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Site and surroundings

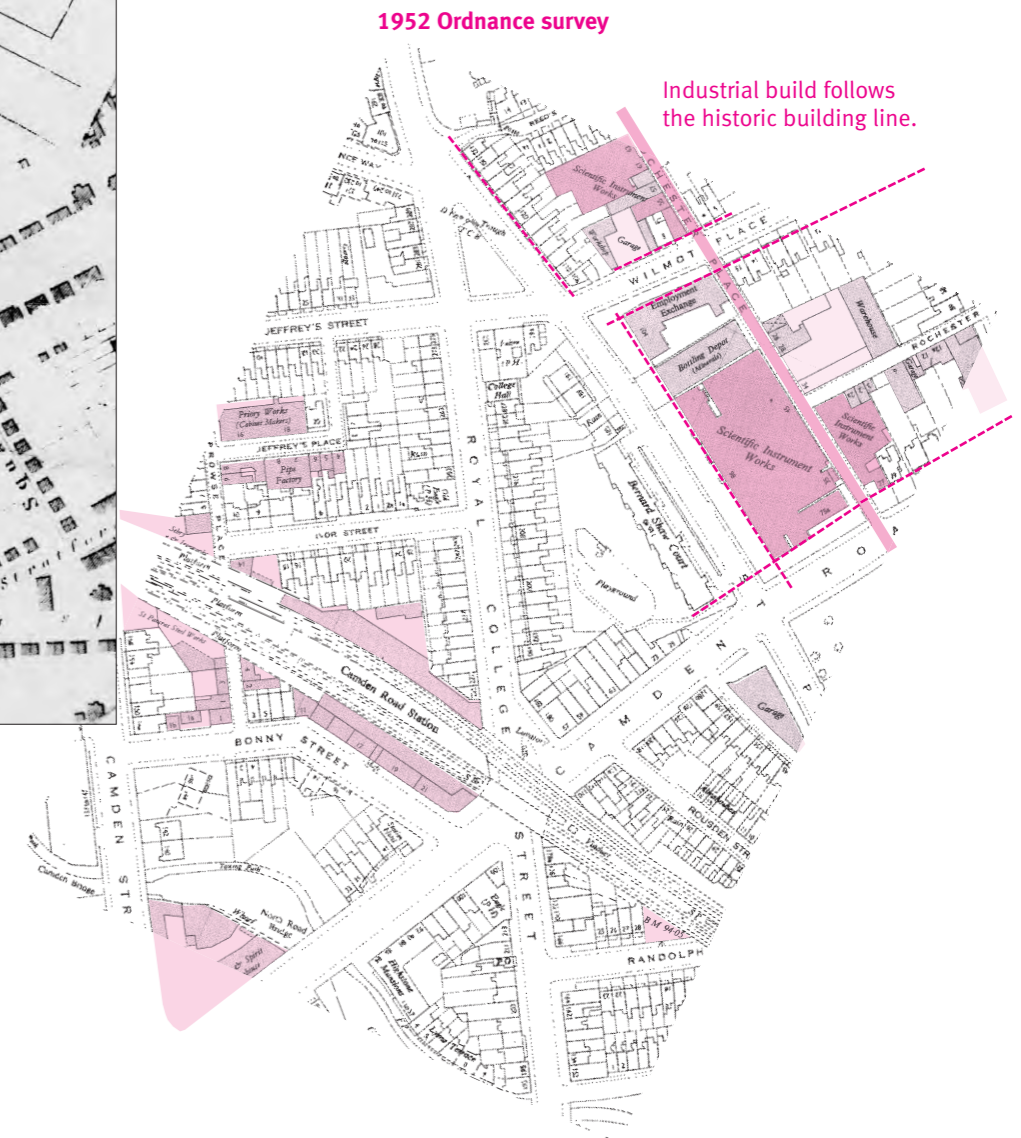
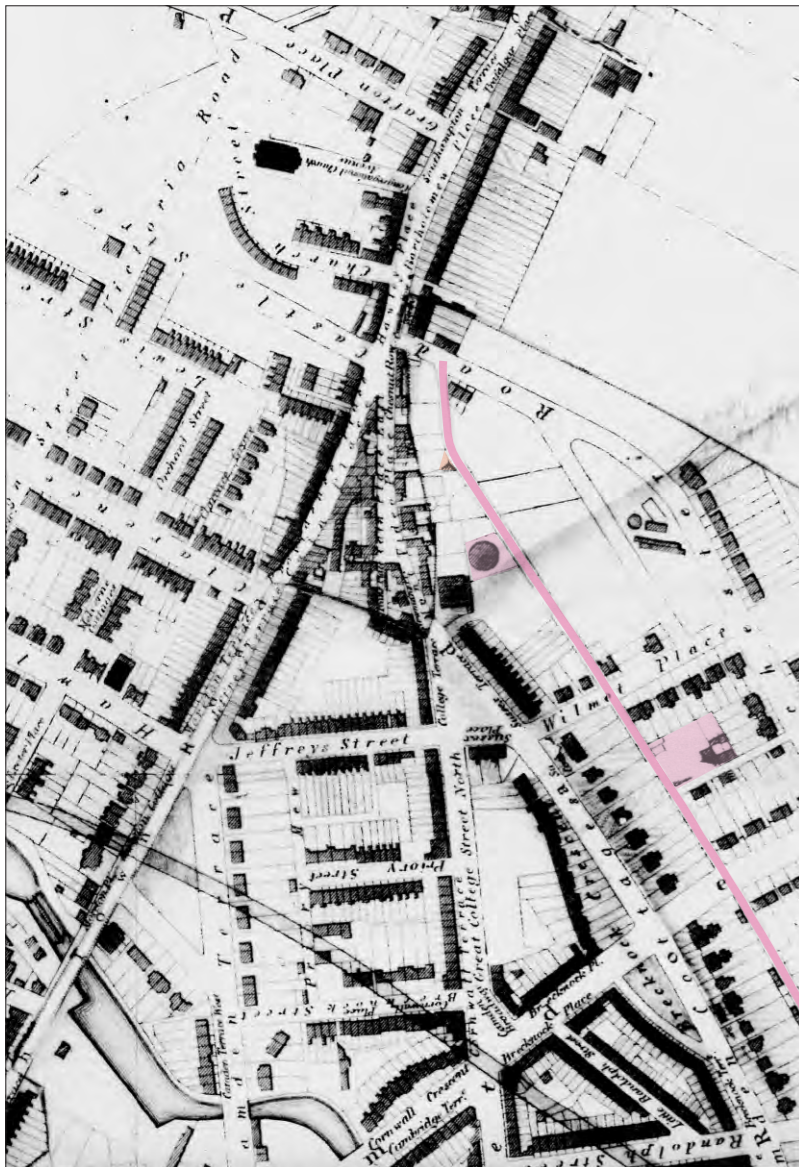
Site and surroundings: Rochester Place

It should be noted by Officers that Rochester Place appears on parish maps from 1849; it was created as a narrow setted street providing access to small scale industrial and workshop properties. The 1860 map showed the growing number of workshops; 1952 map shows the warehouses, industrial and workshop properties that surrounded the application site. Access from the narrow street has not been changed in 165 years – never a main access route.

This is the site of an industrial building; **Rochester Place and its narrow carriageway cannot support the scale of building proposed nor the scale of traffic following build.**

1849 parish map

1860 parish map



1952 Ordnance survey

Industrial build follows the historic building line.

Rochester Place

original route of Rochester Place

industrial/workshop build

original/current build line

2013 Google earth survey



Setting of site in relation to Rochester Place



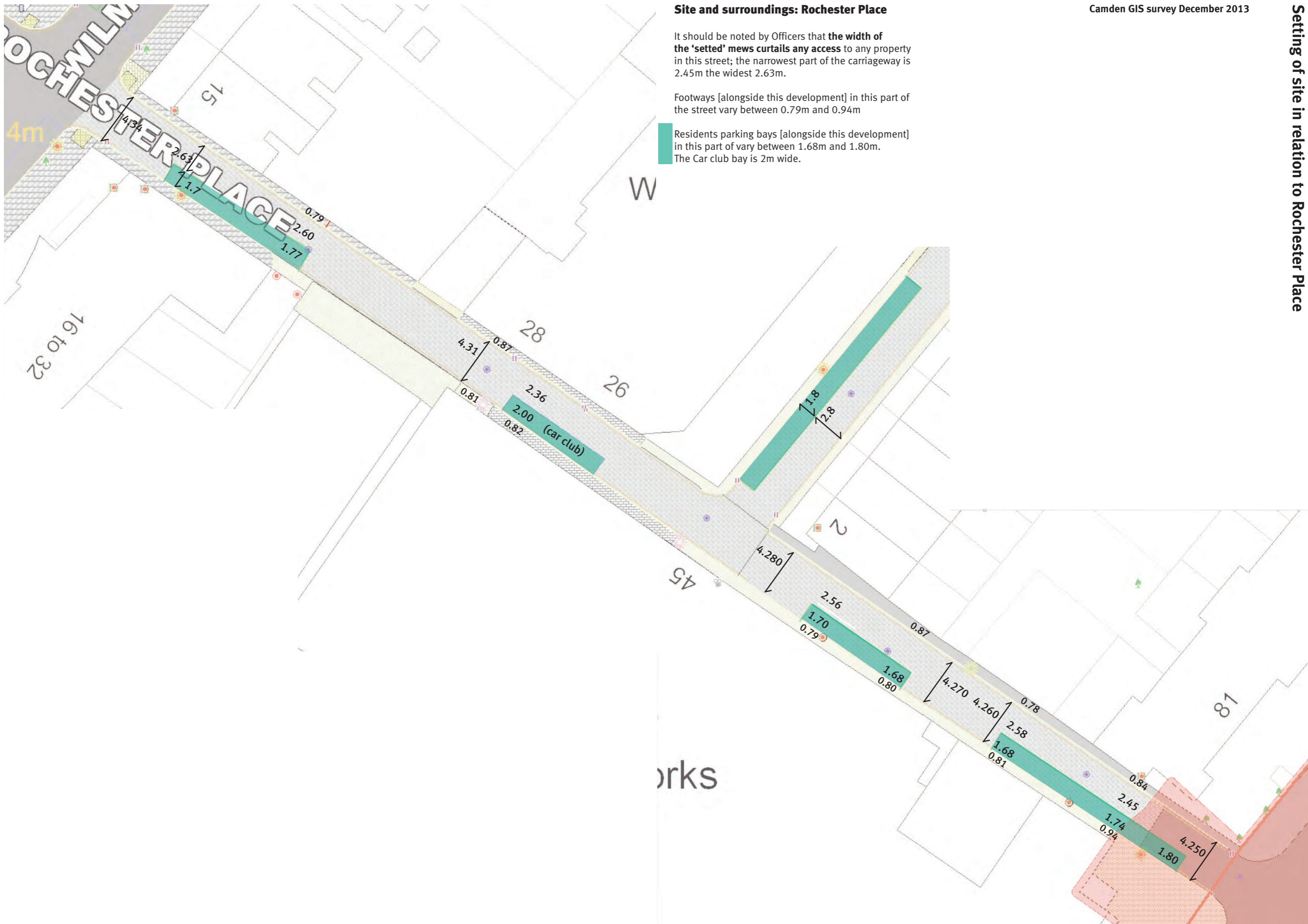


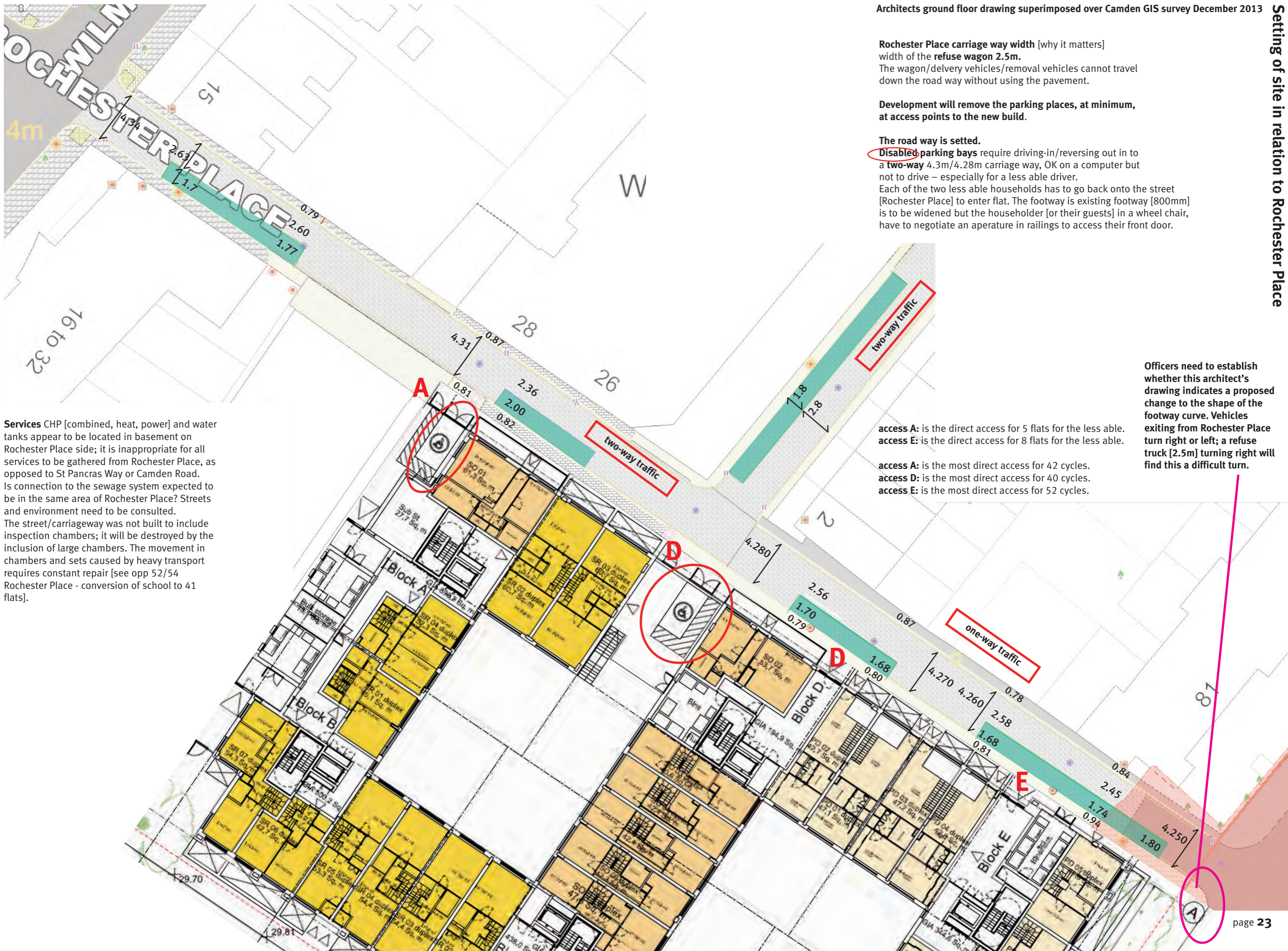
Site and surroundings: Rochester Place

It should be noted by Officers that the width of the 'setted' mews curtails any access to any property in this street; the narrowest part of the carriageway is 2.45m the widest 2.63m.

Footways [alongside this development] in this part of the street vary between 0.79m and 0.94m

Residents parking bays [alongside this development] in this part of vary between 1.68m and 1.80m. The Car club bay is 2m wide.





Services CHP [combined, heat, power] and water tanks appear to be located in basement on Rochester Place side; it is inappropriate for all services to be gathered from Rochester Place, as opposed to St Pancras Way or Camden Road. Is connection to the sewage system expected to be in the same area of Rochester Place? Streets and environment need to be consulted. The street/carrageway was not built to include inspection chambers; it will be destroyed by the inclusion of large chambers. The movement in chambers and sets caused by heavy transport requires constant repair [see opp 52/54 Rochester Place - conversion of school to 41 flats].

Rochester Place carriage way width [why it matters]
width of the refuse wagon 2.5m.
The wagon/delivery vehicles/removal vehicles cannot travel down the road way without using the pavement.

Development will remove the parking places, at minimum, at access points to the new build.

The road way is setted.
Disabled parking bays require driving-in/reversing out in to a two-way 4.3m/4.28m carriage way, OK on a computer but not to drive – especially for a less able driver. Each of the two less able households has to go back onto the street [Rochester Place] to enter flat. The footway is existing footway [800mm] is to be widened but the householder [or their guests] in a wheel chair, have to negotiate an aperature in railings to access their front door.

access A: is the direct access for 5 flats for the less able.
access E: is the direct access for 8 flats for the less able.

access A: is the most direct access for 42 cycles.
access D: is the most direct access for 40 cycles.
access E: is the most direct access for 52 cycles.

Officers need to establish whether this architect's drawing indicates a proposed change to the shape of the footway curve. Vehicles exiting from Rochester Place turn right or left; a refuse truck [2.5m] turning right will find this a difficult turn.