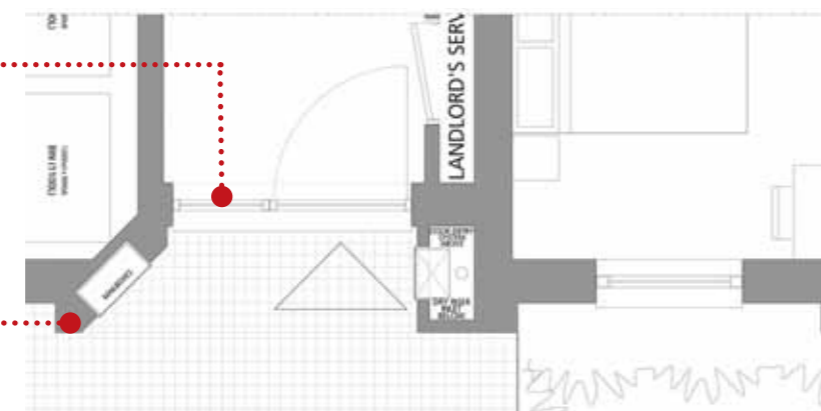


SET BACK TO TOP FLOOR



RECESSED DOOR PROVIDES SHELTER

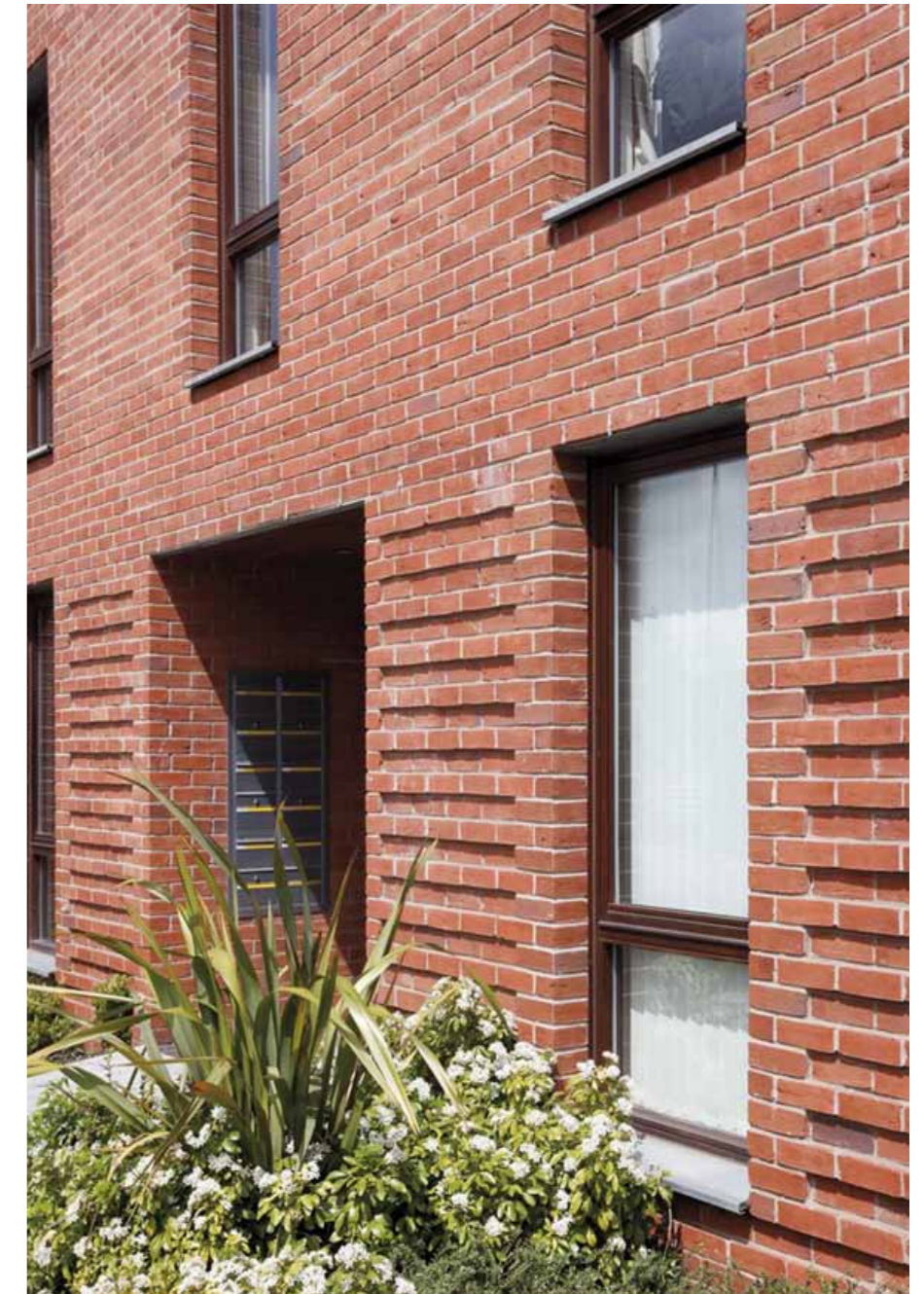
ANGLED WALL TO INVITE AND EMPHASISE ENTRANCE



STRING COURSE REDUCES DOMINANCE OF UPPER AREA OF BRICK

BRICK LINTEL OF 1.5 BRICK HIGH TO ENHANCE VERTICALITY OF WINDOWS

BRICK CORBELLING TO THE BAYS AT GROUND FLOOR



PHOTOGRAPH OF SIMILAR CORBELLED DESIGN WITH RECESSED ENTRANCE



# SUMMARY AND ILLUSTRATIONS

Illustrations on the following pages show the proposed massing within the context of Oak Grove and Richborough Road.

The images show how the expressed brick bays relate to the bay windows and detailing of the adjacent buildings. The three storeys along the front of the building align in height with the pitched roofs of the neighbouring houses.

Through the setback of the top floor the impact of this floor is minimalised.

In summary the proposal brings forward 13 affordable homes on a difficult in-fill site that respects its context and repairs a piece of local townscape through architecture that enhances its context through a modern interpretation of the detail.



Illustration of the proposed building adjacent to 19 Richborough Road





View of the building up close, set between the garage and 19 Richborough Road



**POCKET**