

# LIGHTHOUSE LONDON CHURCH

DESIGN & ACCESS REPORT

MARCH 2025



**REED  
WATTS**

INTRODUCTION

This Design & Access Statement outlines the proposal to rejuvenate the Lighthouse London Church - by extending, re-modelling and refurbishing the existing building located on Finchley Road to provide improved worship space and an ancillary cafe, with new level access provided.

This report has been produced in collaboration with the client and the design team, and follows a pre-application consultation with Camden Council in December 2024.

The proposals follow a previous application by the Church in 2017 for a significantly larger scheme (2017/2092/P) where the resolution was to grant planning permission subject to a s106 Legal Agreement (September 2017).

The brief, and budget, have since been reduced to focus on the key requirements to improve the existing fabric of the building to re-invigorate the existing facilities with a focus on the following design principles.

Design principles:

- More accessible
- More visible
- More sustainable

Spatial requirements

- Generous gathering space at street level to improve accessibility
- Revitalised Worship space for the community
- Open flexible spaces for large groups to promote community based activities and training opportunities
- Office space to allow dedicated space for the charity groups located within the church
- Kitchen facility
- Upgraded sanitary provision including accessible toilets and showers
- Storage

This document contains information about the existing site history, constraints and requirements of the brief, followed by a more detailed explanation of the design proposals and materials.

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LOCATION & SITE

The site is located within the Finchley Road/Swiss Cottage Town Centre, opposite Finchley Road tube station and the O2 shopping centre.

The site is bounded by Finchley Road to the west, Lief House and associated surface car parking to the north/northwest, Alban House to the east and 120 Finchley Road to the south/southeast.

Holy Trinity Church occupies the majority of the site, with the main entrance accessed via a series of steps from Finchley Road. A second accessible entrance is situated on the northern elevation, accessed via a pedestrian footpath which runs from Finchley Road to Alban House, a residential building located to the rear of the Church.



 LLC

1. Finchley Road Tube

2. Lief House

3. Alban House

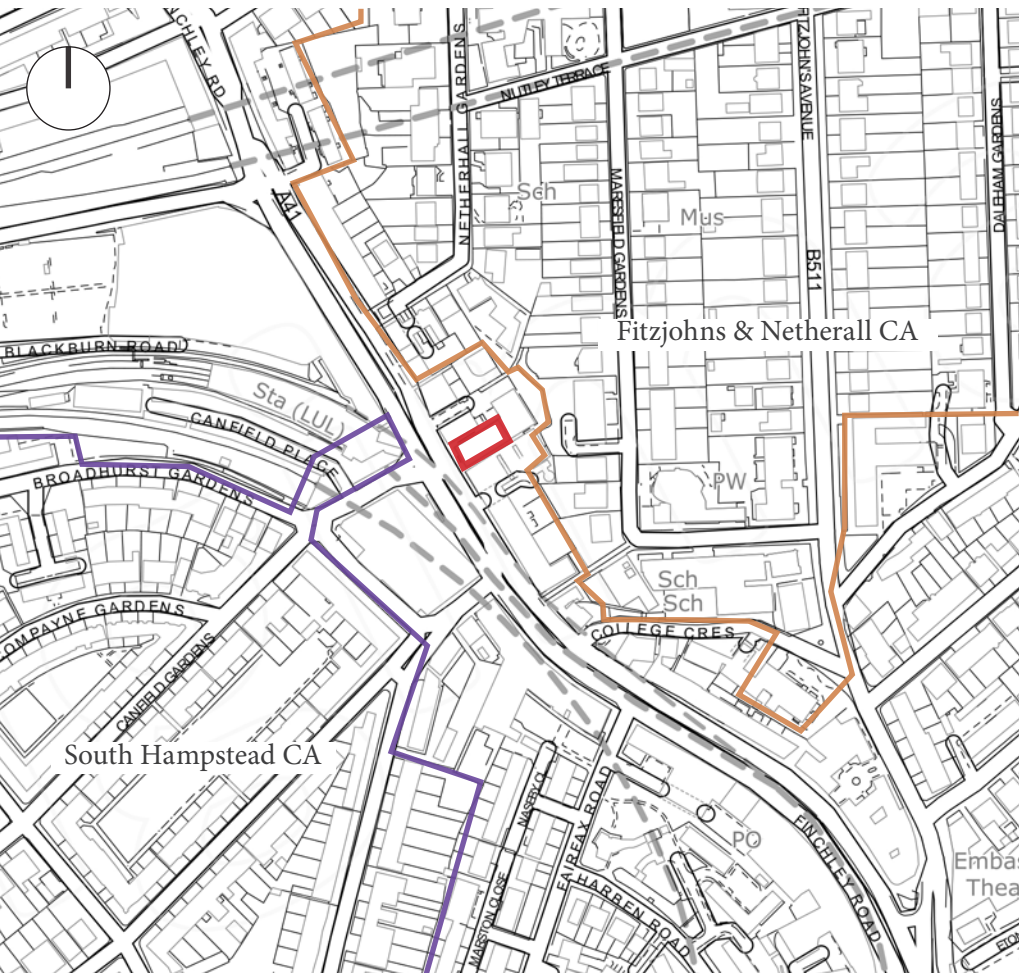
4. 120 Finchley Road

Although the Church has a prominent location on the road, due to its modest scale in comparison to its immediate neighbours the existing building is easy to miss.

The tight site has a number of significant constraints including:

- Change in level (approximately 1.5 meters from front to rear)
- Neighbours/boundary/party wall
- Existing services
- Finchley Road - red route
- Easements
- Existing power supply
- Daylight/ sunlight

The site is not listed, nor is it located in a Conservation Area, however it does sit adjacent to the Fitzjohns Netherhall Conservation Area and opposite the South Hampstead Conservation Area.



Location plan



LIGHTHOUSE LONDON CHURCH

The original church was founded in 1871 and by the early 1900's there were more than 1,000 people worshipping on the site each Sunday. This was a community church for the working classes. However, in 1964, the Victorian building was deemed unsafe and was demolished.

In its place a new, smaller church was built along with Lief House and Alban House, all constructed together out of the same red brick. In 2006 Holy Trinity Swiss Cottage became a Holy Trinity Brompton church plant and in 2018 took on the new name of Lighthouse London Church (LLC).

LLC operations are currently restricted due to the layout and lack of space for multiple groups, especially on Sunday. This limits the growth of the children’s and young people’s ministry, and of the Church as a whole. The LLC ambition is to create a new series of spaces and a highly visible landmark, opposite Finchley Road Tube, marked out as a Christian building of worship and welcome.

The existing single storey Church building is a hybrid reinforced concrete frame/ load bearing masonry structure, built in 1978 to a design by Biscoe & Stanton. Its geometric and sculptural nature are a product of its time, but the building is no longer fit for purpose - both spatially and in terms of its fabric.

Internally the Church is inefficient, tired and worn and no longer appropriate for today’s needs. The Worship space is not large enough to hold the congregation, and limited peripheral space limits the flexibility the Church needs to cater for families and provide space for younger and older children’s groups.

The front entrance and café are raised above street level. Inaccessible and uninviting, it appears exclusive to the congregation rather than open to the general public. Towards the rear of the Church the office provision and ancillary spaces such as WC’s, kitchen and storage are extremely limited.



Original church on the site



Existing church on the site



Finchley Road elevation

Church site



The quinquennial report (conducted in 2022) by Hanslip & Co identifies a significant number of building failings, mainly caused by wear and tear from half a century of constant use.

While piecemeal rectification may solve individual problems the proposals in this report describe a more holistic approach- improving the fabric, upgrading energy efficiency and creating a series of new spaces which are uplifting, fit for use and accessible for all.



View from the tube station entrance.



Outside space vulnerable for anti-social behaviour. Steps at the front prevent accessibility for all.



Water ingress from failing roof coverings.



A tired and worn Worship space.



Lack of adaptability to the rear spaces create unusable spaces to be enjoyed by the congregation and staff.



DESIGN STRATEGY

To extract the most from the brief the design strategy is a delicate balance of retention, refurbishment and extension.

The proposal retains as much of the existing structure as possible but extending where practical to increase space, whilst improving the Church's visibility and accessibility on Finchley Road.

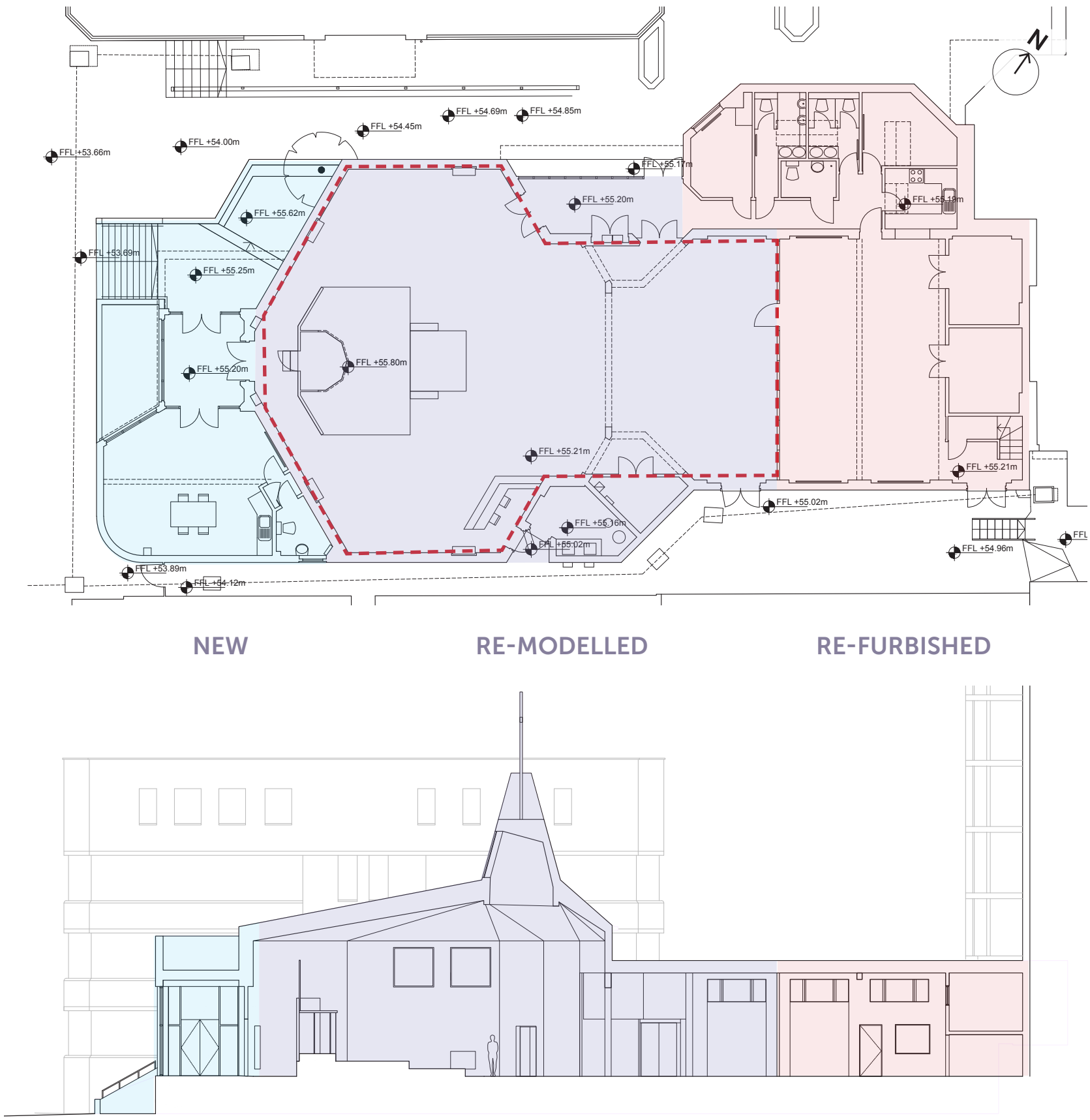
The majority of significant change will happen at the front of the building, facing onto Finchley Road, which includes the demolition of the smaller single storey structures and a reduction in levels to provide a new extension which can be accessed directly from the street. The upper floors of the new structure provide a second storey and flat roof for mechanical plant.

The current building line is maintained which means that the sewer in the pavement will not require re-direction.

The majority of the existing hexagonal plan (denoted by the red dashed line on plan) which forms the Worship space is retained - including the concrete frame, which is embedded within the brickwork wall. The Worship space is retained in the heart of the building but transformed by expanding it eastwards. By replacing the existing roof and moving the stage to follow traditional layout the congregation shall now be facing east and entering from the back.

Towards the rear of the building spaces will be re-modelled and re-furbished to allow the Church to continue providing the services they already do in upgraded facilities.

- New build
- Significant refurbishment
- Lighter refurbishment



Plan and section of existing Church and extent of work proposed in each area



## DESIGN OVERVIEW

The new proposals respond to the Church's need to evolve by improving and extending the current building, acknowledging LLC's prominent location and creating a modern place for worship, accessible for all. Care has been taken to minimise impact on neighbours.

At the heart of the building will be the enlarged main Worship space, with room to allow for Christmas and Easter celebrations.

The front of the building is lowered to provide public access at street level. The entrance will be located at the north/east corner, which is directly opposite Finchley Road tube station. A cafe/reception runs the width of the building at ground floor - a welcoming social space for gathering. New flexible spaces, and a recording studio are located on the floor above. These provide valuable accommodation for young Church members but also increases the building mass at the front, helping to fill the gap between the Church's taller neighbours.

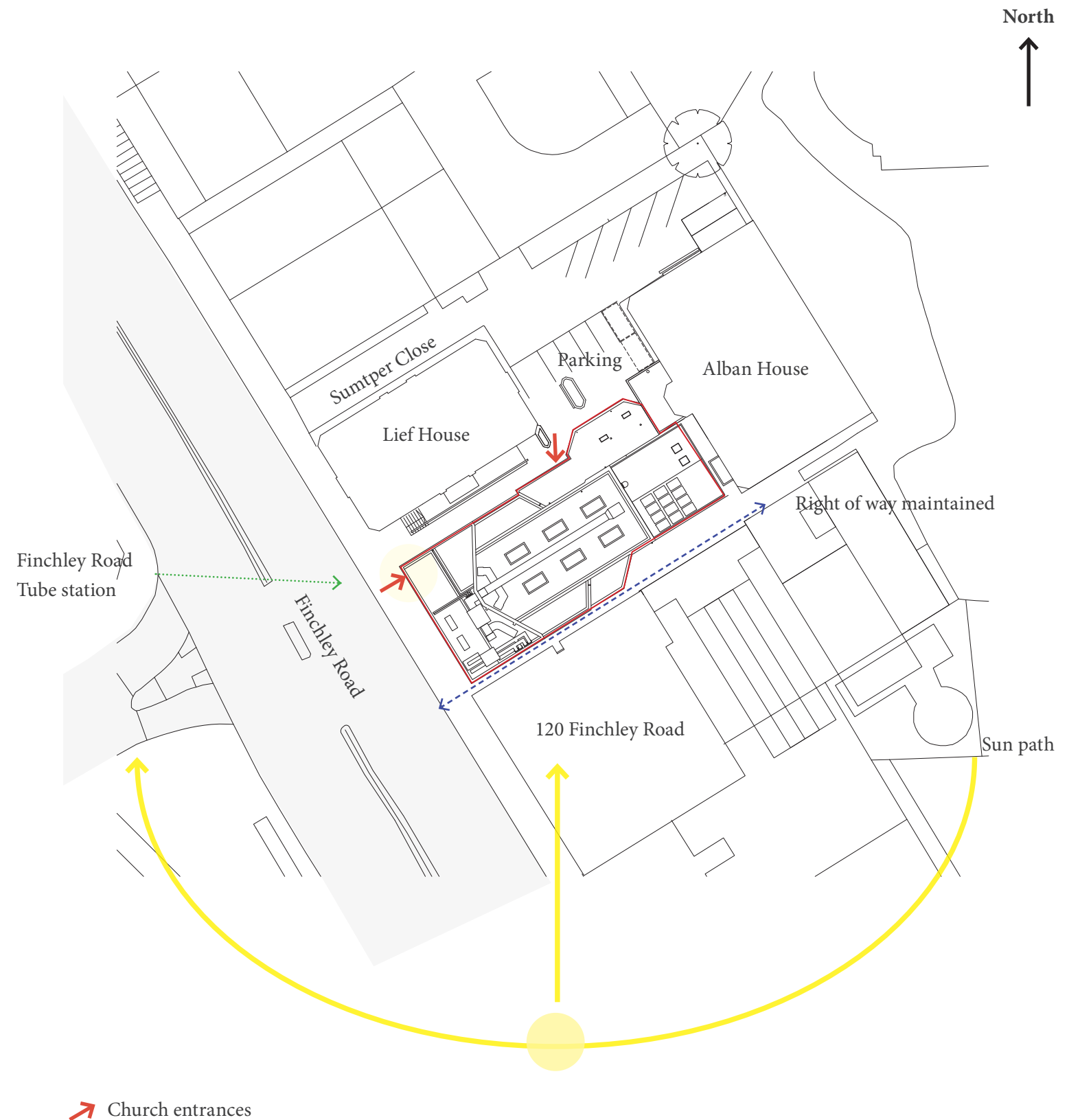
Towards the rear of the building is the side entrance and the ancillary spaces - toilets, kitchen and storage, re-modelled and refurbished withing the existing space. A further flexible space for training or similar social activities is located here and a small office.

At the top of the building is space for modern plant. This heightened parapet allows for acoustic attenuation and a reduction in visual impact to the neighbours.

A Schedule of Areas is shown on the following page.

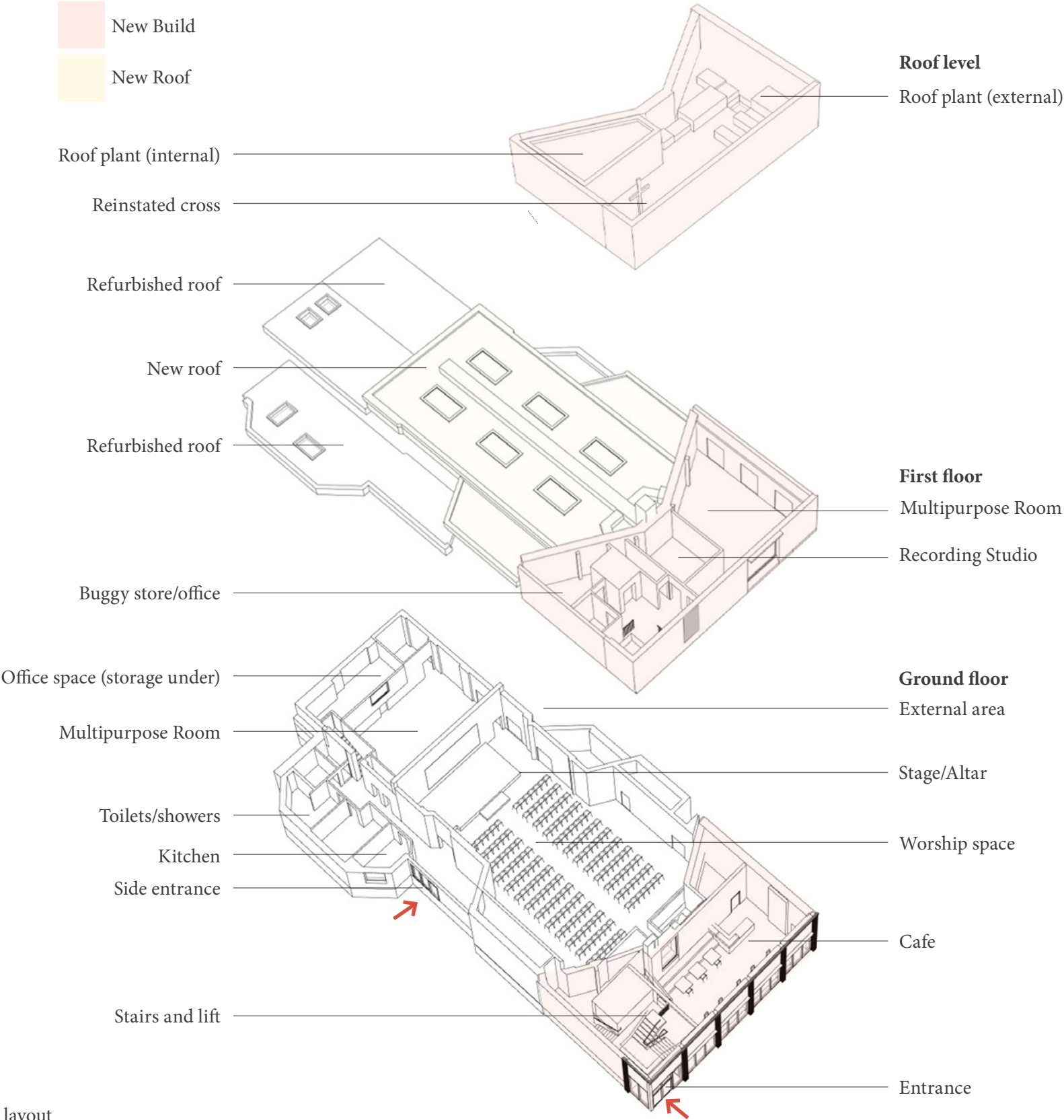


View from south



Church entrances

PROGRAMME



Exploded Axonometric showing proposed church layout



# GROUND FLOOR

Accessed directly from Finchley Road, visitors enter through a lobby before moving into a tall open space- a welcoming place to gather which incorporates a cafe. The facade at this level is predominately glazed, to create a highly animated, naturally lit space.

Steps and a lift provide access to the main Worship space and the rest of the building above/to the rear. The Worship space is located in the centre of the plan, with seating either side of a central aisle and a large stage at the far end, following traditional layouts.

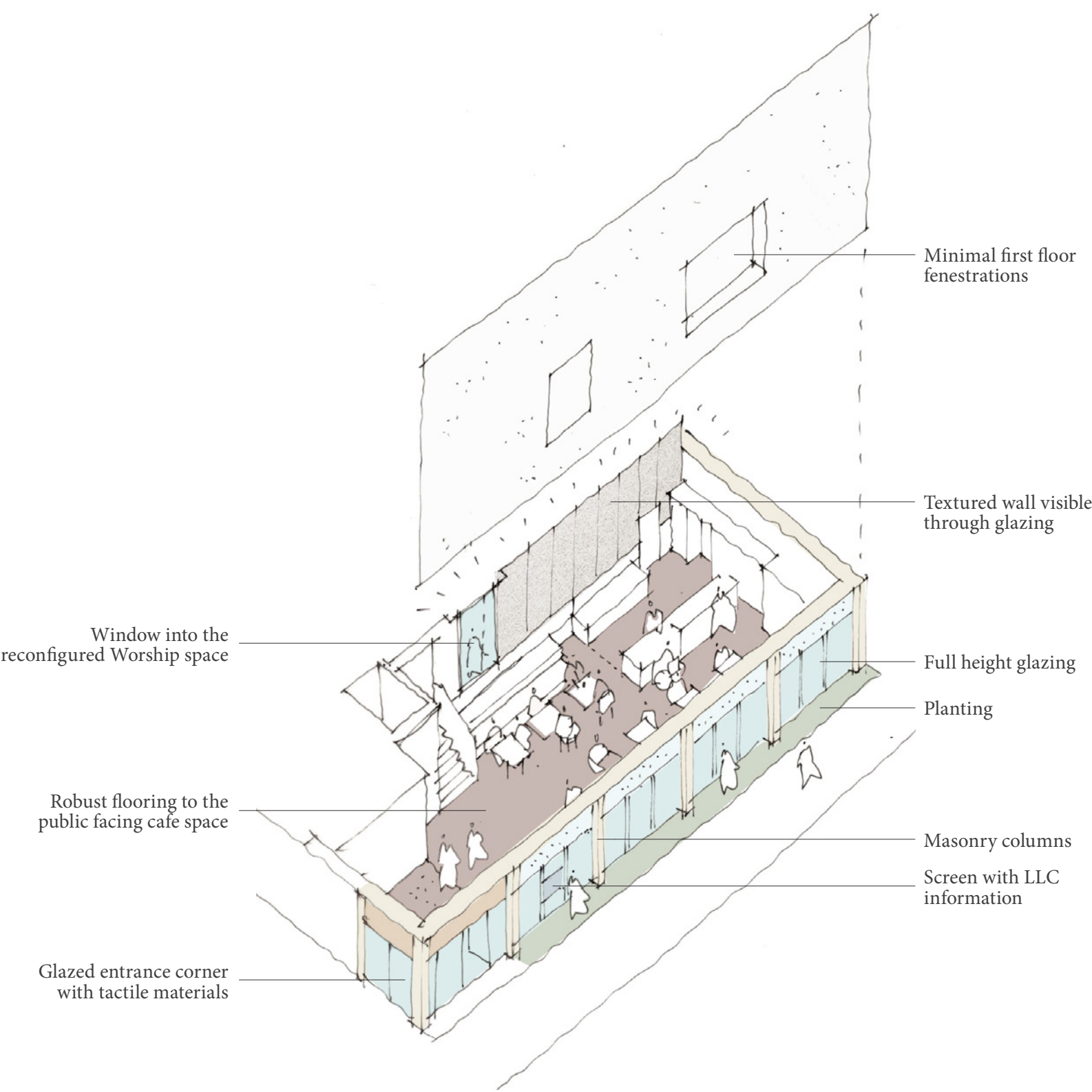
On the other side of the stage is a flexible space for a crèche or training, which can be divided in two with a folding screen if required. This space also has access onto a small external area.

The rear of the Church can be accessed from a secondary entrance, adjacent Alban House entrance, via the pedestrian pathway from Finchley Road. The main toilet block, showers and kitchen are positioned towards the rear of the church (where the current facilities are located).

It is proposed the original bell, which is currently concealed behind the parapet on the roof, be relocated to the side entrance.

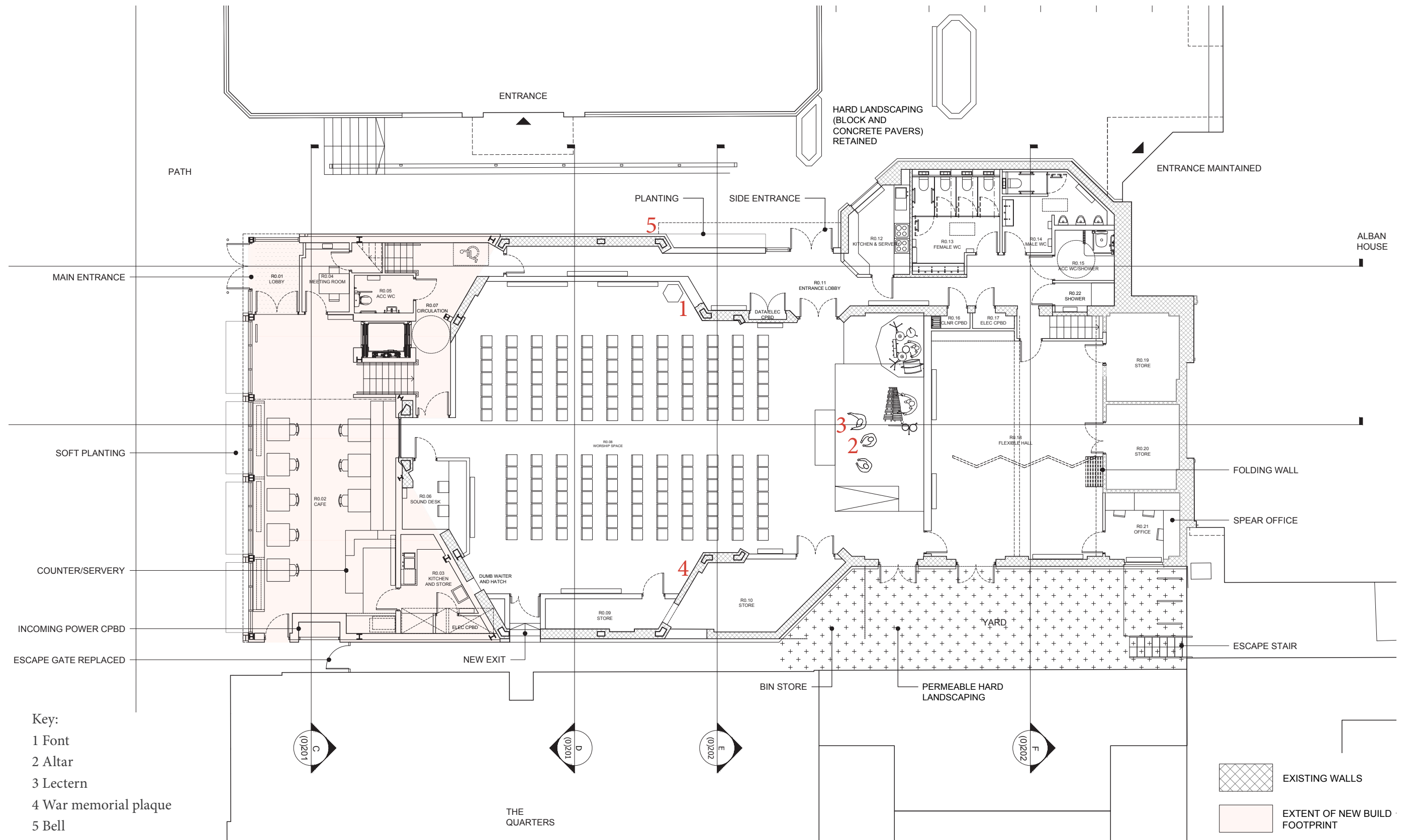


New cafe space on Finchley Road



Exploded axonometric sketch of the front proposal

## GROUND FLOOR PLAN





## WORSHIP SPACE

At the heart of the plan is the large Worship space.

This space has been designed to accommodate their existing congregation, with space to accommodate a growing community. The space has been designed to support live music which is a regular occurrence and specific to the congregation's service. The existing stage is re-instated, with the wall behind used for large projection.

The majority of the existing hexagonal structure will be retained- with as little demolition as possible. However, the space will be transformed by re-modelling the volume and removing the existing roof. The ceiling will be raised along its length, with new exposed steel beams and timber joists spanning the roof.

At high level textured panels will provide acoustic attenuation whilst 6 roof lights punctuate the new roof allowing daylight into the reinvigorated space.

Services such as duct work, lighting, speakers and projectors will be integrated into the new roof structure.

There is additional storage space to the wings on the south side of the plan for additional furniture and equipment.



Image taken from a Sunday service in the existing Worship space



Proposed view towards the cafe



Proposed view towards the stage

UPPER FLOORS

The stairs and lift, located above the main entrance on the north east corner provide access to the first floor.

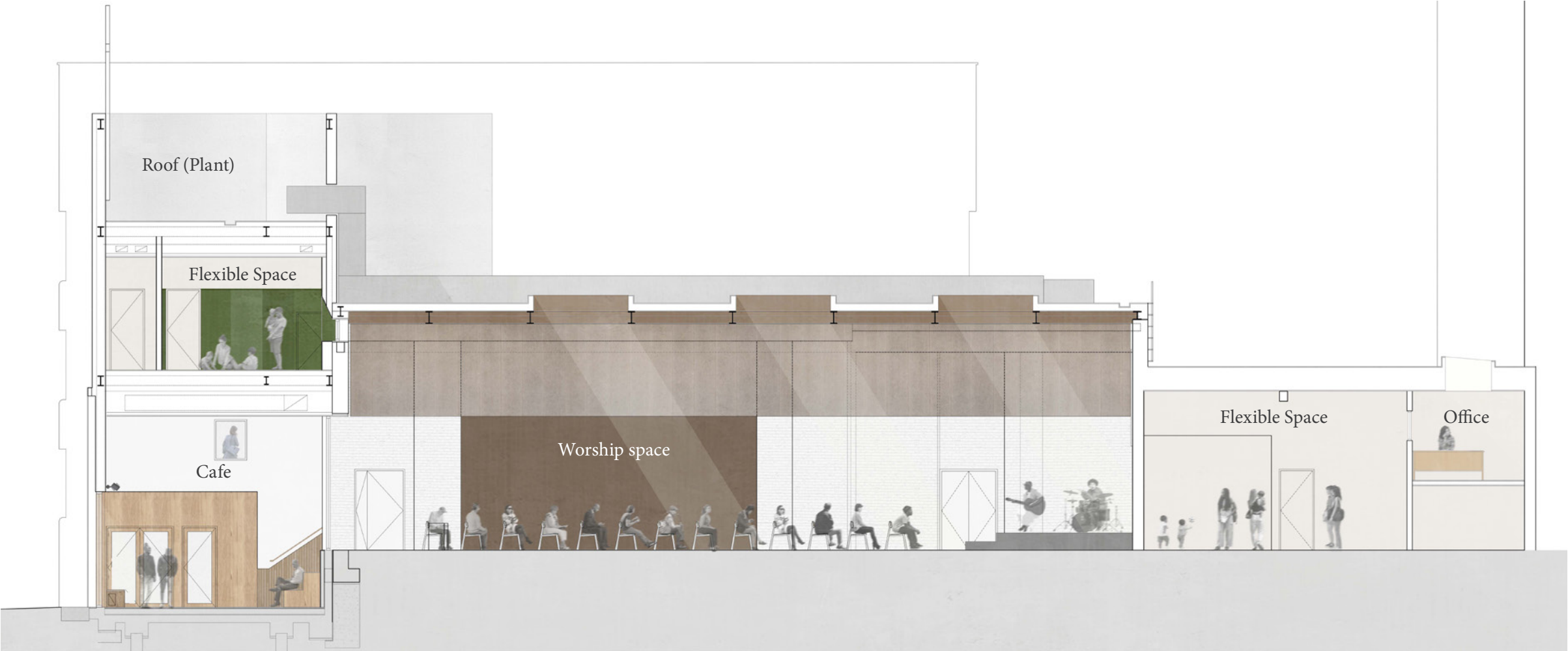
At first floor a large flexible space spans across the frontage. Adjacent to this is a modest recording studio, overlooking the Worship space with a good view to the stage. An accessible toilet is provided at this level and a buggy store which can also be used as an office.

In response to the Church's requirement to upgrade the building and move to a more efficient and sustainable strategy for heating and cooling significant plant is required, which is located at roof level.

The plant is screened by a full height wall to the perimeter of the new extension. Analysis shows that the optimum position to place photo-voltaic panels is towards the rear of the site, on the refurbished roof behind the Worship space.



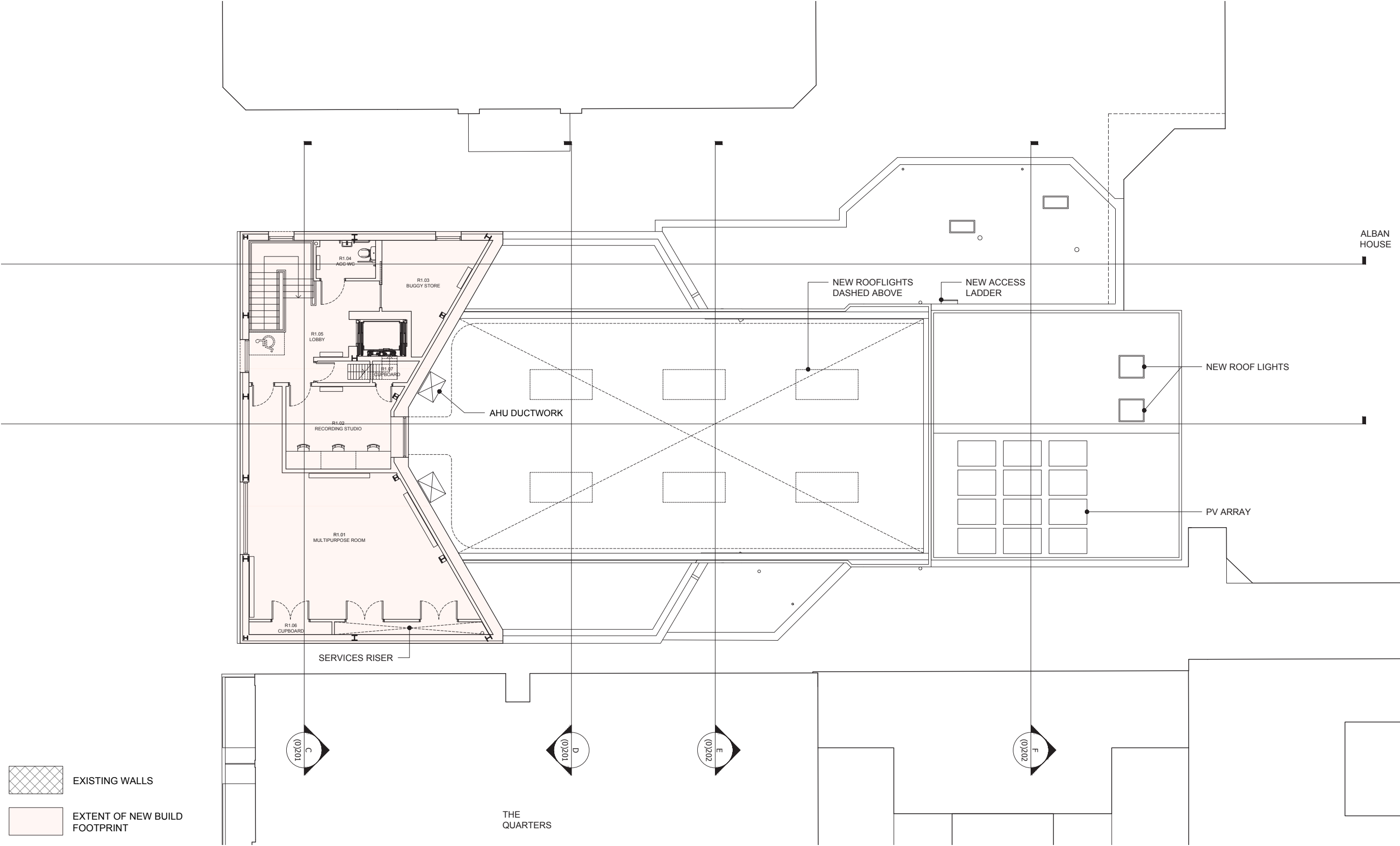
Proposed first floor flexible space



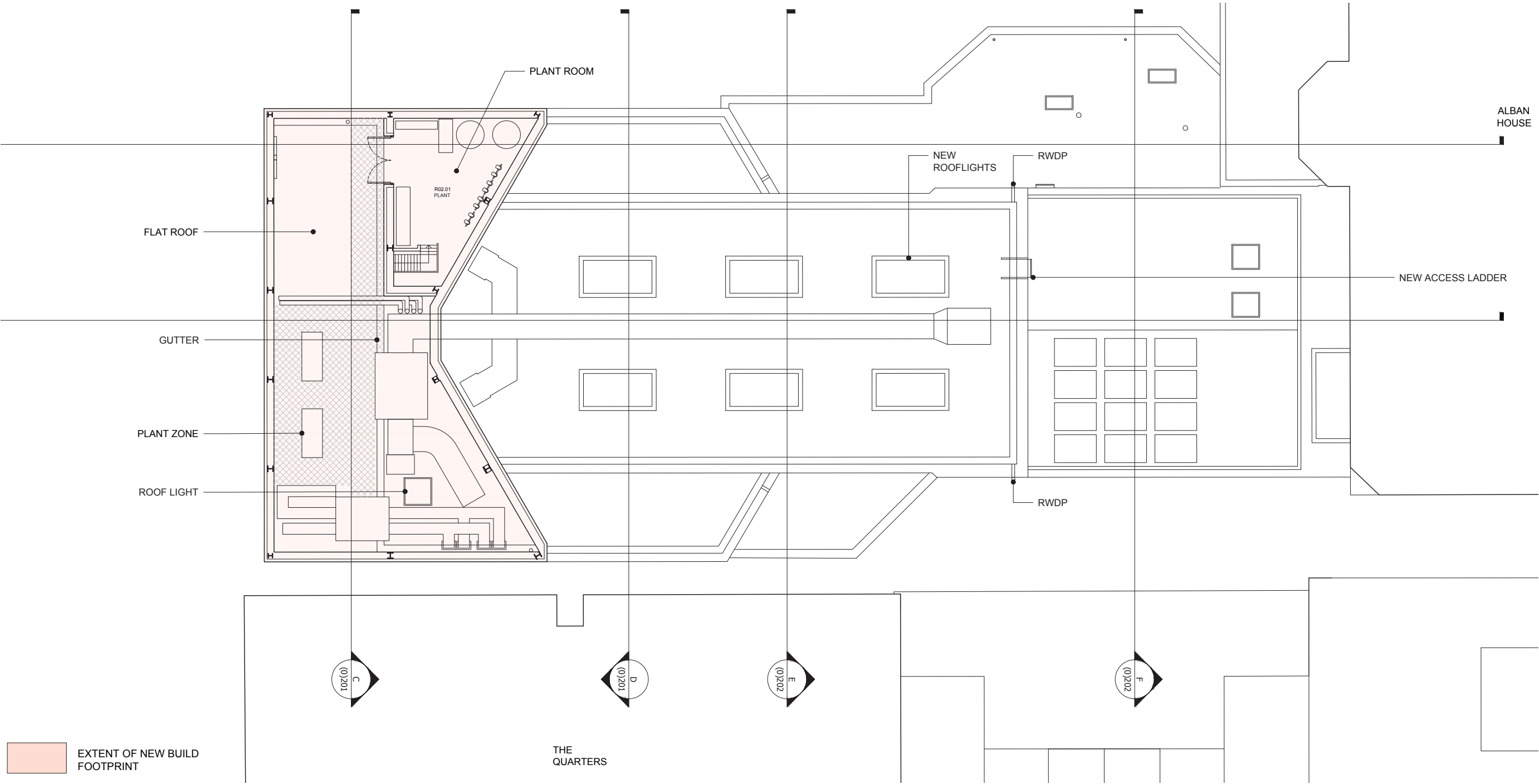
Long section through the proposal



FIRST FLOOR PLAN



ROOF PLAN





## ELEVATION & CHARACTER

Finchley Road is composed of mainly commercial and residential buildings, varying from four to seven storeys. Many of the late Victorian buildings have been replaced with larger muscular blocks, which are typically faced with masonry or render, punctuated and ordered by grids of windows.

The new elevation will respond to the brief to create a distinct, welcoming character. At three storeys high the new building is smaller than the majority of buildings nearby. However, the civic nature of the Church building provides a case for a confident (self referential) facade approach, so that the building is not visually overpowered by its larger neighbours.

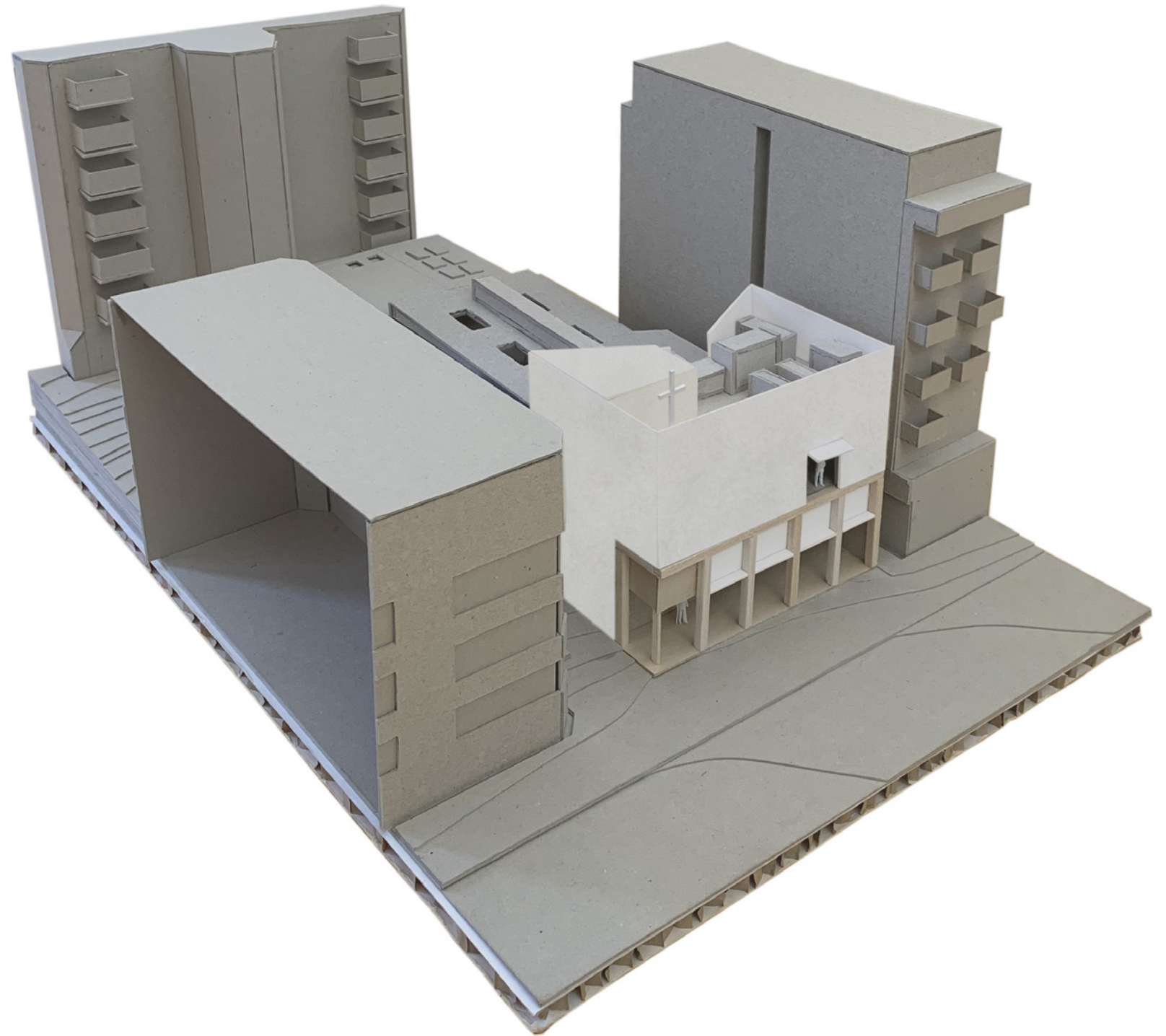
The framework to guide the elevational treatment is summarised below:

- New façade to distinguish the church from the neighbouring buildings
- Friendly, modern, playful, not traditionally religious
- Animated / visually porous ground floor
- Strong identifiable entrance – visible from up and down Finchley Road as well as the tube entrance/exit opposite
- Incorporation of signage including display of the cross

In response to the officer's comments in regards to the pre-application in January 2025, the design has been developed and modified, which is summarised below.

Design detail is further articulated on the following pages.

1. The existing cross is moved to the roof to emphasise the corner (and add 'interest').
2. The entrance has been set back slightly, the corner column is smaller than the others and clad in timber to read as the door frames. As well as the front glazing continuing down the side to appear more 'open'.
3. A bench has not been included at the front of the building as it may attract anti-social behaviour.
4. Instead, greenery has been included in the current proposal - in front of each glazed bay at the front and next to the side entrance.
5. Render on the facade has been chosen as the preferred finish for a number of reasons. The proposed acrylic based render is much more durable than the cementitious type. Furthermore the colour will be an off-white/buff (not pure white) and will be coated with a clear render protector to provide greater longevity and prevent algae/ pollution build-up.



The facade has a purity and simplicity that will distinguish it against the surrounding buildings, and embodies the spiritual essence of the Church's mission.

The majority of the facade at ground floor onto Finchley Road will be glazed- a welcoming and inviting public facing frontage- allowing views into the tall day-lit cafe space, with glimpses of the auditorium beyond. The entrance is located at the northern end of this animated strip, opposite the tube station.

Slender columns divide the bays to support and elevate the floors above. Within each bay planting offers a relief in the landscape.

Above ground floor the facade will receive a rendered finish. The facade has a scale-less character and will provide a calm and elegant crown to the busier ground floor. The render returns around either end of the extension, until the joint with the existing structure.

The render shall enclose the existing fabric to upgrade it thermally and provide a homogenous aesthetic.

The existing large cross is reinstated on the northern corner, signposting the entrance while visually balancing the picture window across the facade.

Below highlights how the proposal for Lighthouse London Church sits within Finchley Road's existing street scene.



Finchley Road elevation

Lighthouse London Church



ELEVATIONS



Street elevation



Side elevation

MATERIALITY

The choice of facing materials is a careful selection responding to civic location and use, and the Church's desired modern aesthetic.

The choice of render for the upper part of the elevation has been developed following a lengthy process of investigating options. The simplicity of the rendered facade at high level, with relatively small areas of fenestration (as compared to its neighbours) will provide a calm, distinct identity on the busy road. The acrylic base of the render means the facade will not need movement joints, will not crack and will require little or no maintenance.

The ground floor columns to the front are double height and faced with stone bricks to provide a more robust/ civic finish at this public level.

Both entrances will be clad in timber - a softer/ more humane material to enter through.

The existing facades of the building (to the rear) will also be sheathed with a new skin of insulated render. Not only will this tidy areas of dated and stained brickwork, it will significantly improve the thermal performance of the existing building and reduce its energy use.



Street facade composed in pale tones of render, stone and timber, with planting in front



Entrance corner model



Bay study model



## THE ENTRANCES

The two entrances, both of which are accessible, allow the Church the flexibility to provide spaces for different groups, ensuring security and safeguarding requirements, throughout the week.

The entrances are linked materially - both are clad in a natural timber which differentiates the entrances from the rest of the facade.

The front entrance to the Church is located to the north corner, opposite the tube station. The re-instated cross at roof level creates a strong, identifiable marker for the entrance, visible up and down Finchley Road. Slightly set back, the glazing at ground level returns around the corner towards the side entrance. The glazing opens the corner visually without compromising the internal lobby or creating an overhang which could encourage antisocial behaviour.

Soft planting is proposed to run along the facade leading to both entrances - providing an attractive base to the building.



Left: New entrance from Finchley Road



Left: Side entrance to Church



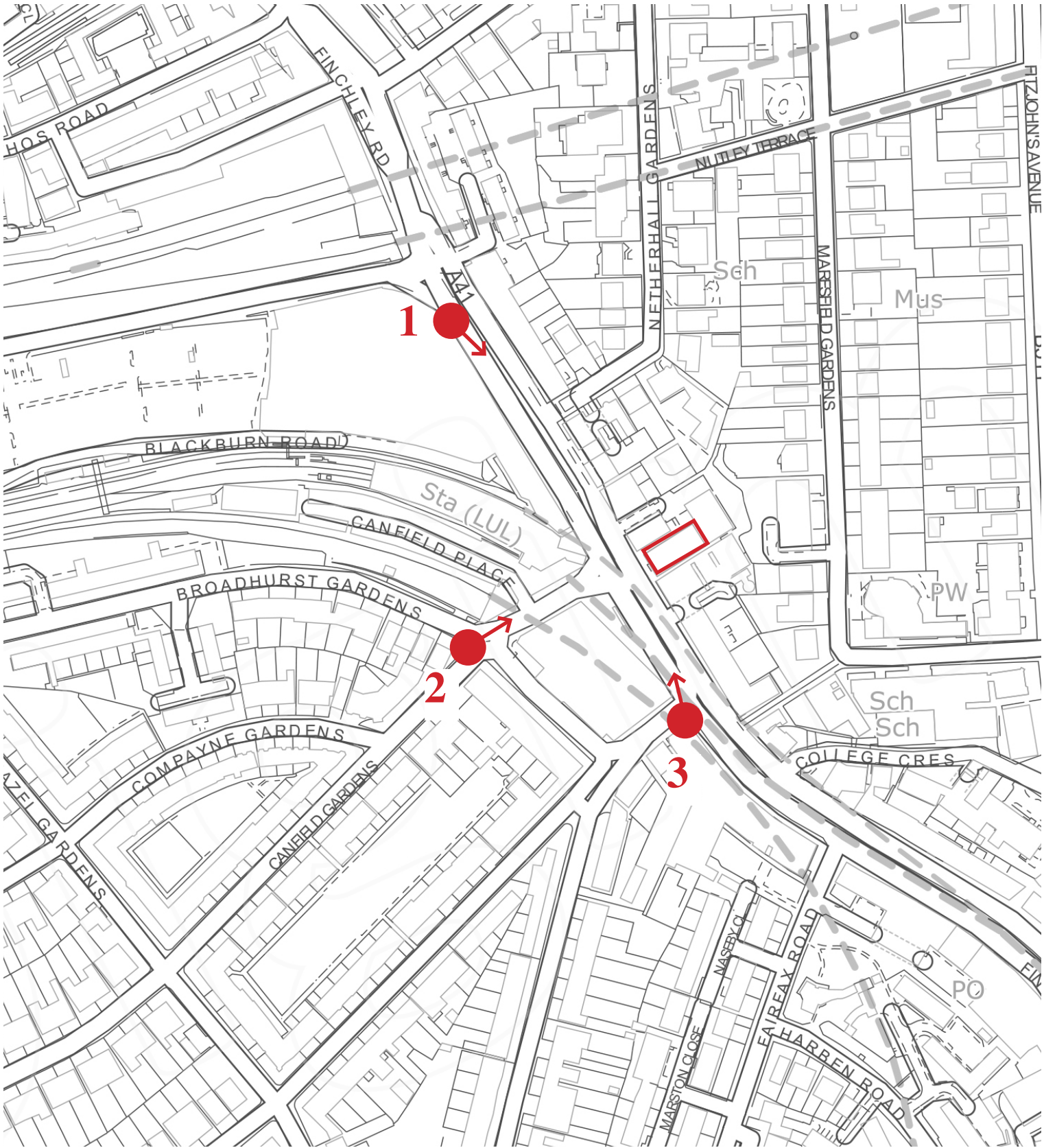
Detail model of north west corner

# TOWNSCAPE

Views have been constructed from three important viewpoints to illustrate the proposal in its townscape context.

The existing building currently has very little impact on the street scene - almost invisible against it's larger neighbours. The views indicate that the new proposal will have a stronger presence on the street, with a distinct identity.

The following pages show existing and proposed views.





## TOWNSCAPE VIEW 1

View along Finchley Road from North-west



Existing



Proposed



TOWNSCAPE VIEW 2

View from Canfield Gardens/ Broadhurst Gardens junction



Existing



Proposed



## TOWNSCAPE VIEW 3

View from Goldhurst Terrace/ Finchley Road junction



Existing



Proposed

# TECHNICAL INFORMATION

## Accessibility

The design will significantly improve the Church's ability to serve congregation members, visitors and staff who have a wider variety of access needs.

Creating a new entrance and café space at street level provides a welcoming and accessible entrance space. From this, the Worship space, half a level above, is accessed via a stair or adjacent platform lift, which also serves the first floor. Accessible WC's are located on both floors in the building and an accessible shower provided to the rear. All are assumed to be non-gendered.

In summary the new proposals will significantly improve the existing provision, allowing the building to be accessed by all through the following modifications;

- Level entrance from Finchley Road
- Powered entrance doors
- Step free access to all areas (apart from the rear office which will retain its stair access only).
- Provision of accessible & ambulant toilets and an accessible shower
- New serving counters to incorporate lowered serving areas.
- New ramp to stage area

## Transport

The site is very well served by existing public transport services, being located opposite Finchley Road underground station and a variety of bus services. This is also supported through the site's PTAL score of 6b, demonstrating that the site has an excellent level of accessibility to public transport.

## Parking

The Church has two car parking spaces, located to the north of the site. These spaces shall remain as existing.

## Cycles

There are no current cycle parking spaces on site but it is proposed that 6 new secure spaces are created in the small external space to the south/east.

(Cycle parking spaces are calculated using the London Plan requirements for minimum cycle parking standards (Table 10.2). Camden policy requires an additional 20% on top of London Plan requirements. Churches fall within category D1(other), and need 1 long stay spec for every FTE member of staff, as well as 1 short stay space for every 100m<sup>2</sup> GEA. GEA uplift for the new-build element is 132m<sup>2</sup>, which equates to 2 new spaces. One space is required for staff, and Camden's additional requirement brings this to a minimum of 4 new spaces in total).

It is noted that there are many short stay spaces nearby along Finchley Road and two large clusters of cycle stands on the opposite side of the road (totalling 42 spaces).

Further information on Transport is provided by RGP consultants.

## Refuse

Refuse collection will continue to operate as per the current situation with bins stored in the small external area to the south of the building, but with a new fenced enclosure. It is anticipated that the current capacity of 1 x 240 refuse & 1 x 360 recycling will be doubled. Bins are collected from the roadside weekly.

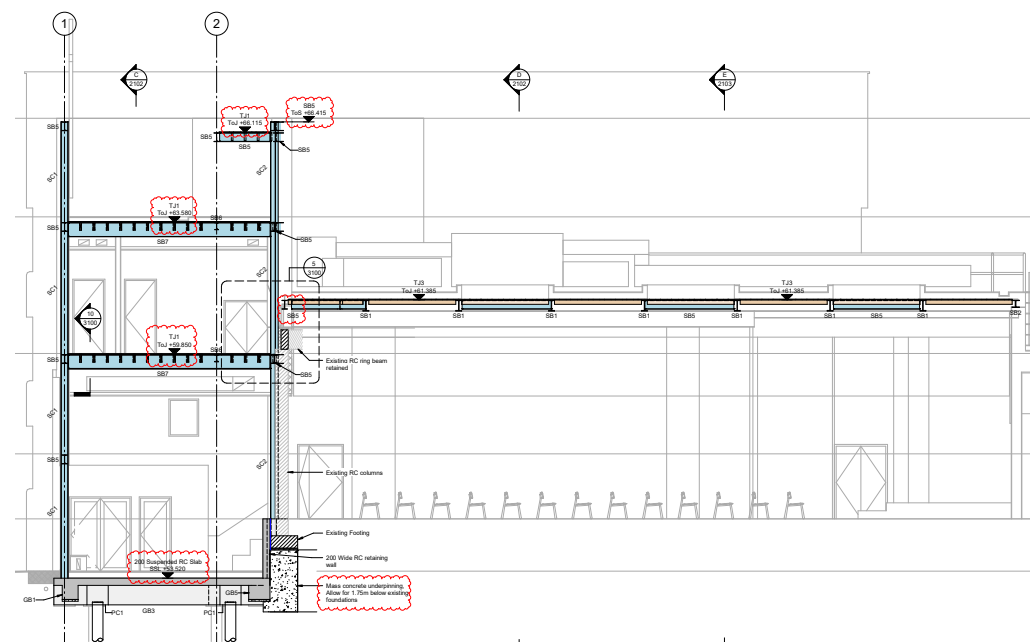


## Structure

The proposal requires the demolition of the existing building in front of the auditorium, the removal of the auditorium roof, and the partial removal of the hexagonal ring beam internally. The defining principle is to reuse elements of the existing structure wherever possible and to use existing elements to re-support new construction where this is efficient and economical.

The area in front of the existing auditorium will be rebuilt in all new construction, with two storeys and a third partial storey on the north side to form the plant room. The primary structure of the front building has been based on the use of a steel frame with timber joists used for the floors. This lightweight construction is inherently sustainable and relatively quick to erect - and will be readily understood by the contractor.

Foundations in this area are driven by the need to limit disturbance to the existing sewers. Small diameter piles are set behind the existing concrete strip footings with small pile caps and cantilever ground beams allowing the columns to be set forward over the existing footings.



Structural section through building - by Price&Myers Engineers

## Construction Logistics

Construction work will present obvious challenges on the tight urban site. The guiding construction principle is to re-use elements of the existing structure where this is efficient and economical. This is both practical, sustainable and will minimise disruption.

Work in the ground (foundations and new trenches) has been limited to the front of the site where the new extension is located. New build components - steel frame, timber infill and cladding have been selected to reduce construction time and also for their limited impact on the environment.

A basic Construction Management Plan, (CMP) has been developed by RGP.



Construction methodology & design were developed early and in tandem to respond to logistical challenges at the Roundhouse site (by Reed Watts).

Environmental

Although there are no formal targets or energy reporting requirements, all reasonable measures have been targeted to improve the energy efficiency and minimise carbon use.

The project’s key sustainability driver is the re-use of the existing building, which brings significant environmental benefits. However, although the majority of the building will be re-used it is envisaged that new services will be installed throughout. Constraints on the (new) electricity supply require all the existing building fabric to be upgraded to reduce heat loss. This will deliver benefits in terms of lower operational costs and energy consumption.

The majority of new plant will be located on the new roof, with screening to reduce the visual/ acoustic impact.

Key principles:

- Upgrade all existing fabric and new fabric to levels beyond Building Regs requirements
- Mechanical Ventilation with Heat Recovery throughout
- Low temperature radiators
- Good natural day lighting - balanced with need for solar control
- Enhanced acoustics to Worship space and recording studio
- Energy efficient lighting throughout
- Photo-voltaic panels on existing roof for on site energy production
- Re-use of existing drainage positions

Refer to Skelly & Couch report for the Energy / Sustainability report.

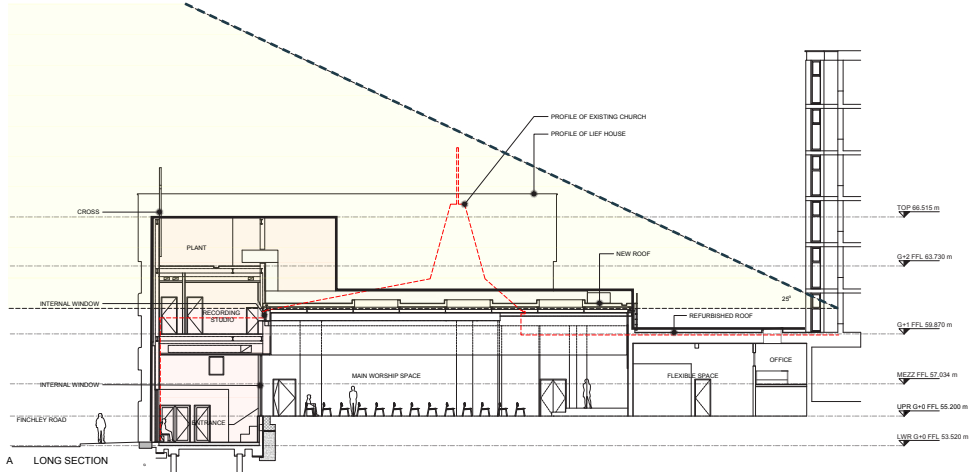
Acoustics

Quantum Acoustics are advising on all acoustic issues pertinent to planning, namely existing acoustic conditions, noise associated with plant and external breakout.

Good acoustic design principles have been incorporated at this stage to manage sound transfer through and between the building fabric but also to provide an appropriate acoustic environment within.

Daylight/Sunlight and Right to Light

The current proposals are significantly reduced compared to the previous proposal which obtained planning back in 2017. The modest scale of the new works, with the taller section toward the road, will not impact the residents of Alban House daylight and sunlight - as indicated on the diagram below.



Biodiversity

The proposal does not impact a priority habitat and affects less than 25 square metres of on-site habitat.

The two small planters which currently contain planting will be replaced by a strip of low level shrubs along the front facade and by the side entrance.



**Fire Safety**

The building is within purpose group 5 – Assembly and Recreation.

Means of warning and escape

- Means of warning. New detection and alarms are proposed throughout.
- Escape will be via new and existing escape routes to the north, south and west sides. Maximum occupancies & distances are shown on the drawings. Final exit door widths are also indicated.

The main Worship space has a maximum capacity of 250 (which is only expected at Christmas and Easter). Three alternative exits are provided. Notably the ‘main’ entrance to the worship space is not a fire escape route. This will allow the new front of the building to be used independently from the existing.

A fire curtain has been included to segregate the open public space at the front of the building from the main escape route from the first floor.

- A new evacuation lift will facilitate evacuation of people with access needs. The lift shaft is designed to be fire rated to 60 minutes.
- Compartmentation: 60 minutes compartmentation is shown. Fire doors are typically FD30S.

Internal Fire Spread (Linings)

- Areas of exposed timber linings, beyond the areas described in table 6.1, will be treated with a Class 0 flame retardant finish. This will include the timber lining and acoustic treatment in the café area and the worship space.

Internal Fire Spread (Structure)

- Stability of load bearing elements. Structural sizes as P&M drawings. Fire protection of steelwork will typically be done by encasement, with fire rated plasterboard.
- Proposed compartmentation/areas are indicated on the Fire Strategy drawings. Assumptions have been made as to the fire resistance of existing building fabric.
- Compartmentation: 60 minutes compartmentation is shown. Fire doors are typically FD30S.
- Fire-stopping details for service penetrations to be developed at Stage 4. At this stage, where ductwork passes through compartment walls, a fire and smoke damper is assumed.

External Fire Spread

- The existing building and the new extension will both be faced with non-combustible (mineral wool), insulated render.

Access and facilities for the fire service

- No changes are proposed to current access arrangements or facilities for the fire service (i.e directly from Finchley Road, where over 15% of the perimeter is accessible). A new fire alarm panel will be located next to the front entrance behind a glass fronted door and a repeater panel located in the side entrance.

## Mezzanine Floor





APPENDIX 1 - ARCHITECTURAL DRAWINGS ISSUE

2401 Lighthouse London Church  
Document Issue Sheet

Workstage			
Number	Title	Scale	Size
2401(0)001	Location Plan	1:1000	A1
2401(0)005	Existing Block Plan	1:500	A1
2401(0)006	Proposed Block Plan	1:500	A1
2401(0)010	Existing G+0 Plan	1:100	A1
2401(0)011	Existing G+1 Plan	1:100	A1
2401(0)012	Existing Roof Plan	1:100	A1
2401(0)020	Existing Sections 1	1:100	A1
2401(0)021	Existing Sections 2	1:100	A1
2401(0)030	Existing Elevations 1	1:100	A1
2401(0)031	Existing Elevations 2	1:100	A1
2401(0)100	Proposed G+0 Plan	1:100	A1
2401(0)101	Proposed Mezzanine Plan	1:100	A1
2401(0)102	Proposed G+1 Plan	1:100	A1
2401(0)103	Proposed G+2 and Roof Plan	1:100	A1
2401(0)200	Proposed Sections 1/3	1:100	A1
2401(0)201	Proposed Sections 2/3	1:100	A1
2401(0)202	Proposed Sections 3/3	1:100	A1
2401(0)300	Proposed Elevations 1/2	1:100	A1
2401(0)301	Proposed Elevations 2/2	1:100	A1
	Stage Report/ DAS		
	Area Schedule Summary		
	Proposed Internal Areas		

Distribution: Lighthouse London  
Storey Project Ltd  
Stockdale  
Skelly & Couch  
Price & Myers  
Lichfields

Date:	20	07	03	23	05	16	30	23	14	21	17	19
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