

# 100 Avenue Road

## Construction Management Plan

February 2025

REGAL

Dear Maxim Lyne,

We are committed to delivering the development with the utmost sensitivity and in strict accordance with the previously approved Construction Management Plan (CMP) by Essential Living.

Key principles already agreed within the CMP include that Avenue Road will serve as the primary route for all construction vehicles, with no access permitted from alternative routes. Material deliveries will be managed via two methods: a designated 'pit-lane' occupying the inner lane of the A41 or a controlled route through Finchley Road, Avenue Road, and Adelaide Road. The southern exit will require a temporary license from Camden Parks to relocate existing structures, as previously agreed. The basement access from Eton Avenue will remain open exclusively for small-scale deliveries.

To ensure safety, construction vehicle movements will be restricted during peak school drop-off and pick-up times. Delivery schedules will be coordinated to prevent vehicle stacking, and all waiting lorries must switch off engines. Traffic marshals will oversee all vehicle movements to maintain safety and efficiency.

Throughout the construction period, air quality monitors will be installed at key locations, and noise, dust, and pollution levels will be continuously assessed. Noisy works will be limited to Monday–Friday, with efforts to avoid them on Saturdays. Quiet periods will be scheduled along the northern side of the site to align with Hampstead Theatre's performance schedule. Monitoring results will be published on the project's website for transparency.

Regal Avenue Road Limited will continue to engage with local residents, business, groups and Ward Councillors throughout the construction period.



# Construction Management Plan

pro forma

100 Avenue Road

By Regal

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- W. Response to Queries raised at Committee 19.07.18 **no change**
- X. Planning Submitted Draft CMP (2014) **no change**

## Revisions & additional material

Please list all iterations here:

Date	Version	Produced by
03.08.17	1	Essential Living
21.09.17	2	Essential Living

27.09.17	3	Essential Living – further updates from team
09.10.17	4	Essential Living – version published for public consultation
04.12.2017	5	Essential Living – further version published following public meeting and first construction working group
14.03.2018	6	Essential Living
26.04.2018	7	Essential Living
18.07.2018	8	Essential Living
29.08.2018	9	Essential Living – further updates and appendices responding to 19 <sup>th</sup> July Planning Committee
22.10.18	Approved v.9	Essential Living – Approved in November Committee
04.01.19	10	Essential Living – Addressing PAP
22.01.19	11	Essential Living
26.07.24	12	Regal
20.08.24	13	Regal
21.10.24	14	Regal
14.11.24	15	Regal
10.02.25	16	Regal

### Additional sheets

Please note – the review process will be quicker if these are submitted as Word documents or searchable PDFs.

Date	Version	Produced by

# Introduction

The purpose of the **Construction Management Plan (CMP)** is to help developers to minimise construction impacts, and relates to both on site activity and the transport arrangements for vehicles servicing the site.

It is intended to be a live document whereby different stages will be completed and submitted for application as the development progresses.

The completed and signed CMP must address the way in which any impacts associated with the proposed works, and any **cumulative impacts of other nearby construction sites**, will be mitigated and managed. The level of detail required in a CMP will depend on the scale and kind of development. Further policy guidance is set out in Camden Planning Guidance ([CPG](#) [6: Amenity](#) and [CPG](#) [8: Planning Obligations](#)).

This CMP follows the best practice guidelines as described in [Transport for London's](#) (TfL's Standard for [Construction Logistics and Community Safety](#) (**CLOCS**) scheme) and [Camden's Minimum Requirements for Building Construction](#) (**CMRBC**).

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The approved contents of this CMP must be complied with unless otherwise agreed with the Council in writing. The project manager shall work with the Council to review this CMP if problems arise during construction. Any future revised plan must also be approved by the Council and complied with thereafter.

It should be noted that any agreed CMP does not prejudice or override the need to obtain any separate consents or approvals such as road closures or hoarding licences.

If your scheme involves any demolition, you need to make an application to the Council's Building Control Service. Please complete the "[Demolition Notice](#)."

Please complete the questions below with additional sheets, drawings and plans as required. The boxes will expand to accommodate the information provided, so please provide as much information as is necessary. It is preferable if this document, and all additional documents, are completed electronically and submitted as Word files to allow comments to be easily documented. These should be clearly referenced/linked to from the CMP. Please only provide the information requested that is relevant to a particular section.

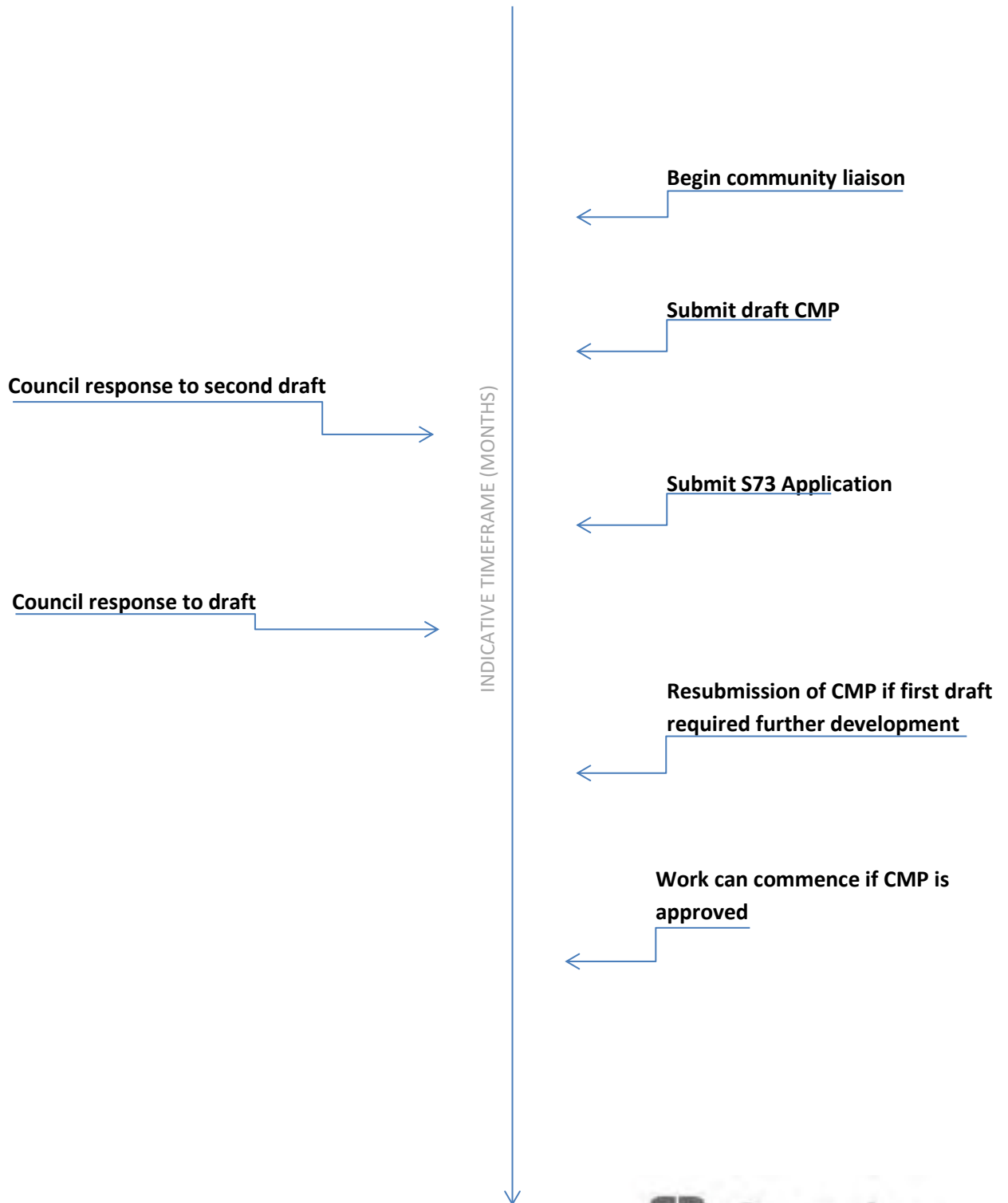
(Note the term 'vehicles' used in this document refers to all vehicles associated with the implementation of the development, e.g. demolition, site clearance, delivery of plant & materials, construction etc.)

Revisions to this document may take place periodically.

# Timeframe

## COUNCIL ACTIONS

## DEVELOPER ACTIONS





# Contact

1. Please provide the full postal address of the site and the planning reference relating to the construction works.

Address: 100 Avenue Road, London, NW3 3HF

Planning reference number to which the CMP applies: TBC

2. Please provide contact details for the person responsible for submitting the CMP.

Name: Gabriele Tomassini

Address: 4-5 Coleridge Gardens, London, NW6 3QH

Email: gabriele.tomassini@regal.co.uk

Phone: 020 7328 7171

3. Please provide full contact details of the site project manager responsible for day-to-day management of the works and dealing with any complaints from local residents and businesses.

Name: Regal Project Manager to be assigned to the project

Address: 4-5 Coleridge Gardens, London, United Kingdom, NW6 3QH

Email: TBC

Phone: TBC

4. Please provide full contact details of the person responsible for community liaison and dealing with any complaints from local residents and businesses if different from question 3. In the case of the Community Investment Programme (CIP), please provide the contact details of the Camden officer responsible.

Name: Regal Community Liaison Manager to be assigned to the project

Address: 4-5 Coleridge Gardens, London, United Kingdom, NW6 3QH

Email: TBC

Phone: TBC

5. Please provide full contact details including the address where the main contractor accepts receipt of legal documents for the person responsible for the implementation of the CMP.

Name: Regal One London Construction Ltd

Address: 4-5 Coleridge Gardens, London, United Kingdom, NW6 3QH

Email: TBC

Phone: TBC

# Site

6. Please provide a site location plan and a brief description of the site, surrounding area and development proposals for which the CMP applies.

## Existing Site:



The site is located immediately off the A41 and College Crescent, in the Swiss Cottage area of the London borough of Camden. The site currently has completed the piling and the basement box. Construction is not in progress.

The site has several existing uses immediately adjacent to it; to the East is an area of Camden Park Lands and Hampstead Theatre. To the south is also a small strip of Camden Park Land followed by Swiss Cottage Leisure Centre. To the west is a major road interchange followed by several commercial units. To the north is the entrance to Swiss Cottage Underground Station followed by The Royal Central School of Speech and Drama. Going further afield to the East and West are mostly residential areas, with commercial units following the main transport high North and South.

## Site location & Surrounding Area:

### Proposal

Demolition of existing building (completed) and redevelopment (in progress) of a 26 storey building and a part 8, part 6, storey building comprising a total of 236 residential units (class C3) and up to 1,188 sqm of flexible retail/financial or professional or café/restaurant floorspace (classes A1/A2/A3) inclusive of part sui generis floorspace for potential new London Underground station access fronting Avenue Road and up to 1,372sqm for community use (class D1) with associated works including enlargement of existing basement level to contain disabled car parking spaces and cycle parking, landscaping and access improvements.

### Proposed Visual:



7. Please provide a very brief description of the construction works including the size and nature of the development and details of the main issues and challenges (e.g. narrow streets, close proximity to residential dwellings etc).

Construction works proposed for the above description of development:

Demolition began Q4 2018 and is now complete. Piling and Basement Box works were also completed in Q3 2020.

Issues and challenges for the construction works will include:

- Theatre next door which may necessitate restricted working hours for noisy works
- The Royal Central School of Speech and Drama located to the north of the site
- Residential access to north east
- Busy red route to south east
- Public Park to the East and South
- Creating multiple safe access points to the site to minimise the impact on the public
- Temporary Bus stop relocation
- Signals relocation
- Working close to the Swiss Cottage underground station

8. Please identify the nearest potential receptors (dwellings, business, etc.) likely to be affected by the activities on site (i.e. noise, vibration, dust, fumes, lighting etc.).

<b>ID</b>	<b>Location</b>	<b>X</b>	<b>Y</b>	<b>Distance</b>	<b>Risk Theme</b>
1	17-24 Finchley Road	526632	184222	107m	Site Traffic
2	115-121 Finchley Road	526632	184259	87m	Site Traffic
3	Cresta House	526623	184299	89m	Site Traffic
4	139 Finchley Road	526616	184348	111m	Site Traffic
5	Northways Parade	526642	184403	130m	Site Traffic
6	Royal Central School of Speech & Drama (1)	526687	184363	74m	Site Traffic, Noise, Dust
7	Royal Central School of Speech & Drama (2)	526710	184386	94m	Site Traffic, Noise, Dust
8	Embassy Theatre	526725	184390	98m	Site Traffic, Noise, Dust
9	Market	526716	184364	72m	Site Traffic, Noise, Dust
10	Hampstead Theatre - west boundary	526721	184341	49m	Site Traffic, Noise, Dust
11	Hampstead Theatre - south boundary	526750	184336	57m	Site Traffic, Noise, Dust
12	Underground Station - Eton Avenue entrance	526686	184332	47m	Site Traffic, Noise, Dust
13	Underground Station - Avenue Road entrance	526701	184287	12m	Site Traffic, Noise, Dust
14	Temporary bus stop lay-by	526670	184323	52m	Site Traffic
15	Ye Old Swiss Cottage pub	526685	184248	52m	Site Traffic
16	Odeon Cinema	526699	184218	76m	Site Traffic
17	Central library	526761	184209	96m	Site Traffic
18	Leisure centre	526782	184220	101m	Site Traffic
19	Tennis courts	526777	184248	79m	Location
20	Community centre	526828	184265	119m	Site Traffic
21	Park (centre of)	526772	184296	60m	Location
22	21A Winchester Road	526815	184292	103m	Site Traffic
23	23 Winchester Road	526810	184302	99m	Site Traffic
24	25 Winchester Road	526807	184308	96m	Site Traffic
25	27-29 Winchester Road	526803	184313	94m	Site Traffic
26	31 Winchester Road	526800	184320	92m	Site Traffic
27	33 Winchester Road	526796	184326	91m	Site Traffic
28	35 Winchester Road	526793	184332	90m	Site Traffic
29	Mora Burnet House	526778	184357	92m	Site Traffic
30	Swiss Cottage School	526889	183997	344m	Site Traffic
31	UCL Academy	526865	184120	231m	Site Traffic

Shown in Appendix P

9. Please provide a scaled plan detailing the local highway network layout in the vicinity of the site. This should include details of on-street parking bay locations, cycle lanes, footway extents and proposed site access locations.

Due to the installation of a required “pit lane” to aid deliveries and the southern access point onto site, bus stop D on Avenue Road needs to be relocated for the duration of the construction of the project on Adelaide Road. This bus stop will be of a similar size to the current stop.

To the north of the site is an access point for deliveries to the basement via Eton Avenue using the existing car park ramp. The existing car park entrance will still be utilised for the remainder of the project as this is the only entrance to the basement.

The second entrance from Avenue Road allows vehicles to exit the project from the southern section of the site.

A third access point has been installed to the north of the LUL entrance on the A41. The crossover and any associated changes within Avenue Road will be implemented prior to any works commencing on site. This access point will allow the safe access of vehicles to site from the A41, negating higher numbers of vehicle movements that would otherwise be necessary to the northern access point.

A full set of drawings detailing the existing and proposed highway network layout including the “pit lane” and bus stop relocation are attached at Appendix A.

10. Please provide the proposed start and end dates for each phase of construction as well as an overall programme timescale.

A draft construction programme is provided at Appendix B. Please refer to that for full details.

The proposed construction start date is October 2025 and the construction work is anticipated to last approximately 135 weeks.

Remobilisation & Enabling	5ew 6ed	01/10/2025	11/11/2025
Basement	19w	12/11/2025	09/04/2026
Blocks B	98ew 4eh	17/04/2026	03/03/2028
Blocks A	112ew 3ed	10/04/2026	05/06/2028

11. Please confirm the standard working hours for the site, noting that the standard working hours for construction sites in Camden are as follows:

- 8.00am to 6pm on Monday to Friday
- 8.00am to 1.00pm on Saturdays
- No working on Sundays or Public Holidays

12. Please indicate if any changes to services are proposed to be carried out that would be linked to the site during the works (i.e. connections to public utilities and/or statutory undertakers' plant). Larger developments may require new utility services. If so, a strategy and programme for coordinating the connection of services will be required. If new utility services are required, please confirm which utility companies have been contacted (e.g. Thames Water, National Grid, EDF Energy, BT etc.) You must explore options for the utility companies to share the same excavations and traffic management proposals. Please supply details of your discussions.

The existing utilities impacted by the works have been assessed by Regal along with an assessment of what needs to be diverted and what new supplies are needed. Set out in Appendix C is a schedule of proposed utilities works and a plan of the existing utilities around the site.



# Community Liaison

A neighbourhood consultation process must have been undertaken prior to submission of the CMP first draft. This consultation must relate to construction impacts, and should take place following the granting of planning permission in the lead up to the submission of the CMP. A consultation process specifically relating to construction impacts must take place regardless of any prior consultations relating to planning matters. This consultation must include all of those individuals that stand to be affected by the proposed construction works. These individuals should be provided with a copy of the draft CMP, or a link to an online document. They should be given adequate time with which to respond to the draft CMP, and any subsequent amended drafts. Contact details which include a phone number and email address of the site manager should also be provided.

Significant time savings can be made by running an effective neighbourhood consultation process. This must be undertaken in the spirit of cooperation rather than one that is dictatorial and unsympathetic to the wellbeing of local residents and businesses.

These are most effective when initiated as early as possible and conducted in a manner that involves the local community. Involving locals in the discussion and decision making process helps with their understanding of what is being proposed in terms of the development process. **The consultation and discussion process should have already started, with the results incorporated into the CMP first draft submitted to the Council for discussion and sign off.** This communication should then be ongoing during the works, with neighbours and any community liaison groups being regularly updated with programmed works and any changes that may occur due to unforeseen circumstances through newsletters, emails and meetings.

Please note that for larger sites, details of a construction working group may be required as a separate S106 obligation. If this is necessary, it will be set out in the S106 Agreement as a separate requirement on the developer.

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## Cumulative impact

Sites located within high concentrations of construction activity that will attract large numbers of vehicle movements and/or generate significant sustained noise levels should consider establishing contact with other sites in the vicinity in order to manage these impacts.

**The Council can advise on this if necessary.**

### 13. Consultation

The Council expects meaningful consultation. For large sites, this may mean two or more meetings with local residents **prior to submission of the first draft CMP**. Please ensure that any changes to parking and loading on the public highway are reflected in the consultation. Please agree highways set up plans in advance with Camden if there is any uncertainty with this.

Evidence of who was consulted, how the consultation was conducted and a summary of the comments received in response to the consultation should be included. Details of meetings including minutes, lists of attendees etc. should be appended.

In response to the comments received, the CMP should then be amended where appropriate and, where not appropriate, a reason given. The revised CMP should also include a list of all the comments received. Developers are advised to check proposed approaches to consultation with the Council before carrying them out. If your site is on the boundary between boroughs then we would recommend contacting the relevant neighbouring planning authority.

Please provide details of consultation of the draft CMP with local residents, businesses, local groups (e.g. residents/tenants and business associations) and Ward Councillors.

#### Consultation: Historical, Current and Future

Table below outlines all consultation meetings undertaken by the previous developer (up to 2020) and by Regal Avenue Road Limited from 2024 onwards (meeting minutes and consultation records are included at appendix D along with the Consultation Statement from Polity where the outcome of the public consultation has been recorded and attached always at appendix D).

Consultation is ongoing and will continue through the S73 planning process and into the construction phase.

Party Consulted	Date	Topic	Outcome
LUL	17/06/2014	Location of basement in proximity with LUL stair case	EL to progress planning drawings expected Q3 2015
LUL	03/07/2014	Movement monitoring of LUL tunnels	URS to progress movement monitoring and condition surveys

LUL	06/08/2014	Monitoring Tender Review	URS recommending letter of intent from LUL to maintain programme
LUL	29/04/2019	List of various points for consideration when working close to LUL asset	LUL confirmed the following: <ul style="list-style-type: none"> <li>- No oversailing of crane over station entrance</li> <li>- Traffic management on Avenue road needs to be rigorous</li> <li>- Concept design statement needs to be completed by Mace</li> <li>- Monitoring Action Plan to be completed by Mace</li> </ul> Load assessment required to be completed of LUL asset
LUL	06/09/2019	License to carry out works for protection deck	LUL issued approved license
LUL	06/09/2019	Concept Design Statement	LUL issued approval following review of JFH CDS
LUL	14/09/2019	Party Wall Award	Agreed and signed by LUL Party Wall Surveyors
TfL	05/06/2017	CS11(Programme)	TfL to review EL development with the CS11 programme and phasing.
TfL	07/06/2017	CS11 (site Logistics)	EL to advance site logistic plans at SPA's for further review with TfL
TfL	26/09/2017	CMP and S278	TfL happy with the logistics plan in principal. To be agreed following further input. TfL proposed a working group of all construction projects in the vicinity to ensure all potential issues are addressed throughout the project.
TfL	29/07/2018	CMP and CS11	Confirmation to proposed CMP submitted by EL and acknowledgement that the document is a live document and

			may need to change depending upon CS11
TfL	22/08/2018	CMP	Confirmation that TfL will speak at the planning committee in support of the CMP.
TfL	13/08/2018	CS11	Confirmation that CS11 JR was successful
TfL	03/04/2019	Pit Lane	Confirmation that EL can proceed with the insertion of the Pit Lane
TfL	May to Sept 2019	Pit Lane	Discussions and detailed design with TfL regarding the pit lane
TfL	18/09/2019	Pit Lane	Issue of the permit to insert the Pit Lane
Camden Parks	06/10/2017	Impact of the project on the adjacent park	EL issue logistics plans and Camden Park to seek advice from legal team in removal of trees and artwork
Camden Parks	11/10/2018	Parkland	Public Consultation event held regarding the use of the parkland for the construction of 100 Avenue Road
Camden Parks	02/10/2019	Parkland	Completion of the parkland licence
Meeting with HS2	16/10/2017	To understand any interfaces with the HS2 scheme and the Theatre Square scheme	The outcome of the meeting was that the timing of the HS2 works would not impact the Theatre Square scheme and vice versa
HS2 and Chalcott Estate	Monthly	To understand any interfaces with the HS2 scheme, Chalcott works and the Theatre Square scheme	Monthly meetings to discuss work programme and vehicle movements
Public Consultation Meeting at Harris School	19/10/2017	Public Consultation meeting to present the CMP	Feedback was received and has been collated into the report by Polity attached at appendix D
Meeting with Camden Environmental Health Officer	10/11/2017	Seek feedback and comment from the Environmental Health Officer	Comments have been incorporated into v.5 of the CMP

First Working Party Meeting with residents	15/11/2017		The meeting was attended by over 50 people and did not constitute a formal working party meeting. Further feedback was recorded which has been collated into the Polity report attached and appendix D
Further public consultation & presentation of v.5 of the CMP	29/11/2017	Presentation of updated details forming v.5 of the CMP	The meeting was attended by c.18 members of the public and local groups. Record in the Polity report attached at appendix D  The key issue that was being raised by the local groups was the traffic generation and the use of Winchester Road. We have reviewed the use of Winchester Road and will cap the total trips per day to 14 and will stop using Winchester Road by month 29 of the project
Construction Working Groups	Dec 2018 – March 2020	Monthly CWG meetings	Regular meetings with representatives from resident groups and local businesses to discuss stages of work that have been undertaken and that will be undertaken.
Camden Open Spaces	08/08/2024	Presentation of landscape layout	Discussion around integration of landscape design within the wider Camden park regeneration scheme. Mention of re negotiation of the parkland license to use part of the park for the south site gate
St Johns Wood Square Project Site	September '24	Site traffic coordination	Introduction and exchange of logistics plans. Agreement that the two sites do not impact each other but that there needs to be coordination when large deliveries are expected.

LUL	October '24	Licenses to carry out work	Introduction to the LUL asset management officer and discussion of next steps towards securing licenses to protect the two LU entrances.
Swiss Cottage Market Consultation	01/10/2024	Deborah Carpenter (Market Development Manager) and Joze Stivan (Senior Transport Planner)	Discussion around market operating hours and coordination with residential deliveries once construction is complete.  Regal confirmed that extant CMP guidelines will be followed with construction vehicles servicing the site from Avenue Rd.

#### 14. Construction Working Group

For particularly sensitive/contentious sites, or sites located in areas where there are high levels of construction activity, it may be necessary to set up a construction working group.

If so, please provide details of the group that will be set up, the contact details of the person responsible for community liaison and how this will be advertised to the local community, and how the community will be updated on the upcoming works i.e. in the form of a newsletter/letter drop, or weekly drop in sessions for residents.

From the outset of their discussions with Camden Officers in 2017, Essential Living carefully reviewed the Council's Construction Management (CMP) Pro Forma and its Community liaison guidance: guidance for developers and contractors. This guidance has continued to be reviewed during the various phases of work and when considering amendments to the CMP.

As stated in the Camden CMP Proforma, the CMP is intended to be a live document whereby different stages will be completed and submitted for application as the development progresses.

Following discussions with Officers it was agreed that a public consultation event to allow the public to comment on the previous proposed amendment to the CMP is appropriate. In addition, Essential Living did add an extra consultation event prior to the submission to allow a further opportunity for the public to input to the document.

Invites were sent to all parties who previously commented on the CMP via e-mail (200+) along with a mailshot to the area previously agreed (see appendix D of the original CMP v11) with officers.

Essential Living had been running monthly Construction Working Group meetings (CWGs) since December 2018, when demolition commenced. The membership of the Group was agreed with Camden officers, with on-going opportunities to make amendments to membership at the request of ward councillors.

The group is made up of:

**Constituted Residents' Groups –**

- Belsize Residents Association (Prabhat Vaze)
- Save Swiss Cottage (Janine Sachs)
- Cresta House Residents Association (Edie Raff)
- Winchester Road Residents Association (Elaine Chambers)
- Combined Residents Associations of South Hampstead (CRASH – Peter Symonds)
- Taplow Tenants & Residents Association (no named representative)
- Visage Building (Alex Andrews)
- Resident of Eton Court (Kitty Balint)
- Belsize Village Residents and Traders' Association (James Brown)
- Belsize Conservation Area Advisory Committee (David Thomas)
- Hampstead District Management Committee (Gianni Franchi)
- The Belsize Society
- Primrose Hill Court Tenants and Residents Association
- Netherhall Neighbourhood Association (Jenny White)
- Save Swiss Cottage Action Group (David Reed)

**Business/Commercial interests/Cultural/Educational –**

- Hampstead Theatre
- Swiss Cottage Market
- Central School of Speech and Drama
- The Winch
- Swiss Cottage Community Centre
- Swiss Cottage Central Library
- Representative of businesses on Winchester Road
- Swiss Cottage Surgery
- Camden Market

## Local Councillors –

- Councillor Ajok Athian (Labour, Primrose Hill)
- Councillor Anna Burrage (Labour, Primrose Hill)
- Councillor Matt Cooper (Labour, Primrose Hill)
- Councillor Judy Dixey (Liberal Democrat, Belsize)
- Councillor Matthew Kirk (Liberal Democrat, Belsize)
- Councillor Tom Simon (Liberal Democrat, Belsize)
- Councillor Nina De Ayala Parker (Labour, South Hampstead)
- Councillor Tommy Gale (Labour, South Hampstead)
- Councillor Izzy Lenga (Labour, South Hampstead)

In addition to the CWG, EL produce bi-weekly dashboards that are distributed to list of registered interested parties including the attendees of the CWG meetings and ward Councillors. Key elements here include:

- Construction updates including work undertaken and upcoming
- Register of vehicle movements comparing actual with permitted
- Forecast vehicle movements
- Environmental summaries
- Website with regular updates and links to live monitoring
- Recording complaints in a register
- Notice board on the external boundary of the site updated on a by monthly basis

## 15. Schemes

Please provide details of your Considerate Constructors Scheme (CCS) registration. Please note that Camden requires [CCS site registration](#) for the full duration of your project including additional [CLOCS visits](#) for the full duration of your project. Please provide the CCS site ID number that is specific to the above site. A company registration will not be accepted, the site must be registered with CCS.

Be advised that Camden is a Client Partner with the Considerate Constructors Scheme and has access to all CCS inspection and CLOCS monitoring reports undertaken by CCS.

Contractors will also be required to follow the [Guide for Contractors Working in Camden](#). Please confirm that you have read and understood this, and that you agree to abide by it.



The scheme will be registered with CCS prior to any works commencing on site. Regal One London Construction will adhere to all CCS codes and policies and will aim for an outstanding score.

## 16. Neighbouring sites

Please provide a plan of existing or anticipated construction sites in the local area and please state how your CMP takes into consideration and mitigates the cumulative impacts of construction in the vicinity of the site. The council can advise on this if necessary.

- HS2 construction at Cardington Street
- Chalcott Estate recladding and refurbishment
- St Johns Wood Barracks
- O2 Finchley Road
- The Royal Central School of Speech and Drama

Essential Living had undertaken regular consultation with both the managers of the refurbishment work of the Chalcott Estate and HS2 to understand the respective programme of works of the projects.

The Chalcott Estate will be serviced via both Fellows Road and Winchester Road, whilst HS2 will be utilising Adelaide Road. The introduction of the additional access point for the 100 Avenue scheme and removal of all non-planned movements along Winchester Road will substantially reduce the potential impact of vehicle movements with the other projects planned in the area

St Johns Wood Barracks main access route is down Finchley Road, turning onto Queens Terrace and into site via gates off Queens Terrace. A second route is Finchley Road turning down Acacia Avenue and then left up Ordnance Hill and into site via gates on Ordnance Hill. Exit routes are generally out Ordnance Hill up onto St Johns Wood Park and onto Adelaide Road.

# Transport

**This section must be completed in conjunction with your principal contractor. If one is not yet assigned, please leave the relevant sections blank until such time when one has been appointed.**

Camden is a CLOCS Champion, and is committed to maximising road safety for Vulnerable Road Users (VRUs) as well as minimising negative environmental impacts created by motorised road traffic. As such, all vehicles and their drivers servicing construction sites within the borough are bound by the conditions laid out in the CLOCS Standard.

This section requires details of the way in which you intend to manage traffic servicing your site, including your road safety obligations with regard to VRU safety. It is your responsibility to ensure that your principal contractor is fully compliant with the terms laid out in the CLOCS Standard. It is your principal contractor's responsibility to ensure that all contractors and sub-contractors attending site are compliant with the terms laid out in the CLOCS Standard.

Checks of the proposed measures will be carried out by CCS monitors as part of your CLOCS monitoring visits through CCS and possibly council officers, to ensure compliance. Please refer to the CLOCS Standard when completing this section.

Please contact [CLOCS@camden.gov.uk](mailto:CLOCS@camden.gov.uk) for further advice or guidance on any aspect of this section.

Please note that this section may also be referred to as a Construction Logistics Plan in the context of the CLOCS Standard.

## CLOCS Contractual Considerations

17. Name of Principal contractor:

Regal One London Construction Ltd.

18. Please submit the proposed method for checking operational, vehicle and driver compliance with the CLOCS Standard throughout the duration of the contract.

Regal and Regal One London Construction will only use Subcontractors and Suppliers that are members of the Fleet Operator Recognition Scheme (FORS) and accredited with a minimum of silver standard. By only using such sub-contractors and suppliers we will be working with organisations that are CLOCS compliant.

At Regal, we will ensure our contractors put in place the following checks:

1. Contractual
  - a. At tender stage, we stipulate the requirement for contractors to be members of FORS and have a minimum of Silver standard.
  - b. At mid tender interviews with CMP is reviewed and the need for FORS accreditation and evidence of compliance with the CLOCS Standard.

- c. Noncompliance with the CLOCS Standard and a lack of evidence that a contractors' vehicle fleet is FORS accredited at Silver will result in the contract not being awarded.
  - d. Regal and their contractors have made the amendment to include the need for FORS Silver.
2. Site Set Up
- a. The CMP will be communicated to the sub-contractor(s) and supplier(s) at pre-contract order meeting and pre-commencement meeting to ensure that they are aware of specific constraints of the site in connection with; access routes, delivery times, booking deliveries, compliance with the traffic marshals instructions and only parking in the designated loading and unloading areas
  - b. Our construction phasing plan will be issued all sub-contractors and suppliers.
3. Site Operations
- a. There will be continued reinforcement of the requirements of the CMP in connection with delivery times and routes and non-compliance will be policed with a warning system and result in persistent offenders being barred from the site.
  - b. When there are requirements for any special deliveries to site such as early mornings of out of hours then permission will be sought from Camden and the residents informed via the Construction Working Group.
  - c. The Project Manager will keep a log of all deliveries with compliance check to ensure that delivery lorries are FORS registered – the log will be submitted to Camden on a monthly basis.
4. Vehicle and Vehicle Operator Check
- a. All vehicles arriving at site will be checked to be at FORS Silver as a minimum and those that are not will be turned away.
  - b. At the site gate, the driver will be asked to present their certificate or card to confirm that they have had vulnerable road user safety training.

19. Please confirm that you as the client/developer and your principal contractor have read and understood the CLOCS Standard and included it in your contracts.

I confirm that I have included the requirement to abide by the CLOCS Standard in my contracts to my contractors and suppliers.

## Site Traffic

Sections below shown in blue directly reference the CLOCS Standard requirements. The CLOCS Standard should be read in conjunction with this section.

**20. Traffic routing:** *“Clients shall ensure that a suitable, risk assessed vehicle route to the site is specified and that the route is communicated to all contractors and drivers. Clients shall make contractors and any other service suppliers aware that they are to use these routes at all times unless unavoidable diversions occur.”* (P19, 3.4.5)

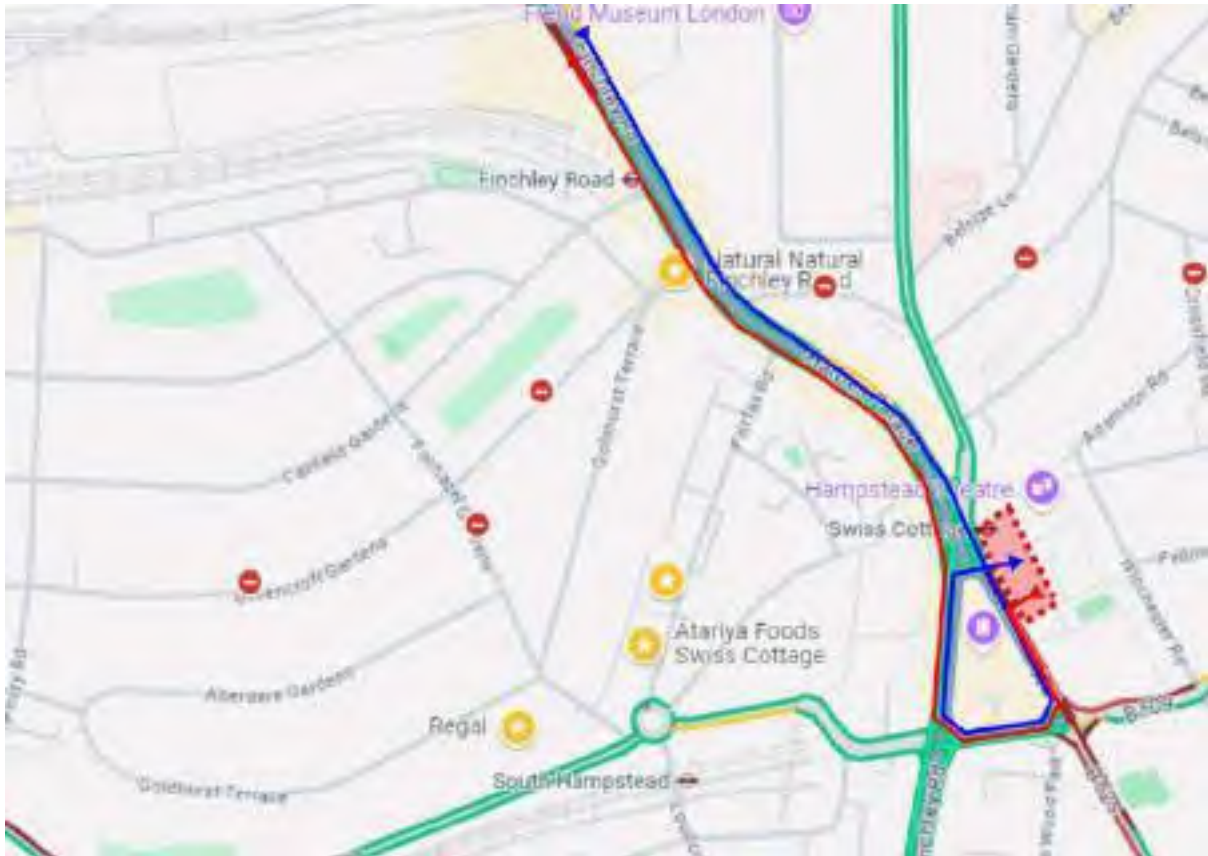
Routes should be carefully considered, and risk assessed, taking into account the need to avoid where possible any major cycle routes and trip generators such as schools, offices, public buildings, museums etc. Where appropriate, on routes that use high risk junctions (i.e. those that attract high volumes of cycling traffic) installing Trixi mirrors to aid driver visibility should be considered.

Consideration should also be given to weight restrictions, low bridges and cumulative impacts of construction (including neighbouring construction sites) on the public highway network. The route(s) to and from the site should be suitable for the size of vehicles that are to be used.

- a. Please indicate routes on a drawing or diagram showing the public highway network in the vicinity of the site including details of how vehicles will be routed to the [Transport for London Road Network](#) (TLRN) on approach and departure from the site.

### **Site entry points**

Due to the location of the site all construction traffic approaches the site from the north following the A41. The A41 is a major road and part of the TLRN (Transport for London Road Network). The primary access point to the site is from the A41 where there are two access gates for construction vehicles to enter and exit the site in a safe one-way gyratory action. In addition to these access points, there is a pit lane which takes the inner lane of the A41 which has space for two construction vehicles.



There is an existing entrance to the already built basement in Eton Avenue, deliveries through this access will only be requested in exceptional circumstances. Construction traffic will be instructed to not approach from any other route.



Existing entrance into basement car park shown above from Eton Ave to be utilised for basement deliveries only.

## **Access Routes 1 and 2**

Access and egress using the Pit Lane (Route 1):

Vehicles will travel along Finchley Road/ A41 onto Avenue Road A41 and into the Pit Lane. Vehicles exit the Pit Lane by turning right onto Adelaide Road, turn right again onto Finchley Road and head back out of London via the A41.



Route 1 - Pit Lane swept path analysis

Northern Crossover on site/ southern crossover exit A41 (Route 2):

Entry to crossover vehicles will travel along Finchley Road/ A41 onto Avenue Road A41, turn right onto Adelaide Road, turn right again onto Finchley Road and a final right across the A41 into the northern crossover into the construction project. Vehicles exit the construction site by exiting the southern gate turning left onto Avenue Road then turning right onto Adelaide Road, turn right again onto Finchley Road and head back out of London via the A41.

2. On delivery day the lorry approaches from the from the A41 at its prescribed time



4. The lorry goes around the block and sits at the junction waiting at the lights ready to cross.



5. The logistical team have the gate open watching for possible bike light jumpers.



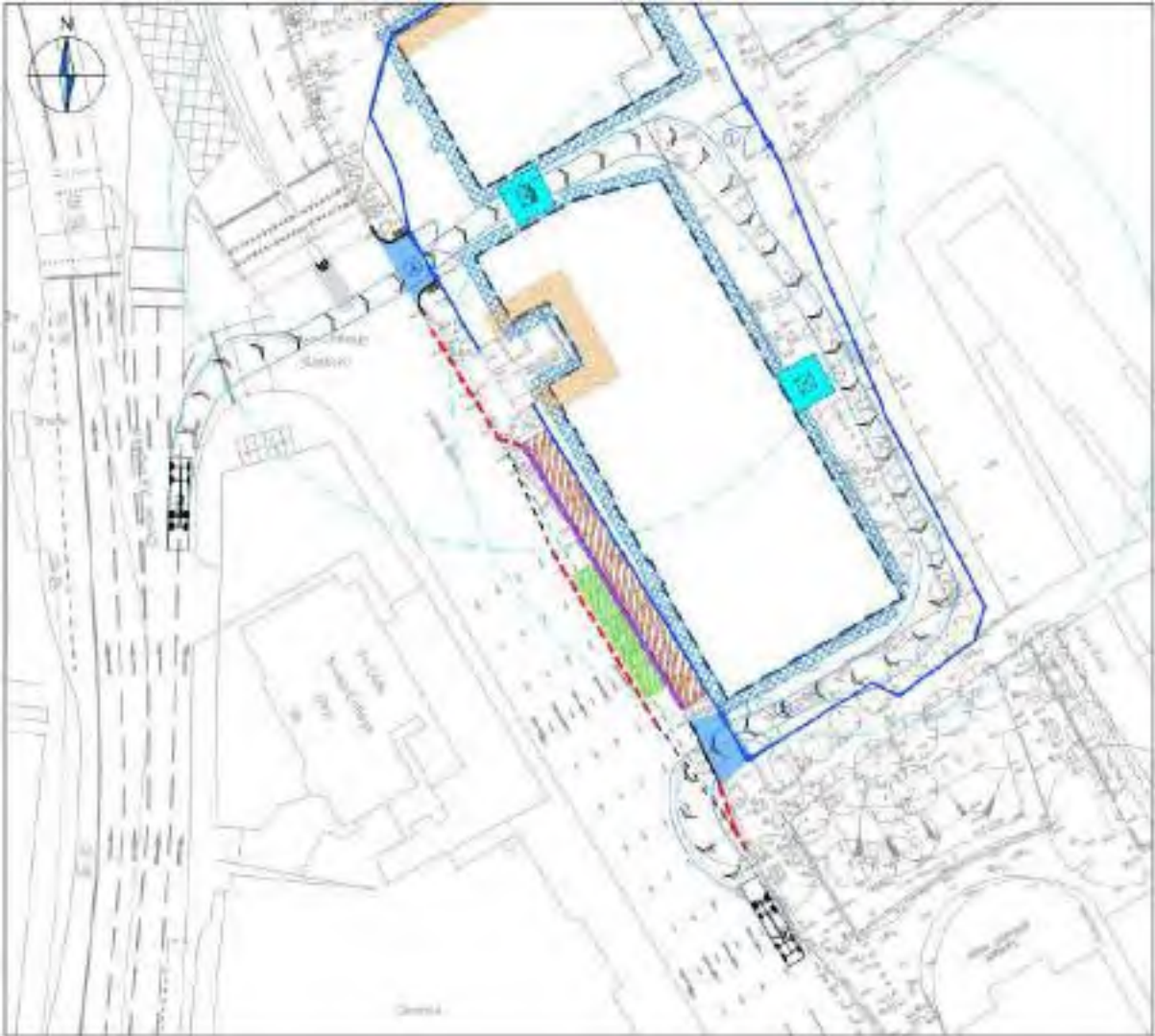


6. The lights change and the lorry is driven across in front of the B511 and onto site where the logistic team walk the vehicle through.



7. The barriers are shut and the gate closed.





Route 2 - Crossover swept path analysis

b. Please confirm how contractors and delivery companies will be made aware of the route (to and from the site) and of any on-site restrictions, prior to undertaking journeys.

As part of our procurement process all contractors, sub contractors and suppliers will be informed of the routes to and from site. We will also establish which type of delivery vehicle will use which entrance during the various stages of construction.

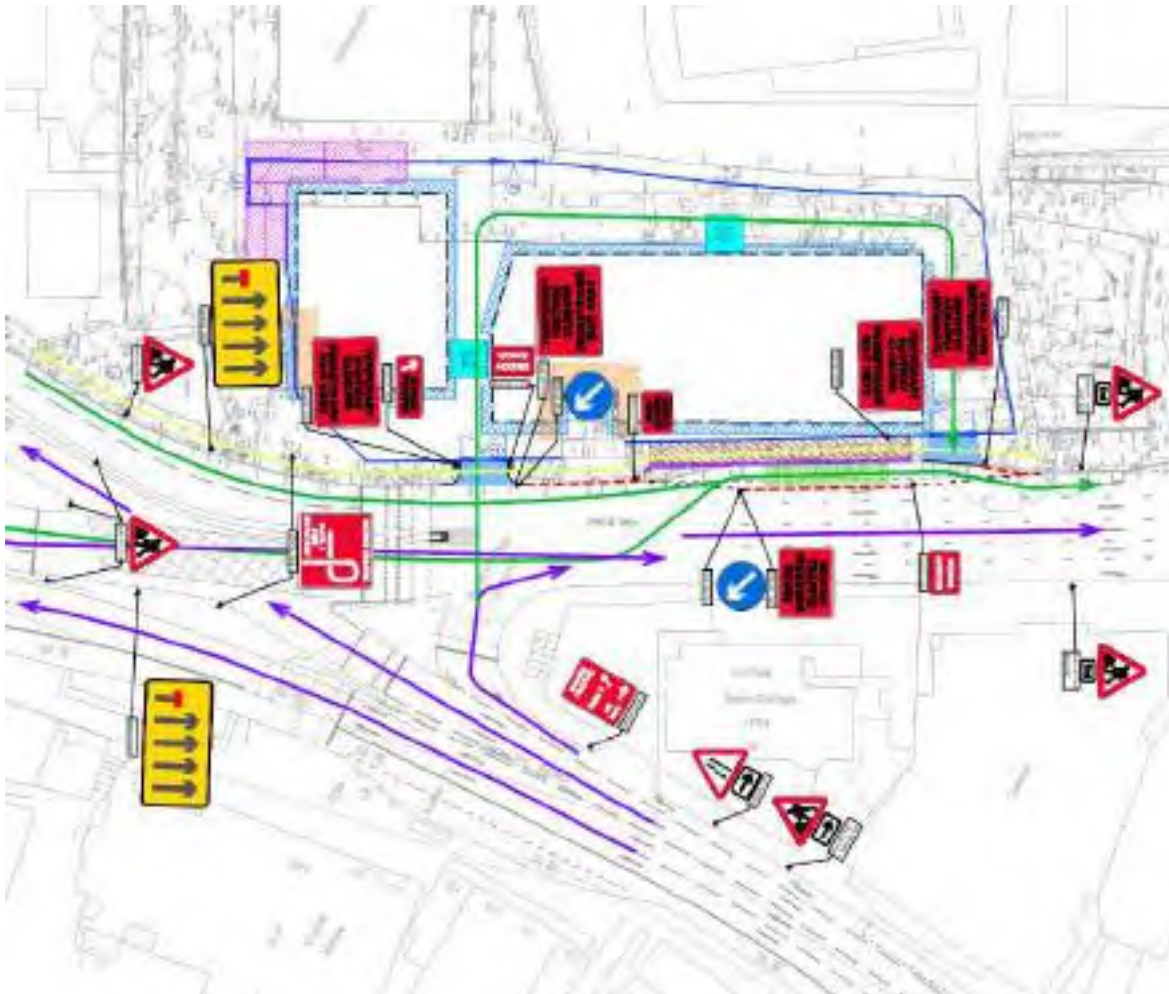
We will ensure that all subcontractors and suppliers that are part of our supply chain, who have to make deliveries, will be members of Transport for London's Fleet Operator Recognition Scheme (FORS), or similar, with a minimum level of Silver. We will use our contractor selection and procurement process to only select contractors who are members of FORS (or similar), by doing this we will be using drivers who are aware of

the demands of driving large vehicles in central London, with particular awareness of cyclists and pedestrians.

Any lorries waiting to enter the site will not be allowed to idle and will be instructed to turn engines off.

By using suppliers and subcontractors who are FORS (or similar) members then all delivery vehicles will have:

- Side Guards fitted, unless it can be demonstrated to the reasonable satisfaction of the Employer, that the Lorry will not perform the function, for which it was built, if Side Guards are fitted.
- A close proximity warning system fitted comprising of a front mounted, rear facing CCTV camera (or Fresnel Lens where this provides reliable alternative), a Close Proximity Sensor, an in-cab warning device (visual or audible) and an external warning device to make the road user in close proximity aware of the driver's planned manoeuvre.
- A Class VI Mirror
- Bear prominent signage on the rear of the vehicle to warn cyclists of the dangers of passing the vehicle on the inside.



**21. Control of site traffic, particularly at peak hours:** *“Clients shall consider other options to plan and control vehicles and reduce peak hour deliveries” (P20, 3.4.6)*

Construction vehicle movements are generally acceptable between 8.00am to 6.00pm on weekdays and between 8.00am and 1.00pm on Saturdays). If there is a school in the vicinity of the site or on the proposed access and/or egress routes, then deliveries must be restricted to between 9.30am and 3pm on weekdays during term time. (Refer to the [Guide for Contractors Working in Camden](#)).

A delivery plan should ensure that deliveries arrive at the correct part of site at the correct time. Instructions explaining such a plan should be sent to all suppliers and contractors. Consideration should be given to the location of any necessary holding areas for large sites with high volumes of traffic. Vehicles must not wait or circulate on the public highway. Whilst deliveries should be given set times to arrive, dwell and depart, no undue time pressures should be placed upon the driver at any time.

- a. Please provide details of the typical sizes of all vehicles and the approximate frequency and times of day when they will need access to the site, for each phase of construction. You should estimate the average daily number of vehicles during each major phase of the work, including their dwell time at the site. High numbers of vehicles per day and/or long dwell times may require vehicle holding procedures.

Detailed below of the typical vehicles that will be servicing the site. In addition to the vehicles detailed in this report we will need to bring to site large items of plant, such as tower cranes – these items of plant are delivered and removed from site infrequently and will need to be pre-booked with the council and for tower crane erection there will be the need for road closure(s) which are normally over a weekend and the appropriate licenses will be sought from the Council. Cars and motorcycles are deemed infrequent and will not be counted in the delivery numbers. Vans (small delivery vehicles) will be included in the delivery numbers. Vehicle classes visiting site over the construction period will also include (pictures are representative as description and will vary) -

Category C1 vehicles weighing between 3,500kg and 7,500kg (with a trailer up to 750kg).



Category C1+E trailer entitlement to Category C1, meaning a tow trailer weighing over 750kg. The combined weight of both the vehicle and the trailer cannot exceed 1,200kg and the trailer cannot weigh more than the towing vehicle either.

Category C vehicles weighing more than 3,500kg (with a trailer up to 750kg).



Category C+E trailer entitlement to Category C, a tow trailer weighing over 750kg. no longer than 16.5m.



Category D1 minibus with no more than 16 passenger seats and a maximum length of 8 metres, and possible tow trailer up to a maximum weight of 750kg.

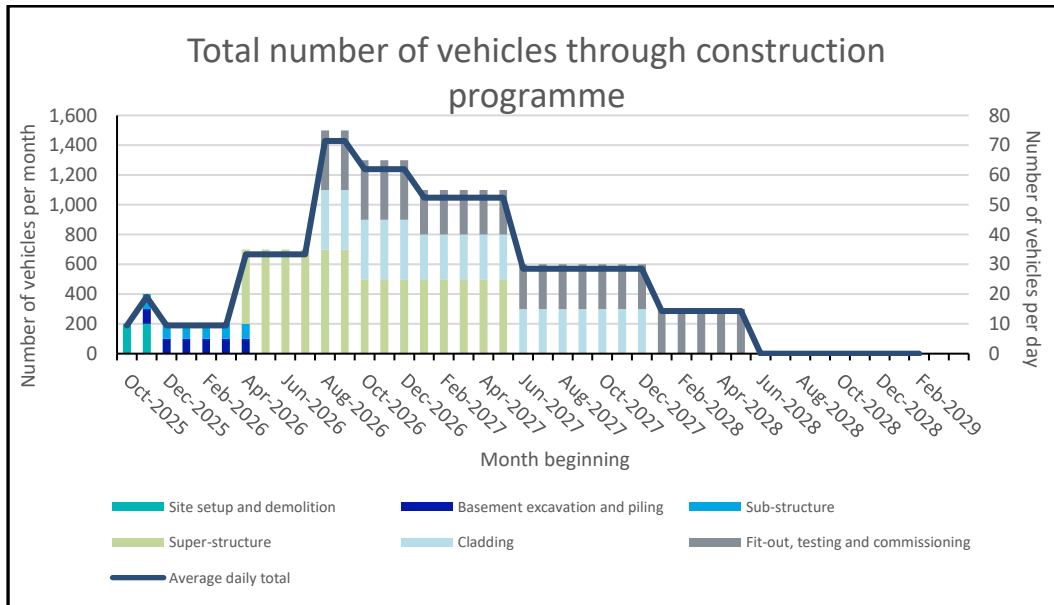
Category D1+E- D1 category vehicles with a trailer over 750kg – provided the combined weight of the trailer and vehicle do not exceed 12,000kg.

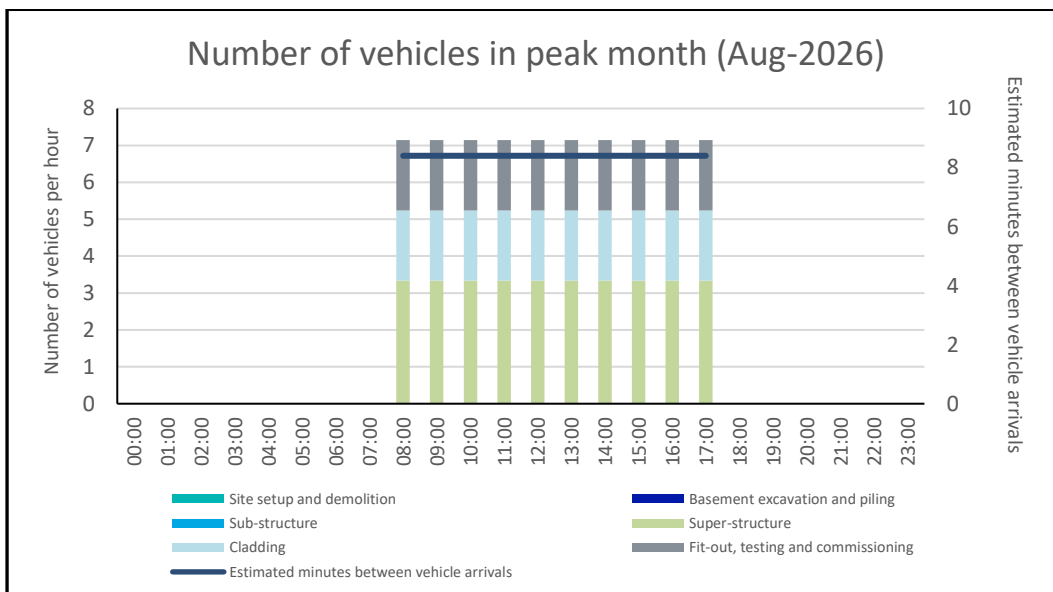
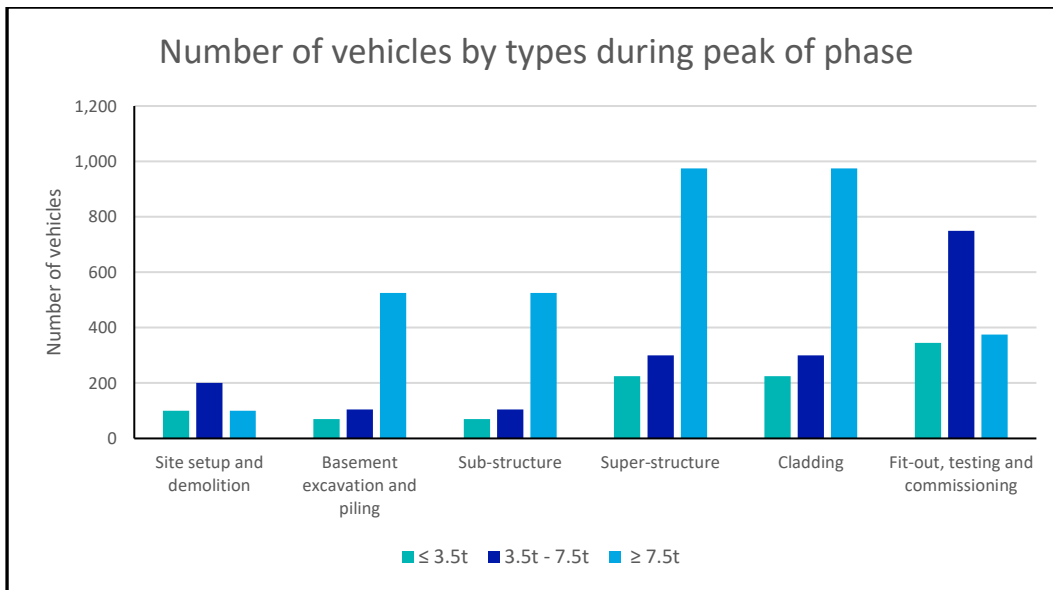
Category D+E tow trailer over 750kg on a Category D vehicle.

Category G- road roller.

Category H tracked vehicles.

Anticipated vehicle movements





The plan is based on delivery hours utilising A41 – 8am- 6pm Mon-Fri/ 8am-1pm Sat.

Only ‘planned and communicated’ use of Winchester Road -This would only be requested in exceptional circumstances for example when the site hoarding is being removed or trees being planted where access from the A41 would not be possible.

b. Please provide details of other developments in the local area or on the route.

For developments in the local area please see section 16. for developments with planning or that are under construction currently access the proposed route. The developments as they currently stand may be impacted include Hampstead Theatre and the Royal Central School of Speech and Drama and they are noted as being part of our consultation process – any outcomes from any consultation will be included in the CMP.

c. Please outline the system that is to be used to ensure that the correct vehicle attends the correct part of site at the correct time.

All deliveries will be pre booked using an online Delivery Management System to book, track and manage site deliveries. This will be controlled and managed by our on-site Logistics Manager. Additionally we will achieve this via the use of our sub-contractor coordination meetings where we will implement short term look-a-head programmes that will include the booking of deliveries.

This system will prevent potential miscommunication, double booking of gates and build-up of site traffic. The Delivery Management System will also produce a range of detailed reports including CO2 reports, FORS/ CLOCS and delivery stats.

d. Please identify the locations of any off-site holding areas (an appropriate location outside the borough may need to be identified, particularly if a large number of delivery vehicles are expected) and any measures that will be taken to ensure the prompt admission of vehicles to site in light of time required for any vehicle/driver compliance checks. Please refer to question 24 if any parking bay suspensions will be required for the holding area.

Due to the stringent delivery schedule that will be adhered throughout the project Regal don't envisage the need for consolidation centres.



The movement of all vehicles as they approach, enter and leave the site will also be controlled by traffic marshals. They will be responsible for the control and coordination of all aspects of material deliveries and movement. Vehicles will pull into the site or pit lane and never deliver on open areas outside the site. 2 tower cranes will be provided to facilitate easy and quick unloading of delivery vehicles. The cranes will be up to 50m in radius, but the use of a luffing jib will prevent the over-sailing of adjacent properties.

To minimise traffic to the western section of the site a pit lane has been implemented in the former bus stop D which will necessitate the temporary relocation of bus stop D.

Materials will be stored within the boundary of the site there will also be no parking policy on the site and all sub-contractors will be informed at the pre order meeting that the surrounding area is for resident parking only. All subcontractors will be encouraged to use public transport.

The scaffolding will be enclosed with a hoarding and we will ensure that a minimum of 2.5m of clear footpath remains open to the public along with emergency access for vehicles along the eastern edge.

The tower cranes that we propose to use will have a luffing jib, thereby eliminating the need for over-sailing, except for unloading/loading from the loading bay and the pit lane.

e. Please provide details of any other measures designed to reduce the impact of associated traffic (such as the use of [construction material consolidation centres](#)).

No other measures are being assessed at this time.

**22. Site entry/exit:** *“Clients shall ensure that access to and egress from the site is appropriately managed, clearly marked, understood and clear of obstacles.” (P18, 3.4.3)*

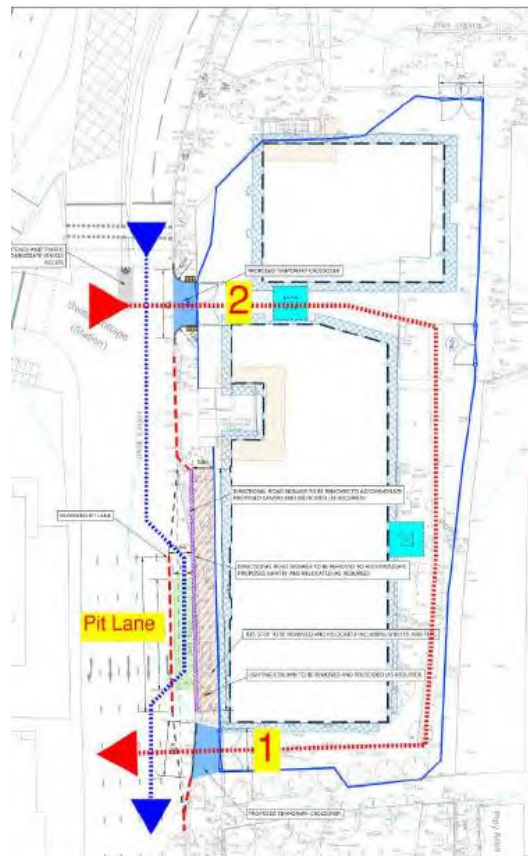
This section is only relevant where vehicles will be entering the site. Where vehicles are to load from the highway, please leave this section blank and refer to Q21. Where loading is to take place from a dedicated pit lane located on the public highway, please use this section to describe how vehicle entry/departure will be managed.

Vehicles entering and leaving the site should be carefully managed, using gates that are clearly marked and free from obstacles. Traffic marshals must ensure the safe passage of all

traffic on the public highway, in particular pedestrians and cyclists, when vehicles are entering and leaving site, particularly if reversing.

Traffic marshals, or site staff acting as traffic marshals, should hold the relevant qualifications required for directing large vehicles when reversing. Marshals should be equipped with 'STOP – WORKS' signs (not STOP/GO signs) if control of traffic on the public highway is required. Marshals should have radio contact with one another where necessary.

- a. Please detail the proposed site entry and exit points on a map or diagram.



Main Site access points Gates 1 and 2 on Avenue Rd.

- b. Please describe how the entry and exit arrangements for construction vehicles in and out of the site will be managed, including the number and location of traffic marshals where applicable. If this is shown in an attached drawing, use the following space to reference its location in the appendices.

#### Access gate 1

A licence will be once again agreed with Camden Parks to enable usage of the strip of land to the south of the site. The Sculpture and seating have been relocated to a more suitable

position to enable vehicles to enter and exit from the A41 and away from site. This will be the main site exit.

#### Access gate 2

Northern Crossover on site/southern crossover exit A41(Route 2). Entry to crossover vehicles will travel along Finchley Road/ A41 onto Avenue Road A41, turn right onto Adelaide Road, turn right again onto Finchley Road and a final right across into the northern crossover onto the construction project. Vehicles exit the construction project through the southern gate turning left onto Avenue Road then turning right onto Adelaide Road, turn right again onto Finchley Road and head back out of London via the A41, this will be the main entrance to site.

#### Pit lane

Access directly off the A41 using the existing bus stop to the west of the Site in a temporary "Pit Lane". Vehicles are parked here for unloading via the TC2 crane. A Gantry will be built over the existing footpath to ensure pedestrians could pass safely. The Bus stop has temporarily been moved.

All deliveries are to be supervised by a traffic marshal and reported to the Site Manager. All deliveries will be pre-booked so that the traffic marshals know when the delivery is coming and will take measures to ensure that the public are not affected by the delivery. The traffic marshal must be obeyed, and no phones or hands-free kits are to be used whilst driving, either on site roads or on public roads. A walkie-talkie system will be used so that the traffic marshals can communicate throughout the site.

Our Logistics Manager will plan works including vehicle movement, deliveries, temporary routes and facilities to ensure that the safety of the public is always maintained.



All deliveries will be co-ordinated and programmed to alleviate pressure on the road network. Deliveries will have to be pre-booked with site so that there are not any delivery vehicles waiting in the street. This will be achieved via the use of our weekly sub-contractor meetings and online booking system where deliveries will be planned and booked.

Delivery driver rules below:

## REMEMBER

**VEHICLES ON SITE KILL MANY CONSTRUCTION WORKERS EVERY YEAR  
BE AWARE OF PEDESTRIANS ON SITE AND PLEASE BE CONSIDERATE**

### VEHICLE RULES TO BE COMPLIED WITH AT ALL TIMES

- Site speed limit of 5mph, or lower where posted
- All vehicles must be registered to CLOCS or FORS  
- No reversing on site without the specific instruction of a marshal traffic
- Mandatory PPE – Hard Hat, Hi-Viz, Safety Boots (with ankle support), and Gloves, Glasses must be worn outside of cab
- Only park where instructed. Do not stop on side roads
- Bad language is prohibited as we are in close proximity to the public
- Do not use mobile phones when driving unless with hands free kit
- Obey all road traffic signage and wear seatbelts at all times
- No offloading without authorisation from Traffic Marshal
- No animals, children or unauthorised passengers permitted onto site
- Zero tolerance to Drugs & Alcohol
- No smoking, eating or drinking on site or in vehicle cabs
- Keys must not be left in unattended vehicles
- When fitted, orange beacons must be used when on site
- Fall prevention equipment required if climbing on to vehicle
- COVID 19 and Social Distancing Guidelines must be followed at all times
- Skip Drivers must hold an A39 CPCS Qualification – Yes / No / NA
- 12 Month Thorough Examination Certificate for Skip Vehicle – Yes / No / NA

**NB: compliance with the above is monitored by the site traffic management team**

Vehicle Registration	Date/Time		
	/	/	:
Do you suffer from or have you ever been affected by the following: -?	Yes	No	
Chest pains or Angina, hearing problems, visual defects, back or spinal problems, physical handicap or injury, diabetes, asthma, epilepsy or fits, dermatitis or reddened skin, alcoholism, claustrophobia, vertigo, other serious illness or occupational diseases?			
Are you currently taking any prescription medicine?			



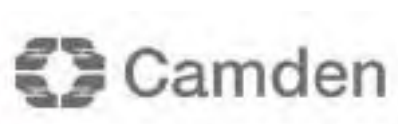
Drivers Name	Company	Signature

Access from the A41 will be as per Camden Guidelines of construction vehicle movements between 7.00am to 6.00pm on weekdays and between 7.00am and 1.00pm on Saturdays. The working time periods for the Pit Lane are based on the London Borough of Camden’s standard working hours for construction sites (0800-1800 hrs Monday to Friday and 0800-1300 hrs on Saturday). The proposed additional hour in the morning (0700-0800 hrs) would allow the first two construction vehicles to be ready for loading/ unloading when the site opens.

All suppliers and sub-contractors who are supplying materials to the site will be issued with a transport plan which will include a prescribed route into the site to deliver materials from the Transport for London Road Network; refer to plans earlier in this CMP. As part of our plans to mitigate the impact of the project and its deliveries on the road network we will in the first instance look to our supply chain to store materials off site and only deliver the materials when they are needed.

c. Please provide tracking/swept path drawings for vehicles entering/exiting the site if necessary. If these are attached, use the following space to reference their location in the appendices.

Swept path analysis drawings for vehicles entering and exiting the site are provided in Appendix A.



d. Provision of wheel washing facilities should be considered if necessary. If so, please provide details of how this will be managed and any run-off controlled. Please note that wheel washing should only be used where strictly necessary, and that a clean, stable surface for loading should be used where possible.

A wash down slab will be installed at site Access points 1 & 2 in order to keep roads and footpaths free from deposits of soil, mud and the like.

We will ensure that the wheels of any vehicles leaving this site are thoroughly cleaned and hosed down prior to going on the public roads. If any mud or construction debris does get onto the street within the vicinity of the site, then these areas will be cleaned via the use of water hoses and manually swept. In addition, a mechanical road sweeper will be used to clear any debris.

**23. Vehicle loading and unloading:** *“Clients shall ensure that vehicles are loaded and unloaded on-site as far as is practicable.” (P19, 3.4.4)*

If this is not possible, Traffic Marshalls must ensure the safe passage of pedestrians, cyclists and motor traffic in the street when vehicles are being loaded or unloaded.

Please provide details of the parking and loading arrangements for construction vehicles with regard to servicing and deliveries associated with the site (e.g., delivery of materials and plant, removal of excavated material). This is required as a scaled site plan, showing all points of access and where materials, skips and plant will be stored, and how vehicles will access and egress the site. If loading is to take place off site, please identify where this is due to take place and outline the measures you will take to ensure that loading/unloading is carried out safely. Please outline in question 24 if any parking bay suspensions will be required.

The logistics plan and swept path plan demonstrate how all access to the site will take place and all unloading will be from the protected pit lane or within the site.

All materials will be stored within the site.

All skips will be located within the site.

## Highway interventions

Please note that Temporary Traffic Orders (TTOs) and hoarding/scaffolding licenses may be applied for prior to CMP submission but won't be granted until the CMP is signed-off.

If the site is on or adjacent to the TLRN, please provide details of preliminary discussions with Transport for London in the relevant sections below.

### 24. Parking bay suspensions and temporary traffic orders

Please note, parking bay suspensions should only be requested where absolutely necessary. Parking bay suspensions are permitted for a maximum of 6 months, requirement of exclusive access to a bay for longer than 6 months you will be required to obtain [Temporary Traffic Order \(TTO\)](#) for which there is a separate cost.

Please provide details of any proposed parking bay suspensions and TTO's which would be required to facilitate construction. **Building materials and equipment must not cause obstructions on the highway as per your Considerate Contractors obligations unless the requisite permissions are secured.**

Information regarding parking suspensions can be found [here](#).

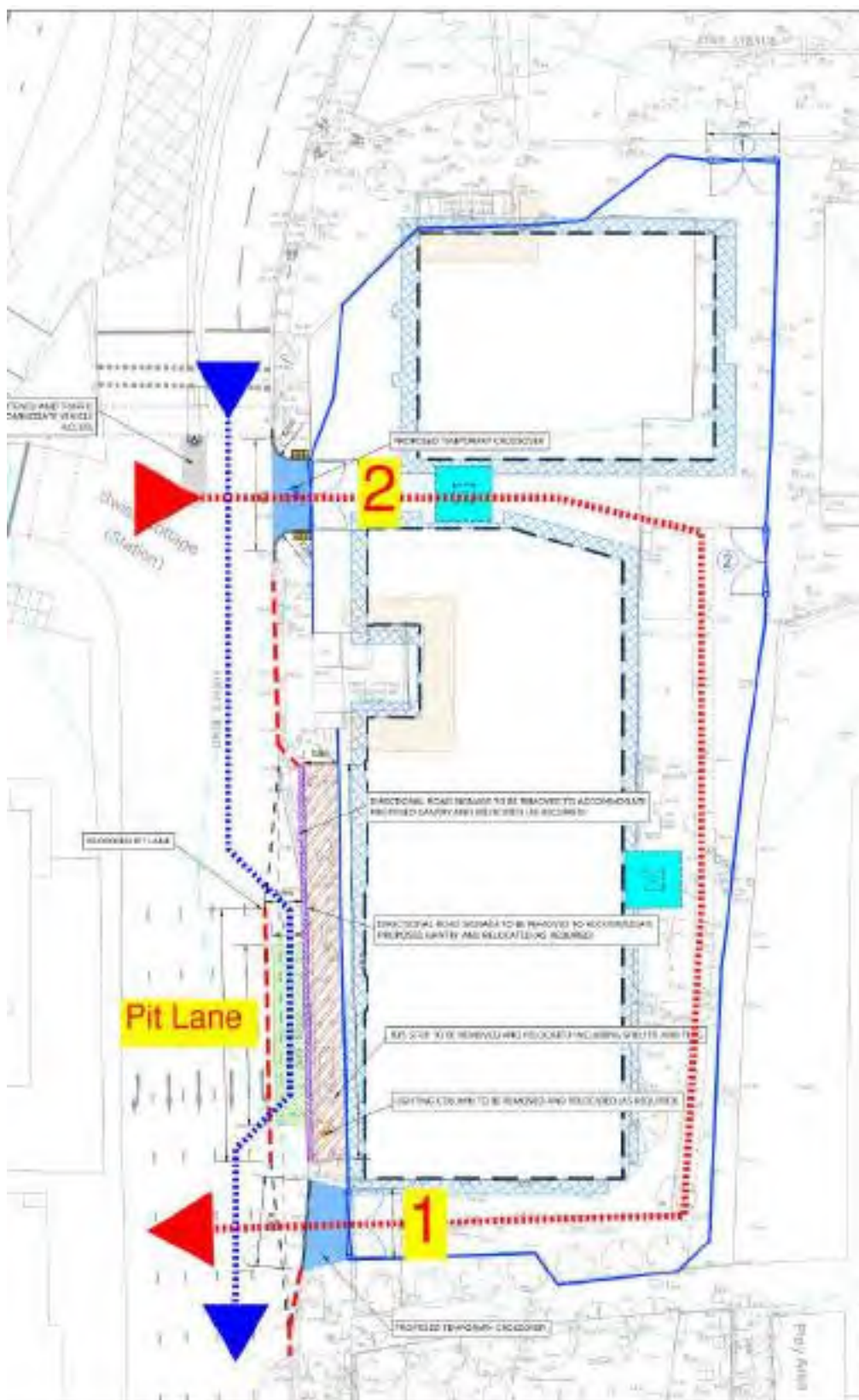
Currently Regal do not foresee the need for any parking bay suspension as the current transport routes have taken current parking bays into account.

### 25. Scaled drawings of highway works

Please note that use of the public highway for storage, site accommodation or welfare facilities is at the discretion of the Council and is generally not permitted. If you propose such use you must supply full justification, setting out why it is impossible to allocate space on-site. You must submit a detailed (to-scale) plan showing the impact on the public highway that includes the extent of any hoarding, pedestrian routes, parking bay suspensions and remaining road width for vehicle movements. We prefer not to close footways but if this is unavoidable, you should submit a scaled plan of the proposed diversion route showing key dimensions.

a. Please provide accurate scaled drawings of any highway works necessary to enable construction to take place (e.g. construction of temporary vehicular accesses).

Refer to the logistics plan below.





b. Please provide details of all safety signage, barriers and accessibility measures such as ramps and lighting etc.

Regal Construction will deploy and use all necessary and appropriate safety signage and barriers to ensure that the public are protected and our operatives and work safely at all times.



## 26. Diversions

Where applicable, please supply details of any diversion, disruption or other anticipated use of the public highway during the construction period (alternatively a plan may be submitted).

Bus stop D will be re-diverted to allow the erection of the pit lane and gantry for the duration of the construction period.

We also need to introduce the second crossover to the north of the LUL Avenue to facilitate deliveries from Avenue Road. We will be making separate highway applications to Camden close sections of the footpath and will be agreeing traffic management with TfL.

The entrance exit to the south of the site has been agreed and installed by Essential Living. A new park license will need to be agreed in due course.

We are aware of the existing utility services around the site and there will be disconnections and diversions. These will be planned to have minimum impact on the highway and the public.

## 27. VRU and pedestrian diversions, scaffolding and hoarding

Pedestrians and/or cyclist safety must be maintained if diversions are put in place. Vulnerable footway users should also be considered. These include wheelchair users, the elderly, those with walking difficulties, young children, those with prams, the blind and partially sighted. Appropriate ramping must be used if cables, hoses, etc. are run across the footway.

Any work above ground floor level may require a covered walkway adjacent to the site. A licence must be obtained for scaffolding and gantries. The adjoining public highway must be kept clean and free from obstructions. Lighting and signage should be used on temporary structures/skips/hoardings etc.

A secure hoarding will generally be required at the site boundary with a lockable access.

- a. Please provide details describing how pedestrian and cyclist safety will be maintained, including any proposed alternative routes (if necessary), and any Traffic Marshall arrangements.

When vehicles are entering or leaving the site, these will be supervised by our traffic marshals. Where possible vehicles will be unloaded from within the sites but where this

is not possible off loading areas will be defined and marshalled – these are noted on the site logistics plans attached.

The general public/pedestrians will have the right of way along the pathways that surround the site. At crossover, concertina barriers will be used to protect pedestrians from vehicles entering site.



The construction site gates will be kept closed and monitored by site security, only when deliveries are made to the site will they be opened to allow vehicles onto the site, at which time barriers will be used to prevent access by pedestrians and warn any passing cyclists. These barriers will be manned by our site security. All delivery vehicles will be supervised/controlled by a traffic marshall.

The Site Manager will also ensure that the external perimeter of the site is regularly patrolled to ensure that any debris is kept clear of the pavements.

Should there be any complaints arising from the works, local residents will be able to call personally to the site offices. Any residents visiting site to raise a complaint will be requested to sign-in and our security guard will escort the visitor to the site offices. There will also be an online complaints submission forum available for members of the public raise there concerns.

We will, as part of our sub-contractor procurement process, ensure that all sub-contractors and suppliers delivering materials to the site follow the conditions outlined in the Standard for Construction Logistics and Community Safety (CLOCS).

The Main Contractor will be obliged to appoint a director level point of contact who will deal personally with comments or complaints from the public or neighbours and will

ensure that they are resolved swiftly. A record will be kept of all comments and complaints.

All pedestrian within the gantry area will be protected by solid hoarding. When pedestrians are crossing site gates and marshals will only cordon the crossings when needed to make it safe to cross.

b. Please provide details of any temporary structures which would overhang the public highway (e.g. scaffolding, gantries, cranes etc.) and details of hoarding requirements or any other occupation of the public highway.

All hoardings to be a minimum of 2.4m high with suitable lighting provided from dawn till dusk.

The pit lane gantry will be detailed and sits to the west of the project.

A protection gantry will also be provided to both LUL access points around the project.

Due the heavily undulating land to the rear of the site the hoarding will be set to a level to ensure that all members of the public are protected.

Hoarding, scaffold and the gantry licenses will be applied for at Camden and TfL.

# Environment

To answer these sections please refer to the relevant sections of **Camden's Minimum Requirements for Building Construction (CMRBC)**.

28. Please list all noisy operation\_ and the construction methods used, and provide details of the times that each of these are due to be carried out.

By its nature construction works can cause noise, noise being created by; mechanical plant, cutting, drilling, hammering and sawing. All noisy work will be restricted to occur only after 8.00am and before 6.00pm Monday to Friday (excluding Bank Holidays). We will always seek to not carry out noisy work on a Saturday when we are permitted to work between 8am and 1pm.

The activities that will create “noisy” operation are:

- Breaking and drilling of concrete
- Concrete lorries, Concrete pumps, hoists, steelwork etc.
- Drilling & the use of nail guns during the fit out

To reduce noise we will:

- Adhere to working hours
- Plan deliveries by arranging routes and times to minimise potential nuisance to the local community
- Keep noisy plant away from public areas
- Minimise drop heights into hoppers, lorries and other plant
- Use local screening where necessary
- Screen is placed between plant and a nearby sensitive location, ie house
- Silenced generators and tower lights where necessary
- Keep acoustic doors, hoods and panels on plant closed – it does make a difference
- Minimise vehicles and plant reversing – incorporate one way systems

Due to the noise, sensitive nature of the some of the surrounding receptors specifically the Hampstead Theatre to the north side of the site. Quiet periods will be arranged along the north side of the site in line with their performance schedule, we will be consulting with the Theatre managers to agree any times where we need to reduce site noise.

29. Please confirm when the most recent pre-construction noise survey was carried out and provide a copy. If a noise survey has not taken place, and it has been requested by the local

authority, please indicate the date (before any works are being carried out) that the noise survey will be taking place, and agree to provide a copy.

The most recent noise survey was undertaken by RBA between 15 May 2024 and 20 May 2024 and is attached at Appendix E.

30. Please provide predictions for noise levels throughout the proposed works.

Please refer to the Construction Environmental Management Plan – Noise Predictions document in Appendix F.

31. Please provide details describing mitigation measures to be incorporated during the construction works to prevent noise and vibration disturbances from the activities on the site, including the actions to be taken in cases where these exceed the predicted levels.

We will ensure that all plant has noise reduction or has an acoustic blanket around the plant.

Noise levels are monitored, should any exceedances trigger an alert our logistics company will be made aware, investigate the issue and carry out measures to reduce the noise.

32. Please provide evidence that staff have been trained on BS 5228:2009

All Project Managers will be required to attend the Site Managers Safety Training Scheme as run by the CITB. All sub-contractors supervisors will have attended the Site Supervisors Safety Training Scheme as run by the CITB.

Noise awareness will be cascaded via toolbox talks.

33. Please provide specific details on how air pollution and dust nuisance arising from dusty activities on site will be prevented. This should be relevant and proportionate to activities due to take place, with a focus on both preventative and reactive mitigation measures.

Regal and its contractors will adhere to the key legislation and site-specific restrictions as detailed in the:

- Control of Pollution Act 1974
- Environmental Protection Act 1990 (ss79-82)
- BS 5228:1997, Code of Practice on Construction and Open Site.
- Camden's Minimum Requirements (CMR230772,)(Addendum233259 CMR230772)
- British Standards BS5228:2009+A1:2014

- The Control of Dust and Emissions During Construction and Demolition (SUPPLEMENTARY PLANNING GUIDANCE) 2014  
Noise/vibration reduction and visible dust prevention philosophies.
- 'O'Keefe Demolition' Document Reference D001-NV-001 'Control of Demolition Noise and Vibration 100 Avenue Road Swiss Cottage' dated 20th August 2017 and further revisions.
- Property and Infrastructure Capita 'Baseline Noise Monitoring' reference 296993-01 (00) dated November 2017 and further revisions.

Site operations will be controlled so that all plant and machinery noise emissions (including the provision of ventilation, heating and cooling) shall be designed, installed and operated at noise levels that do not cause noise nuisance to the nearest adjoining residential properties.

We have, with our sub-contractors, implemented a Dust Management Plan (DMP) that has previously been approved by Camden. The DMP will include but not be limited to:

- Only use cutting, grinding or sawing equipment fitted or in conjunction with suitable dust suppression techniques such as water sprays or local extraction
- Consider full enclosure of sites or specific operations where there is a high potential for dust production and the site is active for an extensive period
- Where reasonably practicable store materials with the potential to produce dust away from site boundaries
- Ensure no burning of waste materials takes place on site
- Do not allow dry sweeping
- Ensure regular cleaning of hard standings using wet sweeping methods
- Maintain all dust control equipment in good condition and record maintenance activities
- Routinely clean public roads and access routes using wet sweeping methods
- Ensure all vehicles carrying loose or potentially dusty material to or from the site are fully sheeted
- Ensure that any crushing or grinding plant used on the site, which falls within the definition in Section 3.5 Chapter 3 of the Pollution Prevention and Control (England and Wales) Regulations 2000 SI 1973, has an appropriate permit issued and is maintained according to the procedures set out in the Pollution, Prevention and Control Act 1999.

We are aware of the Dust & Air Emissions Mitigation Measures as prepared by the Institute of Air Quality Management and will use their mitigation and control measure to ensure that dust is controlled on site both during demolition and construction.

34. Please provide details describing how any significant amounts of dirt or dust that may be spread onto the public highway will be prevented and/or cleaned.

Dust and debris on the road are one of the main environmental nuisance and safety problems arising from construction sites.

Regal have instigated provisions to minimise this problem by having a wheel wash jet on site and the site access road will also be a tarmac construction.

The wash slab area located at the exit from this site will be impermeable and isolated from the surrounding area by a raised kerb or roll over bund to contain solids, with effluent directed to the foul sewer

We will also make provision for cleaning of the road by an approved road sweeper.

We will use a fine spray to suppress dust on the following:

- Unpaved areas that are subject to traffic or wind.
- Sand, spoil and aggregate stockpiles.
- During loading/unloading of dust generating materials

35. For medium or high impact risk level sites, please provide details describing arrangements for monitoring of noise, vibration and dust levels, including instrumentation, locations of monitors and trigger levels where appropriate.

Please refer to Appendix M (Air Quality and Dust Management Plan)

36. Please confirm that an Air Quality Assessment and/or Dust Risk Assessment has been undertaken at planning application stage in line with the GLA policy [The Control of Dust and Emissions During Demolition and Construction 2014 \(SPG\)](#) (document access at bottom of webpage), and that the summary dust impact risk level (without mitigation) has been identified. The risk assessment must take account of proximity to all human receptors and sensitive receptors (e.g. schools, care homes etc.), as detailed in the [SPG](#). **Please attach the risk assessment and mitigation checklist as an appendix.**



Please refer to Appendix M (Air Quality and Dust Management Plan)

37. Please confirm that all of the GLA's 'highly recommended' measures from the SPG document relative to the level of dust impact risk identified in question 36 have been addressed by completing the GLA mitigation measures checklist. (See Appendix 7 of the SPG document.)

The measures listed in section 2 of the DMP are all of the GLA's 'highly recommended' measures from the SPG document relative to the level of dust impact risk identified.

38. Please confirm the number of real-time dust monitors to be used on-site.

Note: **real-time dust (PM<sub>10</sub>) monitoring with MCERTS 'Indicative' monitoring equipment will be required for all sites with a high OR medium dust impact risk level.** If the site is a 'high impact' site, 4 real time dust monitors will be required. If the site is a 'medium impact' site', 2 real time dust monitors will be required.

The dust monitoring must be in accordance with the SPG and IAQM guidance, and **the proposed dust monitoring regime (including number of monitors, locations, equipment specification, and trigger levels) must be submitted to the Council for approval.** Dust monitoring is required for the entire duration of the development and must be in place and operational **at least three months prior to the commencement of works on-site.** Monthly dust monitoring reports must be provided to the Council detailing activities during each monthly period, dust mitigation measures in place, monitoring data coverage, graphs of measured dust (PM<sub>10</sub>) concentrations, any exceedances of the trigger levels, and an explanation on the causes of any and all exceedances in addition to additional mitigation measures implemented to rectify these.

In accordance with Camden's Clean Air Action Plan, the monthly dust monitoring reports must also be made readily available and accessible online to members of the public soon after publication. Information on how to access the monthly dust monitoring reports should be advertised to the local community (e.g. presented on the site boundaries in full public view).

**Inadequate dust monitoring or reporting, or failure to limit trigger level exceedances, will be indicative of poor air quality and dust management and will lead to enforcement action.**

The site has been identified in the Dust Risk Assessment as low risk for construction, and medium risk for trackout. As a result, two PM<sub>10</sub> monitors will be operated, as set out in Section 4 of the DMP. It is confirmed that these monitors will meet MCERTs standard. Further details of the location, equipment and trigger levels and report set out in the DMP. As there is long term PM<sub>10</sub> monitoring data from the Swiss Cottage air quality monitor, which is 80m from the development site, it is not considered necessary to gather 3-months of monitoring data prior to commencement of the construction works. This approach is consistent with the approach in IAQMs Guidance on Monitoring in the Vicinity of Demolition and Construction Sites, which states that, *“In most situations, baseline monitoring may not be required, e.g. in some urban areas where there is a large existing body of monitoring data (and where these sites are expected to continue to operate throughout the duration of the construction works).”* Monthly dust reports will be provided to Camden.

39. Please provide details about how rodents, including rats, will be prevented from spreading out from the site. You are required to provide information about site inspections carried out and present copies of receipts (if work undertaken).

If there is evidence of rodents following regular surveys then a qualified pest control firm will follow the procedures set out by the HSE in Information Sheet MISC515 for the laying of baits. The baits being approved under the Control of Pesticides Regulations 1986 (as amended 1997)/Biocidal Products Regulations 2001 (as amended 2003). As part of the work by a qualified pest control firm will be employed.

All further works will be completed in accordance with “Pest minimisation Best Practice for the Construction Industry” for eradication of rat/mice.

40. Please confirm when an asbestos survey was carried out at the site and include the key findings.

An asbestos survey was carried out in September 2014. All works relating to this have now been completed.

The below table summarises the asbestos containing materials identified during the inspection that was removed.

Floor	Location	Material	Level of ID	Asbestos Type	Action
5th	Risers 2, 3 & 4	Rope expansion	Identifi	Chrysotile	Remove
		between			HSE guidelines
		walls & concrete			

		ceili			
4th	Risers 2, 3 & 4	Rope expansion between walls & concrete ceili	Strongl presum	Chrysotile	Remove HSE guidelines
3rd	Risers 2, 3, 4, 5	Rope expansion between walls & concrete ceili	Strongl presum	Chrysotile	Remove HSE guidelines
2nd	Risers 2, 3, 4 &	Rope fire stop to of block built riser	Identifi	Chrysotile	Remove HSE guidelines
1st	Risers 2, 3, 4 &	Rope fire stop to of block built riser	Strongl presum	Chrysotile	Remove HSE guidelines
Ground	Risers 2, 3 & 5	Rope fire stop to of block built riser	Strongl presum	Chrysotile	Remove HSE guidelines
Baseme	Boiler room	Rope gasket to penetration	Identifi	Chrysotile	Remove HSE guidelines
Baseme	Boiler room	Rope gaskets boiler units	Identifi	Chrysotile	Remove HSE guidelines
Baseme	Store	Rope gasket to penetration	Strongl presum ed	Chrysotile	Remove HSE guidelines

41. Complaints often arise from the conduct of builders in an area. Please confirm steps being taken to minimise this e.g. provision of a suitable smoking area, tackling bad language and unnecessary shouting.

Regal take seriously the conduct of the builders on all of our sites to ensure that there is not any anti-social behaviour.

Should there be any complaints arising from the works, local residents will be able to call personally to the site offices. Any residents visiting site to raise a complaint will be requested to sign-in and our security guard will escort the visitor to the site offices.

Our Community Liaison Manager will deal personally with comments or complaints from the public or neighbours and will ensure that they are resolved swiftly. A record will be kept of all comments and complaints.

There will also be a project web site where local residents will be able to comment on any issues with the construction.

At Regal we do not tolerate any bad language or unnecessary shouting on our sites. We operate a “red card” system whereby any operative found to be acting in an anti-social way or smoking outside of designated smoking area will be given a “red card” and asked to leave the site immediately.

No radios will be allowed on site.

42. If you will be using non-road mobile machinery (NRMM) on site with net power between 37kW and 560kW it will be required to meet the standards set out below. The standards are applicable to both variable and constant speed engines and apply for both PM and NOx emissions.

Regal confirm that we will register the project with NRMM and all applicable non road plant will be logged and checked to ensure that its emission levels are below the targets set by the NRMM Register and best practice established in Camden set out in the table below. Any machinery and plant to exceed the required emission levels will not be allowed onto the site.

Contractor HGVs	100% Euro VI. Standards adopted from start of scheme.	Exemptions: specialist vehicles where Euro VI demonstrated unfeasible will be permitted up to a maximum of 8% of unique vehicles over course of scheme.	Contractors to set out their exemptions & compliance management process in LESSMP and report to Applicant on monthly basis, including vehicle numbers and any non-compliances.  Once continuous improvement is shown in each of three separate months, frequency will be reduced.
Contractor Cars	Euro IV (petrol) or Euro VI (diesel) or ultra low emission vehicles. Adopting standards from beginning of construction, reach 100% by 1 <sup>st</sup> January, 2020	Exemptions: None permitted	
Contractor Vans	Euro IV (petrol) or Euro VI (diesel) or ultra low emission vehicles. Adopting standards from beginning of construction, reach 75% by 1 <sup>st</sup> January, 2020	Exemptions: 25% of unique vehicles permitted after above milestone.	

**From 1st September 2015**

**(i) Major Development Sites** – NRMM used on the site of any major development will be required to meet Stage IIIA of EU Directive 97/68/EC

**(ii) Any development site within the Central Activity Zone** - NRMM used on any site within the Central Activity Zone will be required to meet Stage IIIB of EU Directive 97/68/EC

**From 1st September 2020**

**(iii) Any development site** - NRMM used on any site within Greater London will be required to meet Stage IIIB of EU Directive 97/68/EC

**(iv) Any development site within the Central Activity Zone** - NRMM used on any site within the Central Activity Zone will be required to meet Stage IV of EU Directive 97/68/EC

Please provide evidence demonstrating the above requirements will be met by answering the following questions:

- a) Construction time period (mm/yy - mm/yy): 10/25 – 06/28
- b) Is the development within the CAZ? (Y/N): No
- c) Will the NRMM with net power between 37kW and 560kW meet the standards outlined above? (Y/N): Yes
- d) Please confirm that all relevant machinery will be registered on the NRMM Register, including the site name under which it has been registered: The project will be registered with the NRMM as noted in question 42.
- e) Please confirm that an inventory of all NRMM will be kept on site and that all machinery will be regularly serviced and service logs kept on site for inspection: An inventory will be kept on site including service log records – all in accordance with the NRMM Register.
- f) Please confirm that records will be kept on site which details proof of emission limits, including legible photographs of individual engine plates for all equipment, and that this documentation will be made available to local authority officers as required: This requirement will be actioned on site.

43. Vehicle engine idling (leaving engines running whilst parked or not in traffic) produces avoidable air pollution and can damage the health of drivers and local communities. Camden Council and the City of London Corporation lead the London **Idling Action Project** to educate drivers about the health impacts of air pollution and the importance of switching off engines as a simple action to help protect the health of all Londoners.

Idling Action calls for businesses and fleet operators to take the **Engines Off pledge** to reduce emissions and improve air quality by asking fleet drivers, employees and subcontractors to avoid idling their engines wherever possible. Free driver training materials are available from the website: <https://idlingaction.london/business/>

Please provide details about how you will reduce avoidable air pollution from engine idling, including whether your organisation has committed to the Engines Off pledge and the number of staff or subcontractors who have been provided with free training materials.

TBC

## Mental Health Training

44. Poor mental health is inextricably linked to physical health, which in turn impacts performance and quality, and ultimately affects productivity, creativity and morale. Workers in the construction industry are six times more likely to take their own life than be killed in a fall from height.

We strongly recommend signing up to the “[Building Mental Health](#)” charter, an industry-wide framework and charter to tackle the poor mental health in the construction industry, or joining [Mates In Mind](#), which providing the skills, clarity and confidence to construction industry employers on how to raise awareness, improve understanding and address the stigma that surrounds mental health.

The Council can support by providing free Mental Health First Aid training, publicity resources and signposting to local support services.

Please state whether you are or will be signed up to the Building Mental Health charter (or similar scheme), and that and appropriate number of trained Mental Health First Aiders will be available on site.

TBC

# Agreement

The agreed contents of this Construction Management Plan must be complied with unless otherwise agreed in writing by the Council. This may require the CMP to be revised by the Developer and reapproved by the Council. The project manager shall work with the Council to review this Construction Management Plan if problems arise in relation to the construction of the development. Any future revised plan must be approved by the Council in writing and complied with thereafter.

It should be noted that any agreed Construction Management Plan does not prejudice further agreements that may be required such as road closures or hoarding licences.

**Signed:** .....

**Date:** .....

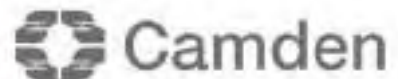
**Print Name:** .....

**Position:** .....

Please submit to: [planningobligations@camden.gov.uk](mailto:planningobligations@camden.gov.uk)

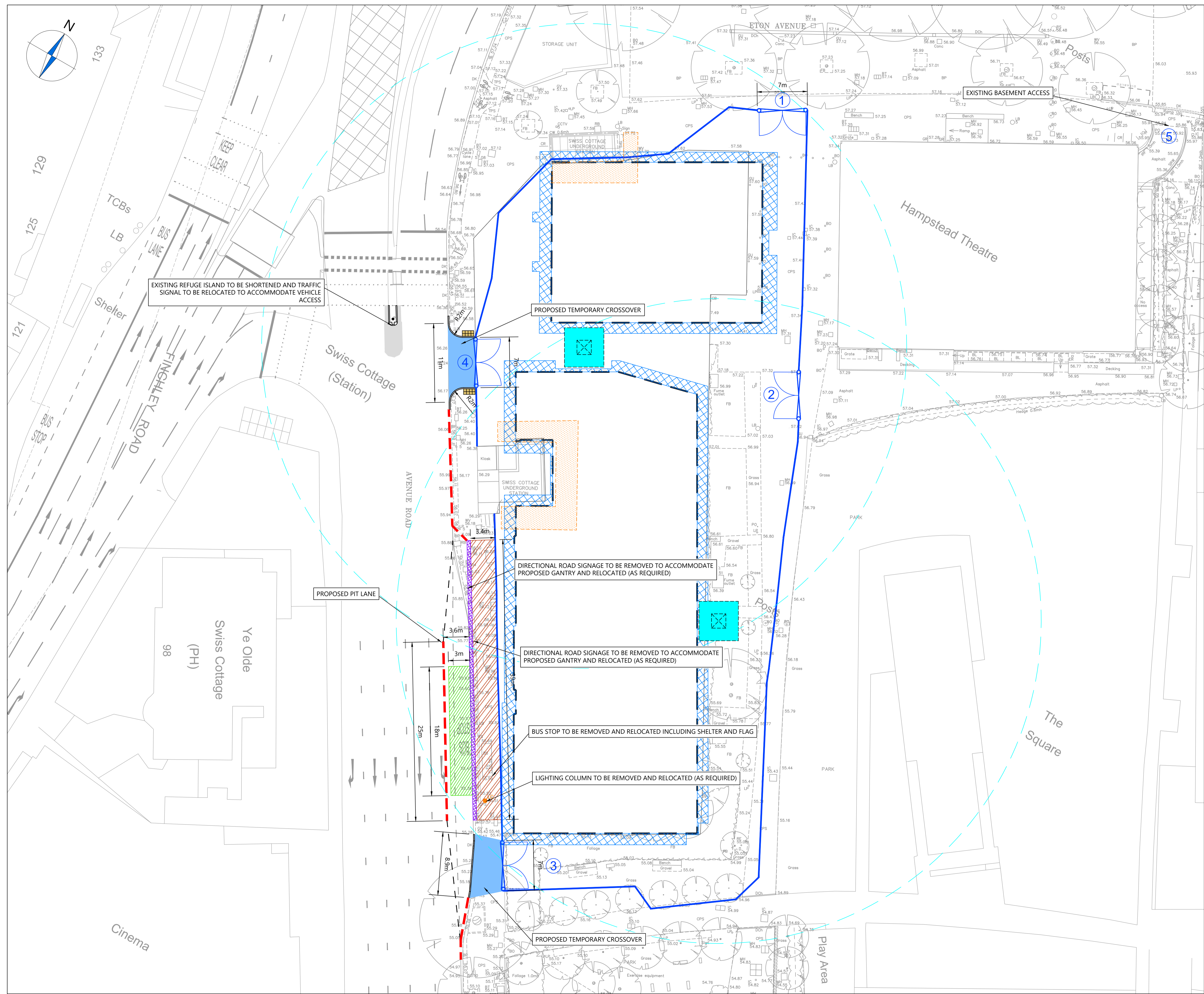
**End of form.**

V2.9





# Appendix A

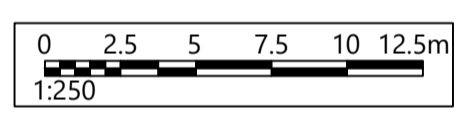


**NOTES**

- GENERAL**
- This drawing to be read in conjunction with other contract drawings.
  - This drawing to be read & printed in colour.
- CONSTRUCTION**
- All works to be in accordance with Manual of Contract Documents for Highway Works, Volume 1 - Specification for Highway Works by National Highways, the Specification shall predominate in the event of conflicts with the Drawings or other specifications.
  - All inspection covers / service valve covers to be raised / lowered to suit proposed levels.
  - All new manhole covers in footway are to be D400 covers.
- CDM (RISKS & HAZARDS)**
- Prior to commencement of construction the contractor is to liaise with all relevant statutory undertakers and protect / divert apparatus and to protect the workforce during the works. Any damage caused to the apparatus to be the responsibility of the contractor.
  - Contractor to undertake their own statutory plant checks on site prior to the commencement of excavation exercise.
  - The contractor is to make sure that any excavation should be adequately covered at night to protect both public and wildlife from becoming trapped.
- DISCLAIMERS**
- The existing Topographical survey has been provided by Greenhatch, and Caneparo Associates shall not be liable for any inaccuracies or deficiencies.

**KEY :**

	SITE HOARDING
	PROPOSED BUILDING LINE
	GATE AND ACCESS NUMBER
	AREA OF GANTRY AND COVERED WALKWAY
	LOADING BAY AREA
	VEHICLE PROTECTION BARRIER
	PROPOSED KERBS
	AREA OF CARRIAGEWAY WORKS
	AREA OF TEMPORARY CROSSOVER WORKS (WITH TREE PROTECTION)
	RELOCATED TRAFFIC SIGNAL
	SCAFFOLD PROTECTION
	PROPOSED TACTILE PAVING
	LIGHTING COLUMN TO BE REMOVED AND RELOCATED (AS REQUIRED)
	AREA OF SCAFFOLDING
	TOWER CRANE TO BE SITUATED ON RAISED PLATFORM TO ALLOW CONSTRUCTION VEHICLES TO PASS UNDER (WITH 45m WORKING RADIUS)
	TOWER CRANE LIFTING EXCLUSION ZONE



B	Updated Hoarding Line	COS	RB	12.08.2024
A	Updated Hoarding Line	COS	RB	07.08.2024
Rev	Details	Drawn	Checked	Date
<b>REVISION HISTORY</b>				
Status:	<input checked="" type="checkbox"/> Preliminary	<input type="checkbox"/> Detailed	<input type="checkbox"/> As Built	

Client: Regal Avenue Road Limited

Project: 100 Avenue Road, Camden

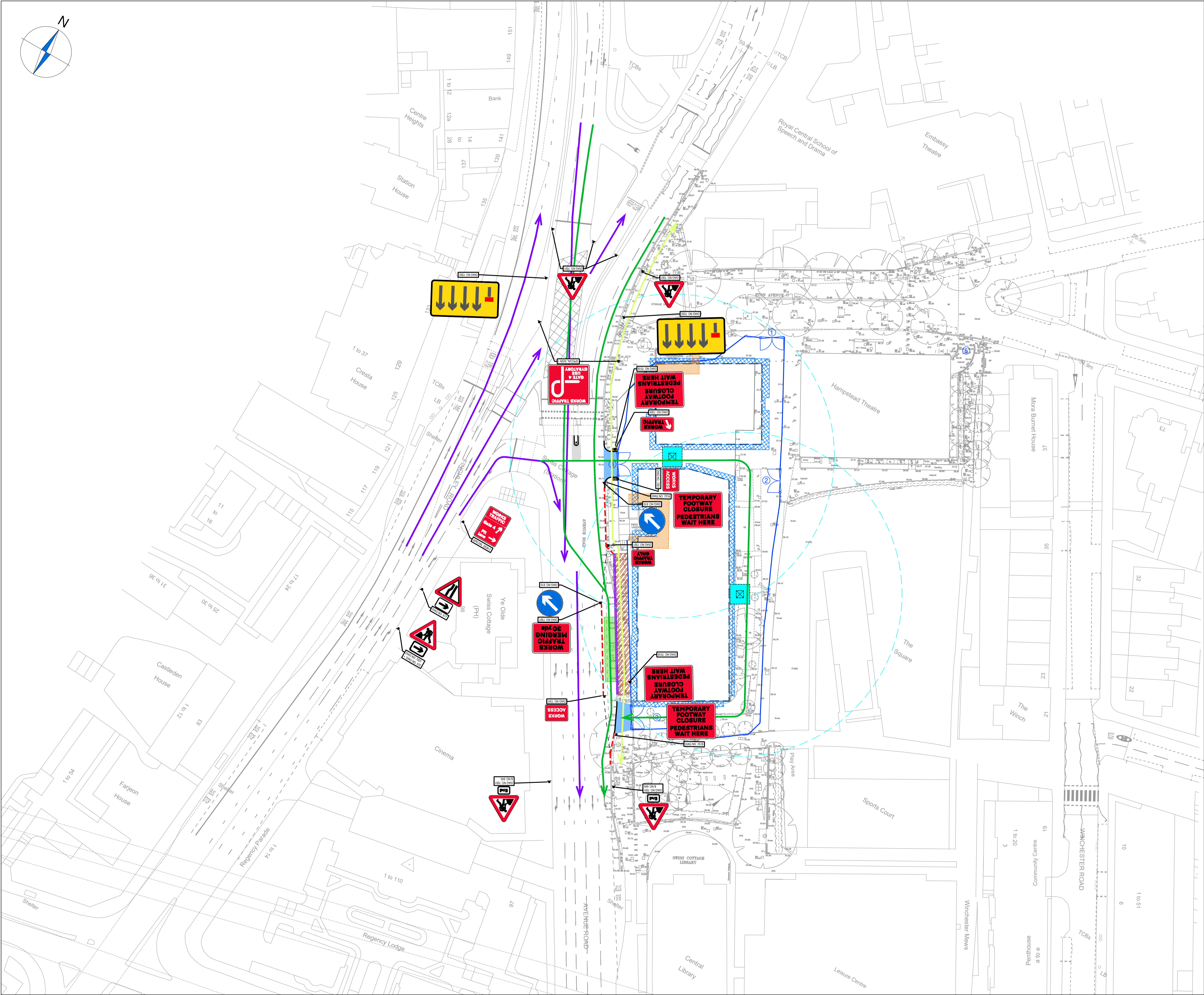
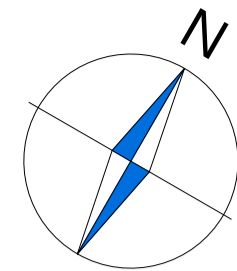
Drawing Title: TMAN Application Proposed General Arrangement

Scale: 1:250 Size: A1

Drawn by: COS Checked by: RB Approved by: RB Date: 05.07.2024

**CANEPARO ASSOCIATES**  
Transport Planning & Highway Design  
21 Little Portland Street • London • W1W 8BT • Tel. 020 3617 8200

Scheme Ref: 5352 Drawing No: 001 Sheet: 1 of 1 Rev: B

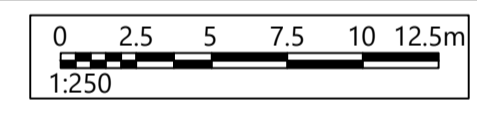


**NOTES**

- GENERAL**
- This drawing to be read in conjunction with other contract drawings.
  - This drawing to be read & printed in colour.
- CONSTRUCTION**
- All works to be in accordance with Manual of Contract Documents for Highway Works, Volume 1 - Specification for Highway Works by National Highways, the Specification shall predominate in the event of conflicts with the Drawings or other specifications.
  - All inspection covers / service valve covers to be raised / lowered to suit proposed levels.
  - All new manhole covers in footway are to be D400 covers.
- CDM (RISKS & HAZARDS)**
- Prior to commencement of construction the contractor is to liaise with all relevant statutory undertakers and protect / divert apparatus and to protect the workforce during the works. Any damage caused to the apparatus to be the responsibility of the contractor.
  - Contractor to undertake their own statutory plant checks on site prior to the commencement of excavation exercise.
  - The contractor is to make sure that any excavation should be adequately covered at night to protect both public and wildlife from becoming trapped.
- DISCLAIMERS**
- The existing Topographical survey has been provided by Greenhatch, and Caneparo Associates shall not be liable for any inaccuracies or deficiencies.

**KEY :**

	SITE HOARDING
	PROPOSED BUILDING LINE
	GATE AND ACCESS NUMBER
	AREA OF GANTRY AND COVERED WALKWAY
	LOADING BAY AREA
	VEHICLE PROTECTION BARRIER
	PROPOSED KERBS
	AREA OF CARRIAGEWAY WORKS
	AREA OF TEMPORARY CROSSOVER WORKS (WITH TREE PROTECTION)
	RELOCATED TRAFFIC SIGNAL
	SCAFFOLD PROTECTION
	PROPOSED TACTILE PAVING
	LIGHTING COLUMN TO BE REMOVED AND RELOCATED (AS REQUIRED)
	AREA OF SCAFFOLDING
	SITE ACCOMMODATION AND OFFICES
	TOWER CRANE TO BE SITUATED ON RAISED PLATFORM TO ALLOW CONSTRUCTION VEHICLES TO PASS UNDER (WITH 45m WORKING RADIUS)
	TOWER CRANE LIFTING EXCLUSION ZONE
	PEDESTRIAN MOVEMENTS
	CONSTRUCTION MOVEMENTS
	GENERAL TRAFFIC MEASUREMENTS



A	Updated Hoarding Line	COS	RB	07.08.2024
Rev	Details	Drawn	Checked	Date
REVISION HISTORY				
Status:	<input type="checkbox"/> Preliminary	<input checked="" type="checkbox"/> Detailed	<input type="checkbox"/> As Built	

Client: **Regal Avenue Road Limited**

Project: **100 Avenue Road, Camden**

Drawing Title: **TMAN Application  
Traffic Management, Pedestrian and  
Vehicle Movement Plans**

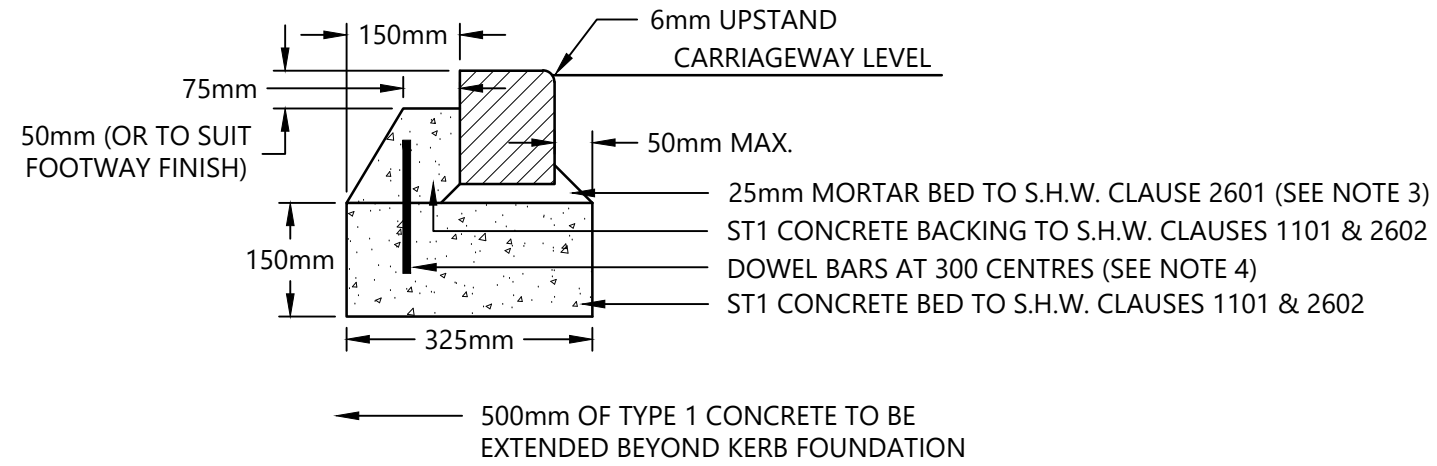
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Drawn by: **COS** Checked by: **RB** Approved by: **RB** Date: **05.07.2024**

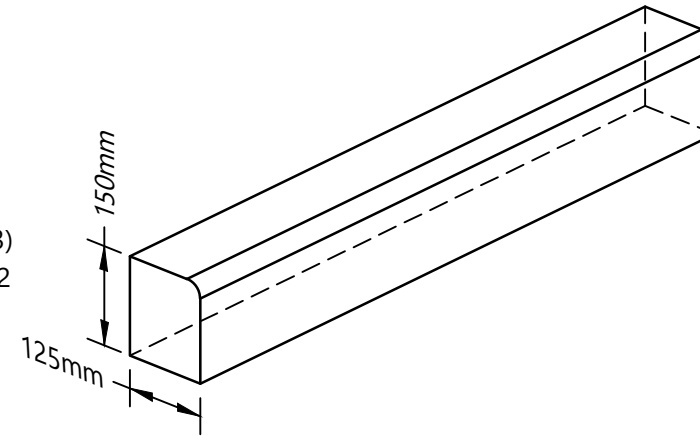
**CANEPARO ASSOCIATES**  
Transport Planning & Highway Design  
21 Little Portland Street • London • W1W 8BT • Tel. 020 3617 8200

Scheme Ref: **5352** Drawing No: **002** Sheet: **1 of 1** Rev: **A**

**125x150mm PCC BULLNOSED KERB TYPE BN BY MARSHALLS (OR SIMILAR APPROVED)**

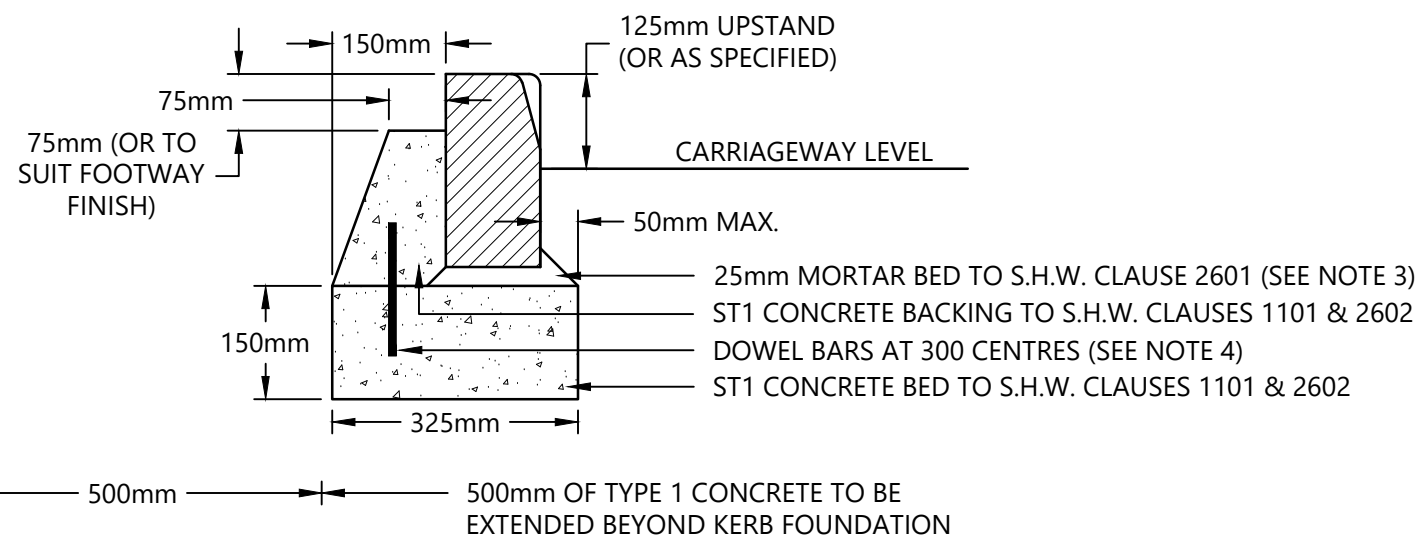


TYPICAL CROSS-SECTION

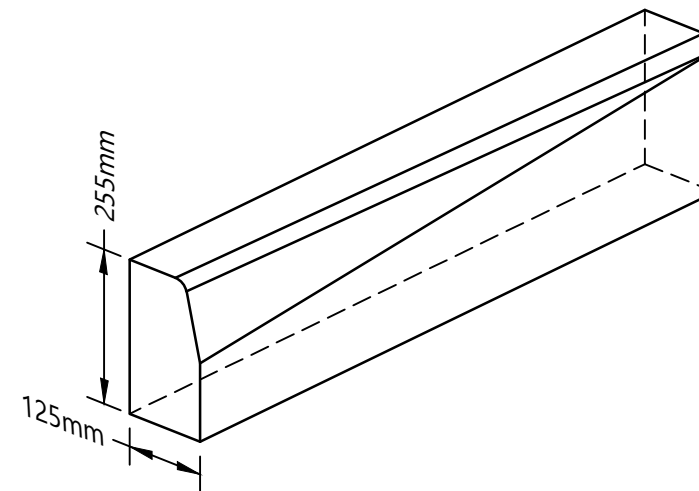


ISOMETRIC DETAIL (NOT TO SCALE)

**125x255mm PCC TRANSITION KERB (BULLNOSED / HALF BATTERED) BY MARSHALLS (OR SIMILAR APPROVED)**



TYPICAL CROSS-SECTION



ISOMETRIC DETAIL (NOT TO SCALE)

**NOTES**

1. Do not scale from this drawing.
2. This drawing to be read in conjunction with other contract drawings.
3. Mortar bed can be omitted if kerbs laid on fresh semi-dry concrete.
4. Dowels can be omitted if backing placed before bed concrete has hardened.

Rev	Details	REVISION HISTORY			Drawn	Checked	Date
Status: <input checked="" type="checkbox"/> Preliminary <input type="checkbox"/> Detailed <input type="checkbox"/> As Built							

Client: Regal Avenue Road Limited

Project: 100 Avenue Road, Camden

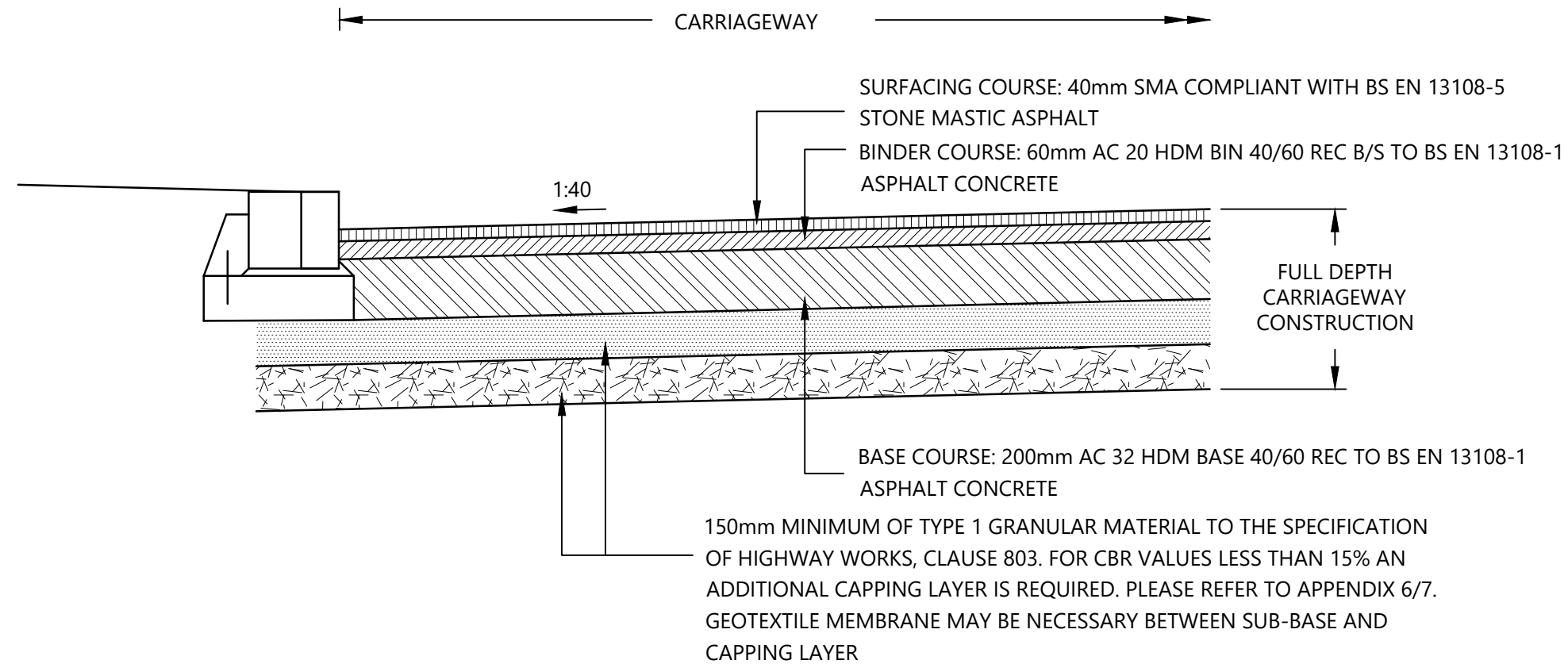
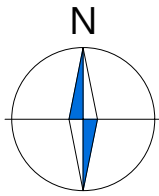
Drawing Title: Kerb and Footway Edging Standard Detail

Scale: 1:10 Size: A3

Drawn by: .COS Checked by: RB Approved by: GS Date: 26.07.2024



Scheme Ref: <b>5352</b>	Drawing No: <b>SD01</b>	Sheet: <b>1 of 1</b>	Rev: <b>...</b>
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**NOTES**

1. This drawing to be read & printed in colour.
2. This drawing is for illustrative purposes only.
3. Design speed for all vehicle swept paths is 5kph.
4. Stationary steering has not been used on this drawing.

Rev	Details	REVISION HISTORY			Drawn	Checked	Date

Status:  Preliminary  Detailed  As Built

Client:  
**Regal Avenue Road Limited**

Project:  
**100 Avenue Road, Camden**

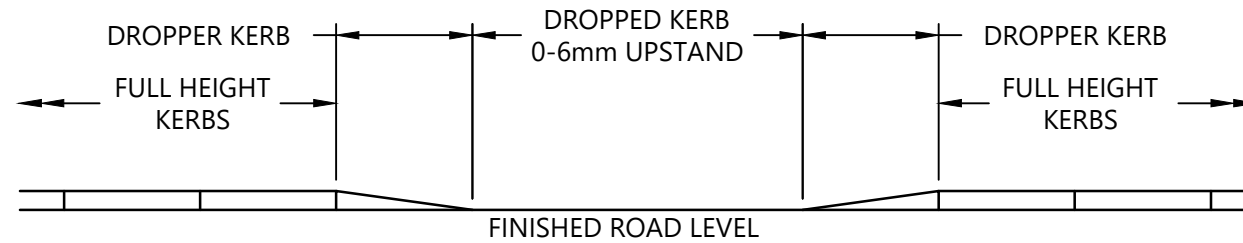
Drawing Title:  
**Proposed Temporary Crossover Construction**

Scale: **1:10** Size: **A3**

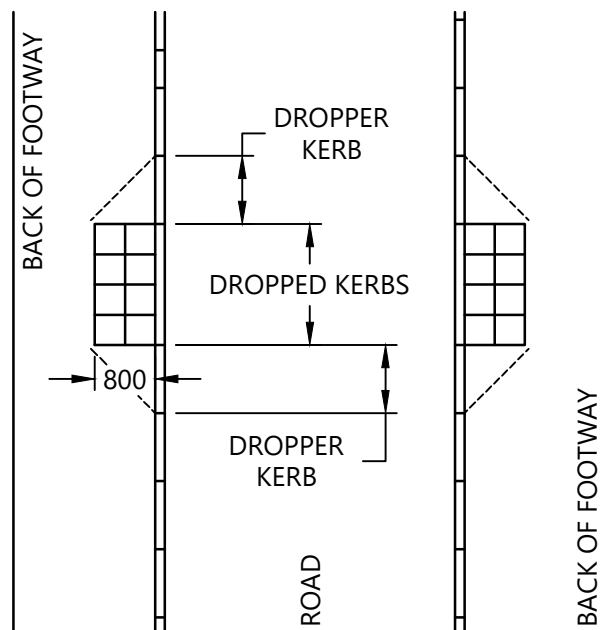
Drawn by: COS	Checked by: RB	Approved by: GS	Date: 26.07.2024
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**CANEPARO ASSOCIATES**  
Transport Planning & Highway Design  
21 Little Portland Street • London • W1W 8BT • Tel. 020 3617 8200

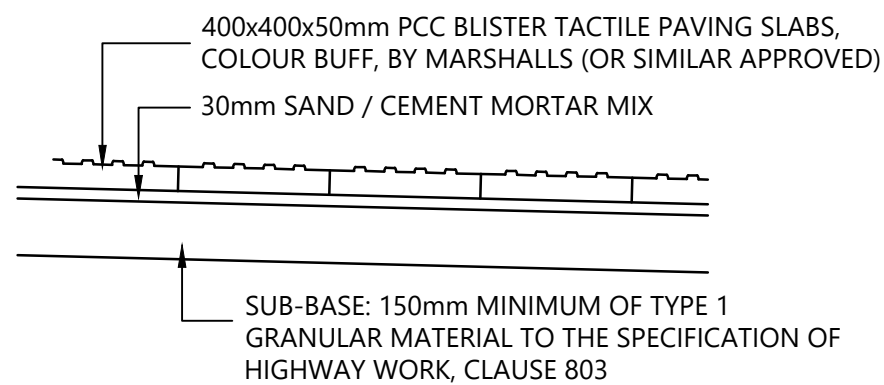
Scheme Ref: <b>5352</b>	Drawing No: <b>SD02</b>	Sheet : <b>1 of 1</b>	Rev: <b>...</b>
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TYPICAL ARRANGEMENT OF DROPPED KERBS AT PEDESTRIAN CROSSING  
SCALE 1:50



UNCONTROLLED CROSSING AWAY FROM JUNCTION  
SCALE 1:100



TACTILE FOOTWAY CONSTRUCTION (FULL DEPTH)  
SCALE 1:20

**NOTES**

1. Do not scale from this drawing.
2. This drawing to be read in conjunction with other contract drawings.

Rev	Details	REVISION HISTORY			Drawn	Checked	Date
Status: <input type="checkbox"/> Preliminary <input type="checkbox"/> Detailed <input type="checkbox"/> As Built							

Client:

Regal Avenue Road Limited

Project:

100 Avenue Road, Camden

Drawing Title:

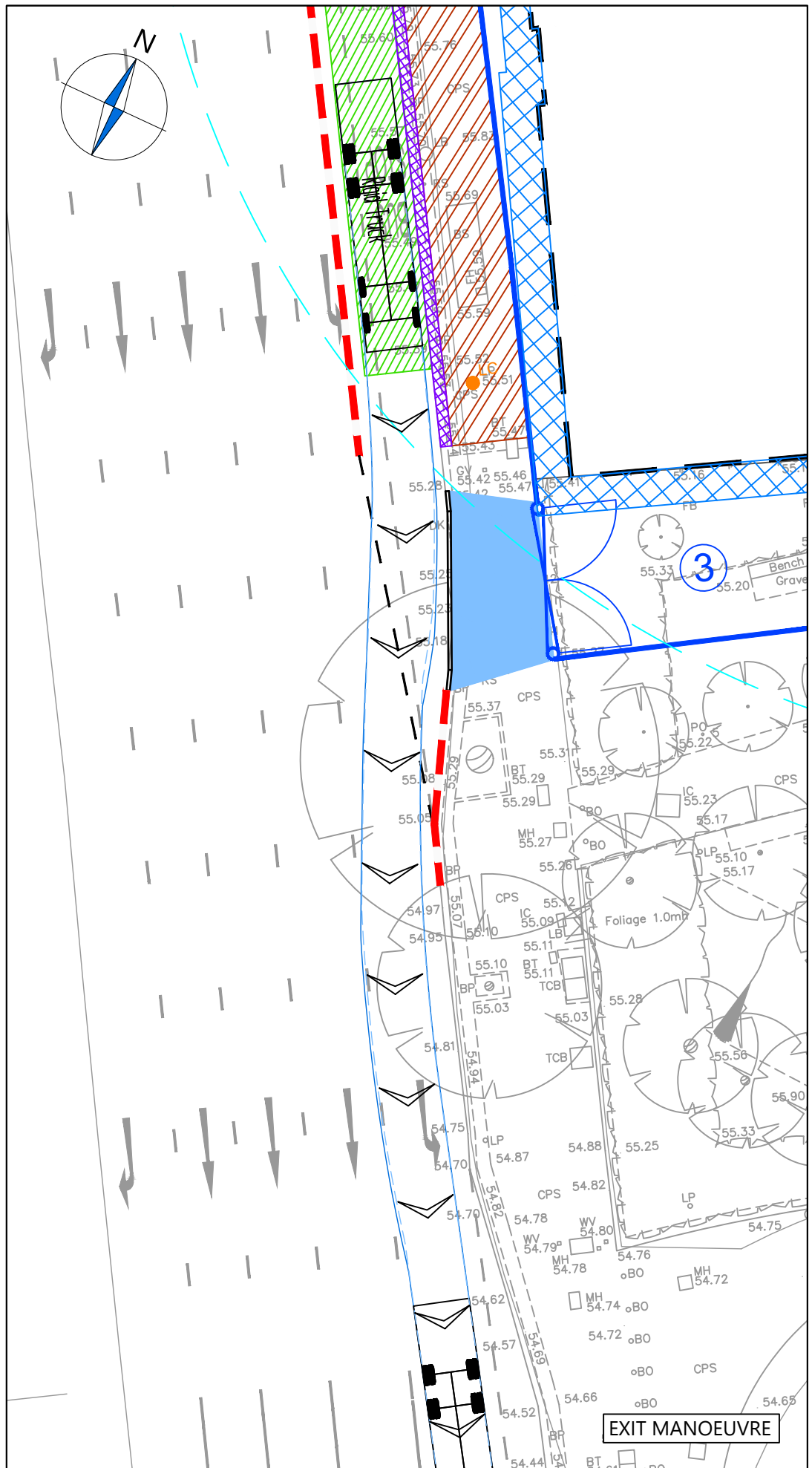
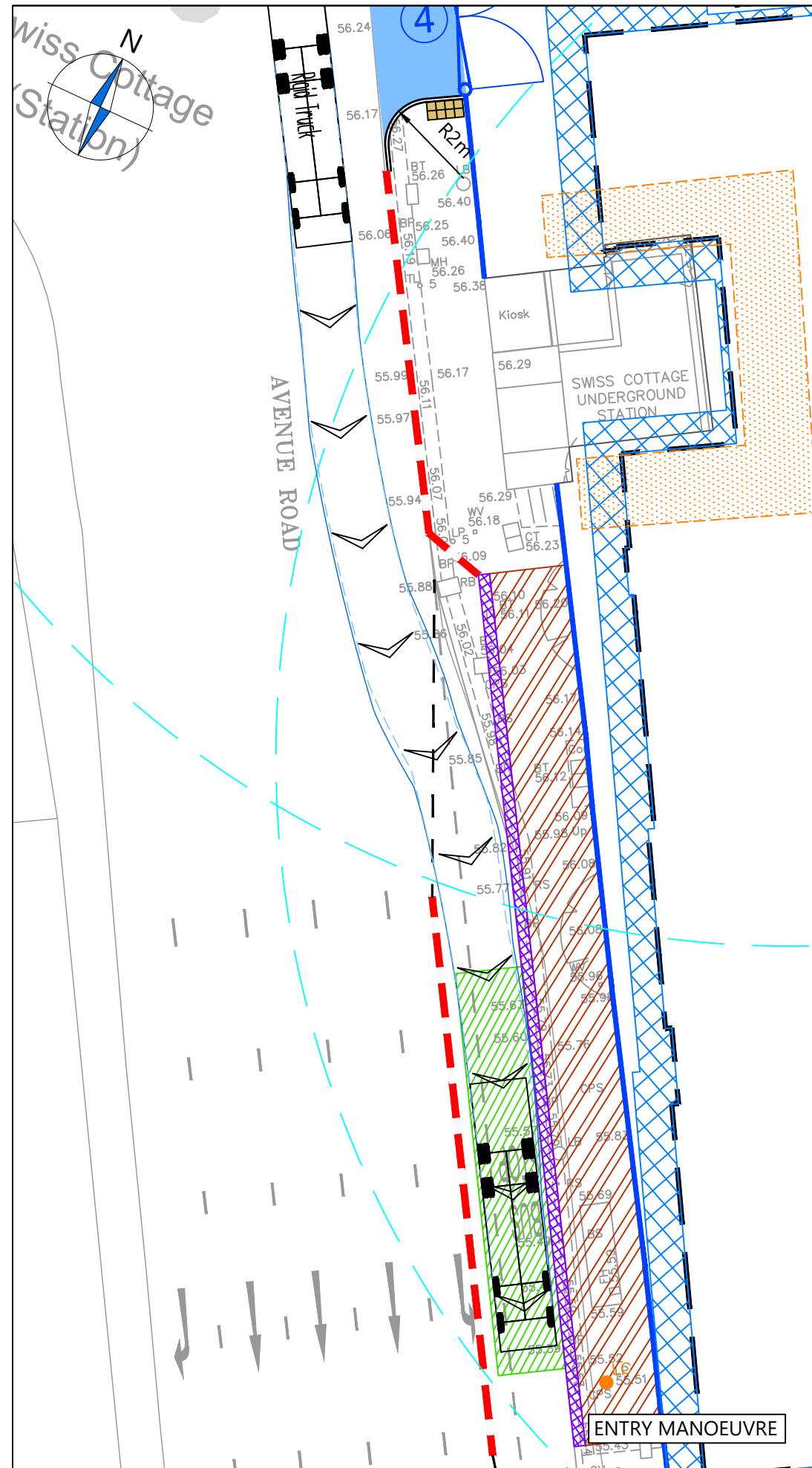
Pedestrian Dropped Crossing Standard Detail

Scale: AS SHOWN Size: A3

Drawn by: COS Checked by: RB Approved by: GS Date: 26.07.2024



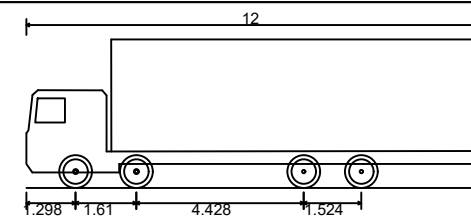
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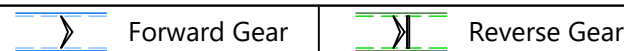
**NOTES**

1. This drawing to be read & printed in colour.
2. This drawing is for illustrative purposes only.
3. Design speed for all vehicle swept paths is 5kph.
4. Stationary steering has not been used on this drawing.

**12m RIGID TRUCK**



Overall Length	12.000m
Overall Width	2.500m
Overall Body Height	3.928m
Min Body Ground Clearance	0.412m
Track Width	2.471m
Lock to Lock Time	6.00s
Kerb to Kerb Turning Radius	11.900m



A	Updated Hoarding Line	COS	RB	07.08.2024
Rev	Details	<b>REVISION HISTORY</b>		Drawn Checked Date
Status:	<input checked="" type="checkbox"/> Preliminary	<input type="checkbox"/> Detailed	<input type="checkbox"/> As Built	

Client:  
**Regal Avenue Road Limited**

Project:  
**100 Avenue Road, Camden**

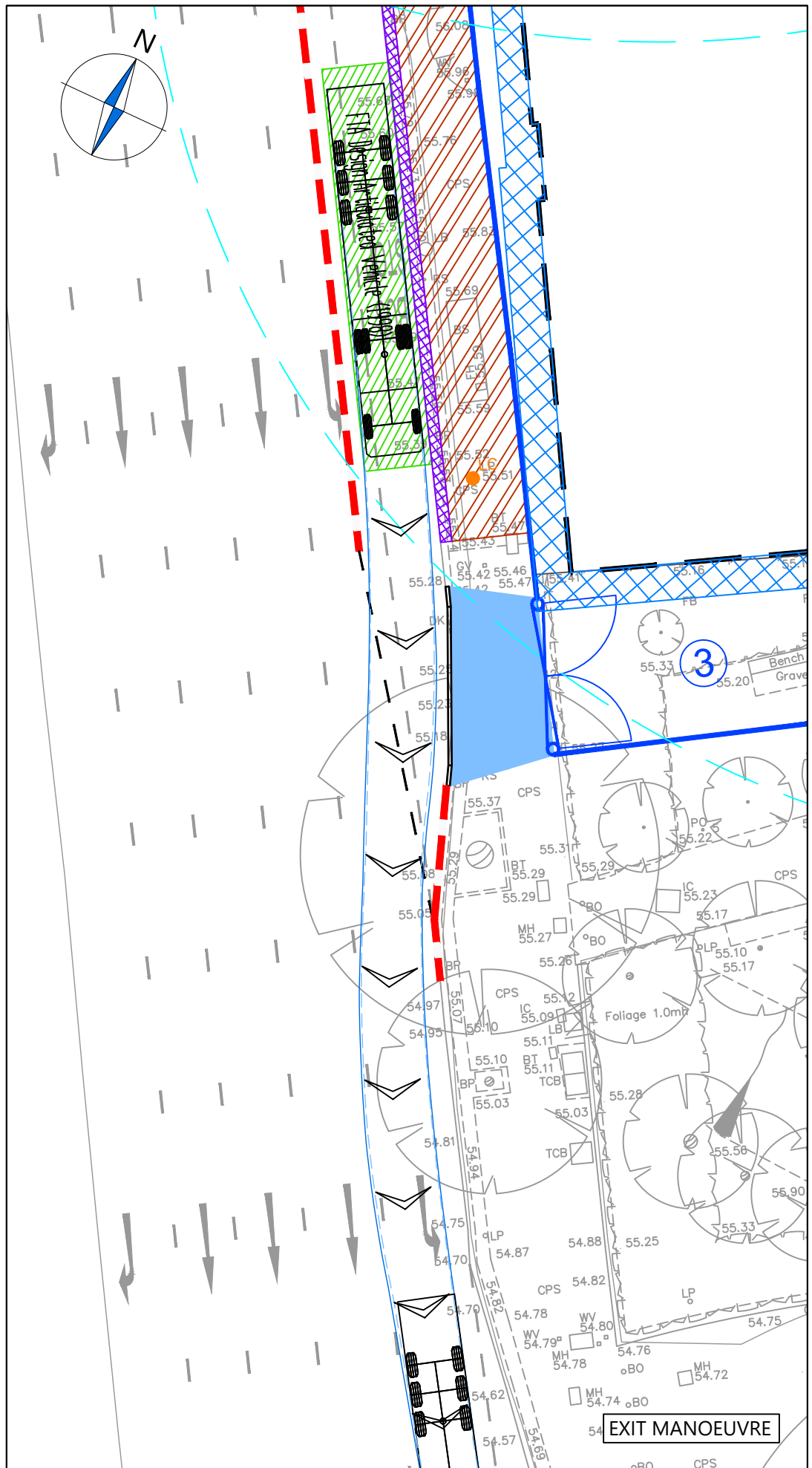
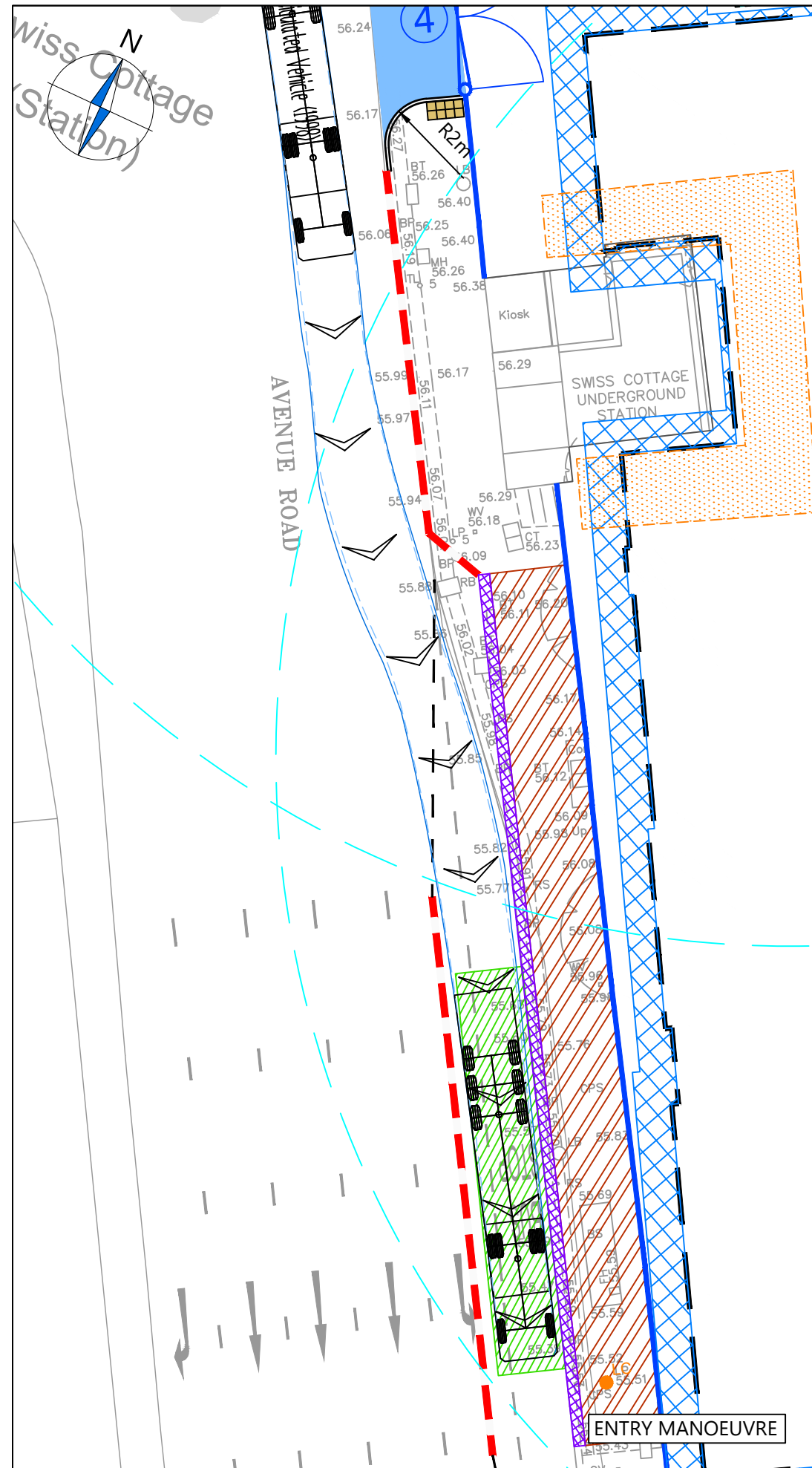
Drawing Title:  
**TMAN Application  
Vehicle Swept Path Analysis for a  
12m Rigid Truck**

Scale: **1:250** Size: **A3**

Drawn by: **COS** Checked by: **RB** Approved by: **GS** Date: **09.07.2024**



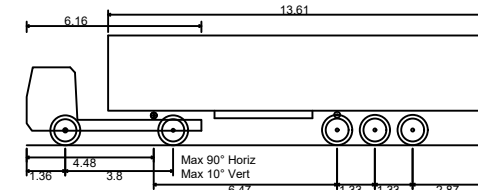
Scheme Ref: **5352** Drawing No: **TR001** Sheet: **1 of 2** Rev: **A**



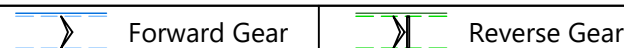
**NOTES**

1. This drawing to be read & printed in colour.
2. This drawing is for illustrative purposes only.
3. Design speed for all vehicle swept paths is 5kph.
4. Stationary steering has not been used on this drawing.

**FTA DESIGN ARTICULATED VEHICLE (1998)**



Overall Length	16.480m
Overall Width	2.550m
Overall Body Height	3.870m
Min Body Ground Clearance	0.515m
Max Track Width	2.470m
Lock to Lock Time	3.00s
Kerb to Kerb Turning Radius	6.550m



A	Updated Hoarding Line	COS	RB	07.08.2024
Rev	Details	<b>REVISION HISTORY</b>		Drawn Checked Date
Status:	<input checked="" type="checkbox"/> Preliminary	<input type="checkbox"/> Detailed	<input type="checkbox"/> As Built	

Client:  
**Regal Avenue Road Limited**

Project:  
**100 Avenue Road, Camden**

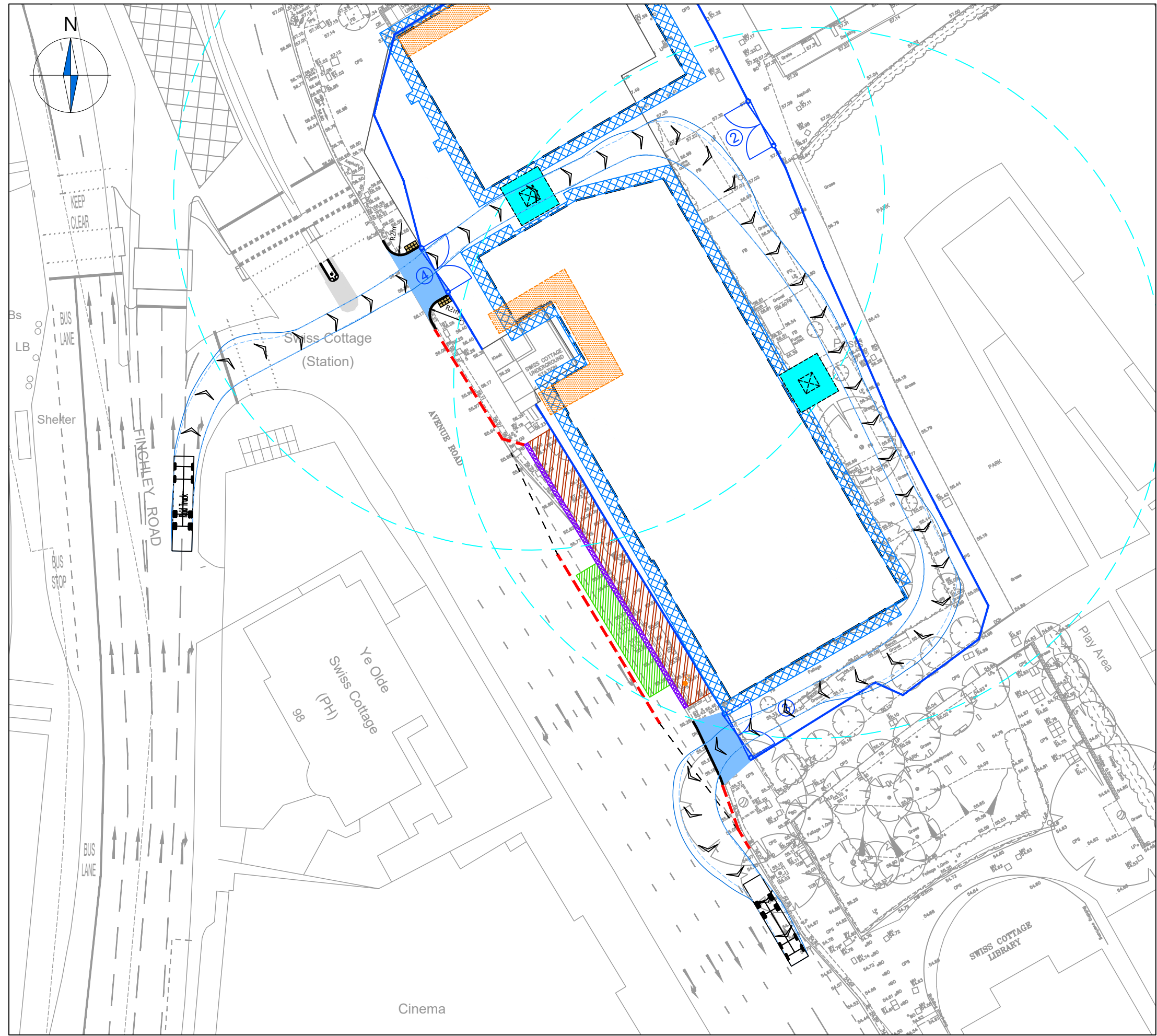
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Vehicle Swept Path Analysis for a  
FTA Design Articulated Vehicle (1998)**

Scale:	1:250	Size:	A3
Drawn by:	COS	Checked by:	RB
Approved by:	GS	Date:	09.07.2024



Scheme Ref:	Drawing No:	Sheet :	Rev:
5352	TR001	2 of 2	A

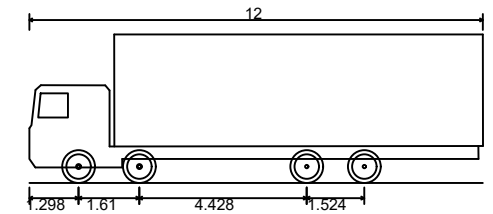




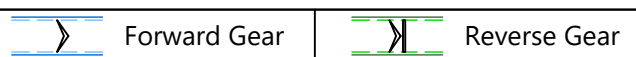
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**12m RIGID TRUCK**



Overall Length	12.000m
Overall Width	2.500m
Overall Body Height	3.928m
Min Body Ground Clearance	0.412m
Track Width	2.471m
Lock to Lock Time	6.00s
Kerb to Kerb Turning Radius	11.900m



A	Updated Hoarding Line	COS	RB	07.08.2024
Rev	Details	<b>REVISION HISTORY</b>		Drawn Checked Date
Status:	<input checked="" type="checkbox"/> Preliminary	<input type="checkbox"/> Detailed	<input type="checkbox"/> As Built	

Client:  
**Regal Avenue Road Limited**

Project:  
**100 Avenue Road, Camden**

Drawing Title:  
**TMAN Application  
Vehicle Swept Path Analysis for a  
12m Rigid Truck**

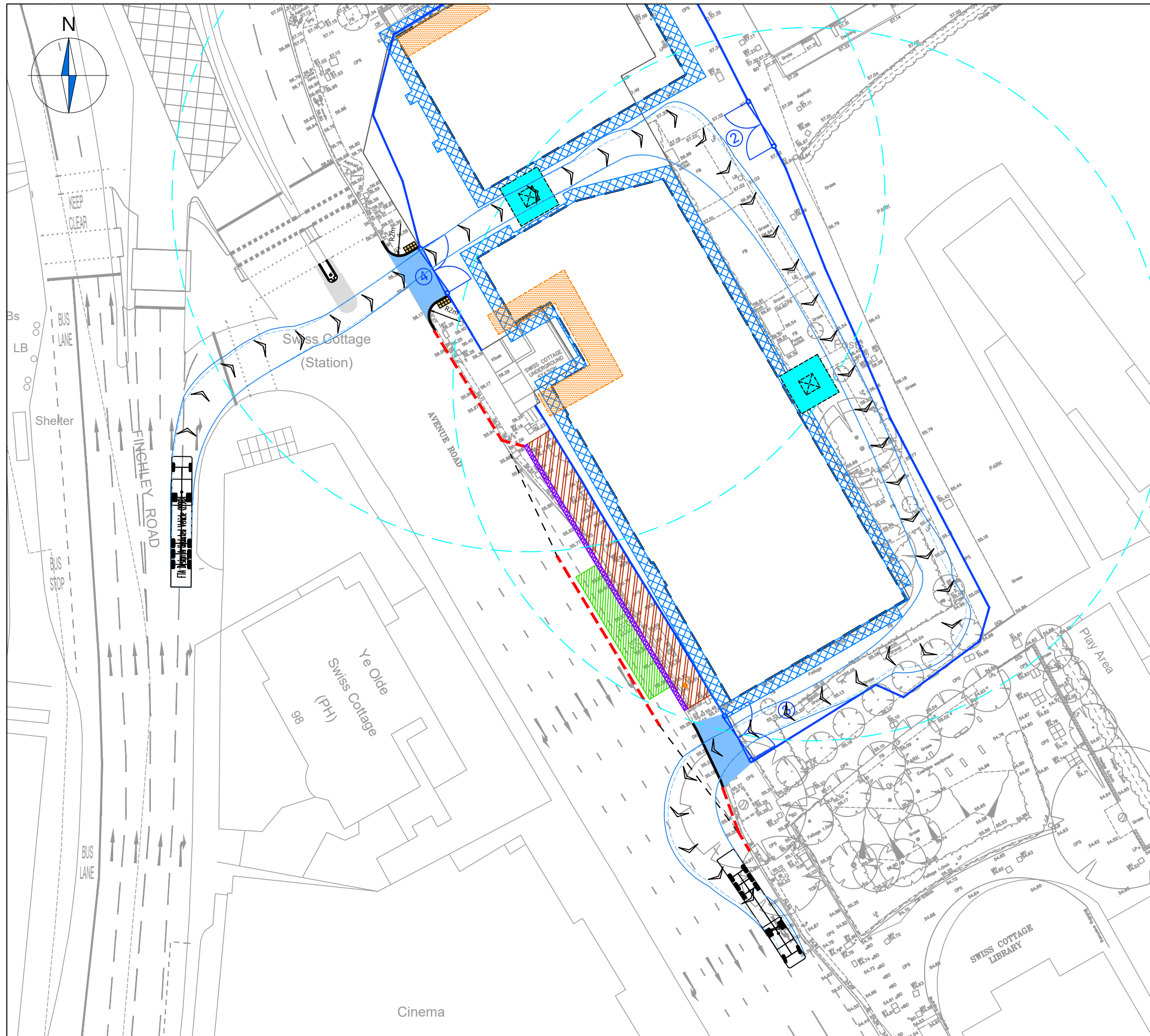
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Drawn by: COS	Checked by: RB	Approved by: GS	Date: 26.07.2024
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Transport Planning & Highway Design  
21 Little Portland Street • London • W1W 8BT • Tel. 020 3617 8200

Scheme Ref: <b>5352</b>	Drawing No: <b>TR002</b>	Sheet: <b>1 of 4</b>	Rev: <b>A</b>
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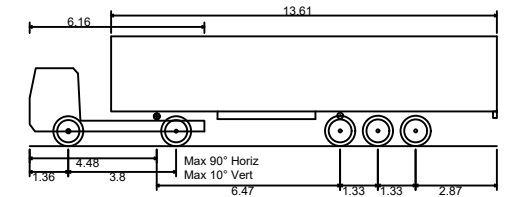
P:\2024\5352 - 100 Avenue Road, Camden\Detailed Design\TMAN And Temporary S278\CA\_5352\_TR002 - Vehicle Swept Path Analysis For Proposed General Arrangement (Inside Site) (Rev A).dwg



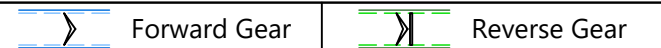
**NOTES**

1. This drawing to be read & printed in colour.
2. This drawing is for illustrative purposes only.
3. Design speed for all vehicle swept paths is 5kph.
4. Stationary steering has not been used on this drawing.

**FTA DESIGN ARTICULATED VEHICLE (1998)**



Overall Length	16.480m
Overall Width	2.550m
Overall Body Height	3.870m
Min Body Ground Clearance	0.515m
Max Track Width	2.470m
Lock to Lock Time	3.00s
Kerb to Kerb Turning Radius	6.550m



A	Updated Hoarding Line	COS	RB	07.08.2024
Rev	Details	<b>REVISION HISTORY</b>		Drawn Checked Date
Status:	<input checked="" type="checkbox"/> Preliminary	<input type="checkbox"/> Detailed	<input type="checkbox"/> As Built	

Client:  
**Regal Avenue Road Limited**

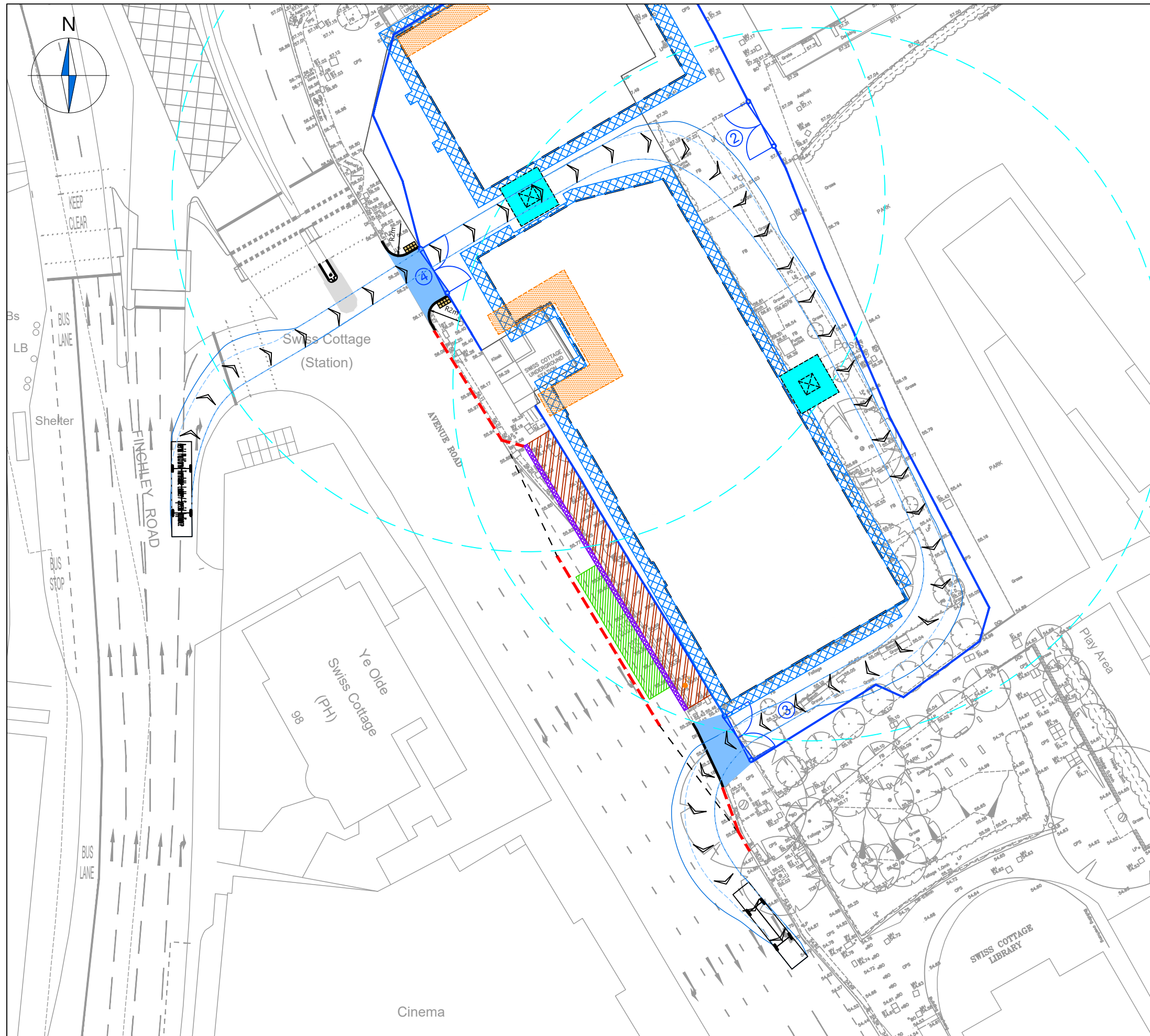
Project:  
**100 Avenue Road, Camden**

Drawing Title:  
**TMAN Application  
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Scale:	1:500	Size:	A3
Drawn by:	COS	Checked by:	RB
Approved by:	GS	Date:	26.07.2024



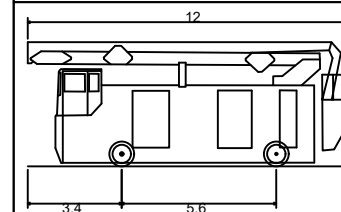
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<b>5352</b>	<b>TR002</b>	<b>2 of 4</b>	<b>A</b>



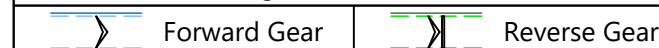
**NOTES**

1. This drawing to be read & printed in colour.
2. This drawing is for illustrative purposes only.
3. Design speed for all vehicle swept paths is 5kph.
4. Stationary steering has not been used on this drawing.

**AERIAL PLATFORM**



Overall Length	12.000m
Overall Width	2.550m
Overall Body Height	4.500m
Min Body Ground Clearance	0.130m
Track Width	2.550m
Lock to lock time	4.00s
Kerb to Kerb Turning Radius	13.750m



A	Updated Hoarding Line	COS	RB	07.08.2024	
Rev	Details		Drawn	Checked	Date

**REVISION HISTORY**

Status:  Preliminary  Detailed  As Built

Client:  
**Regal Avenue Road Limited**

Project:  
**100 Avenue Road, Camden**

Drawing Title:  
**TMAN Application  
Vehicle Swept Path Analysis for a  
Aerial Platform**

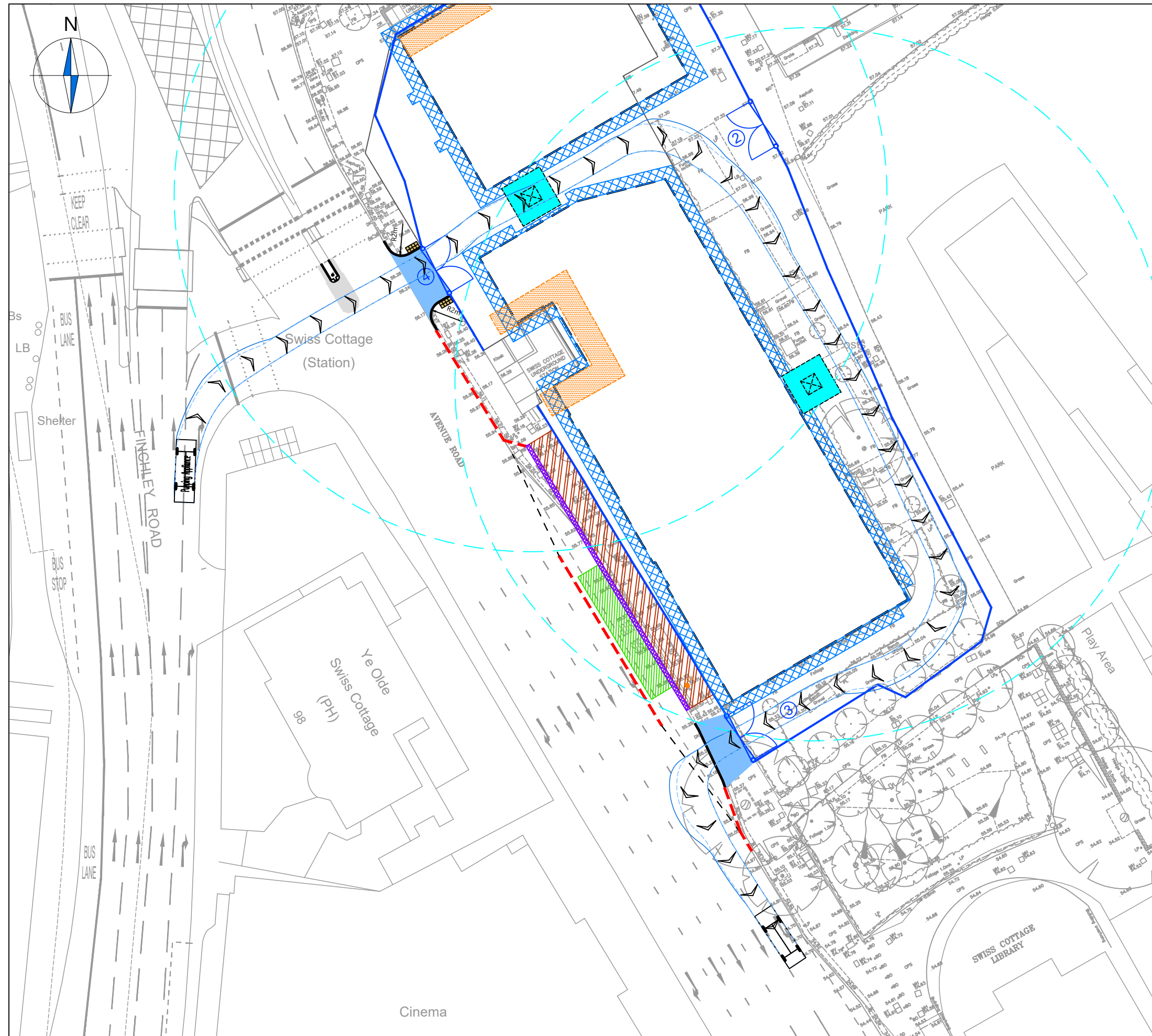
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Drawn by: COS	Checked by: RB	Approved by: GS	Date: 26.07.2024
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Scheme Ref: <b>5352</b>	Drawing No: <b>TR002</b>	Sheet: <b>3 of 4</b>	Rev: <b>A</b>
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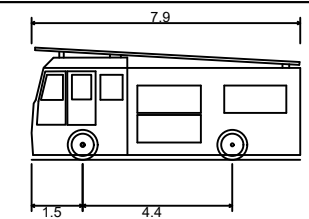
P:\2024\5352 - 100 Avenue Road, Camden\Detailed Design\TMAN And Temporary S278\CA\_5352\_TR002 - Vehicle Swept Path Analysis For Proposed General Arrangement (Inside Site) (Rev A).dwg



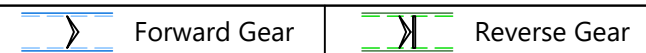
**NOTES**

1. This drawing to be read & printed in colour.
2. This drawing is for illustrative purposes only.
3. Design speed for all vehicle swept paths is 5kph.
4. Stationary steering has not been used on this drawing.

**PUMPING APPLIANCE**



Overall Length	7.900m
Overall Width	2.500m
Overall Body Height	3.300m
Min Body Ground Clearance	0.140m
Track Width	2.500m
Lock to Lock Time	4.00s
Kerb to Kerb Turning Radius	7.750m



A	Updated Hoarding Line	COS	RB	07.08.2024
Rev	Details	<b>REVISION HISTORY</b>		Drawn Checked Date
Status:	<input checked="" type="checkbox"/> Preliminary	<input type="checkbox"/> Detailed	<input type="checkbox"/> As Built	

Client:  
**Regal Avenue Road Limited**

Project:  
**100 Avenue Road, Camden**

Drawing Title:  
**TMAN Application  
Vehicle Swept Path Analysis for a  
Pumping Appliance**

Scale: **1:500** Size: **A3**

Drawn by: <b>COS</b>	Checked by: <b>RB</b>	Approved by: <b>GS</b>	Date: <b>26.07.2024</b>
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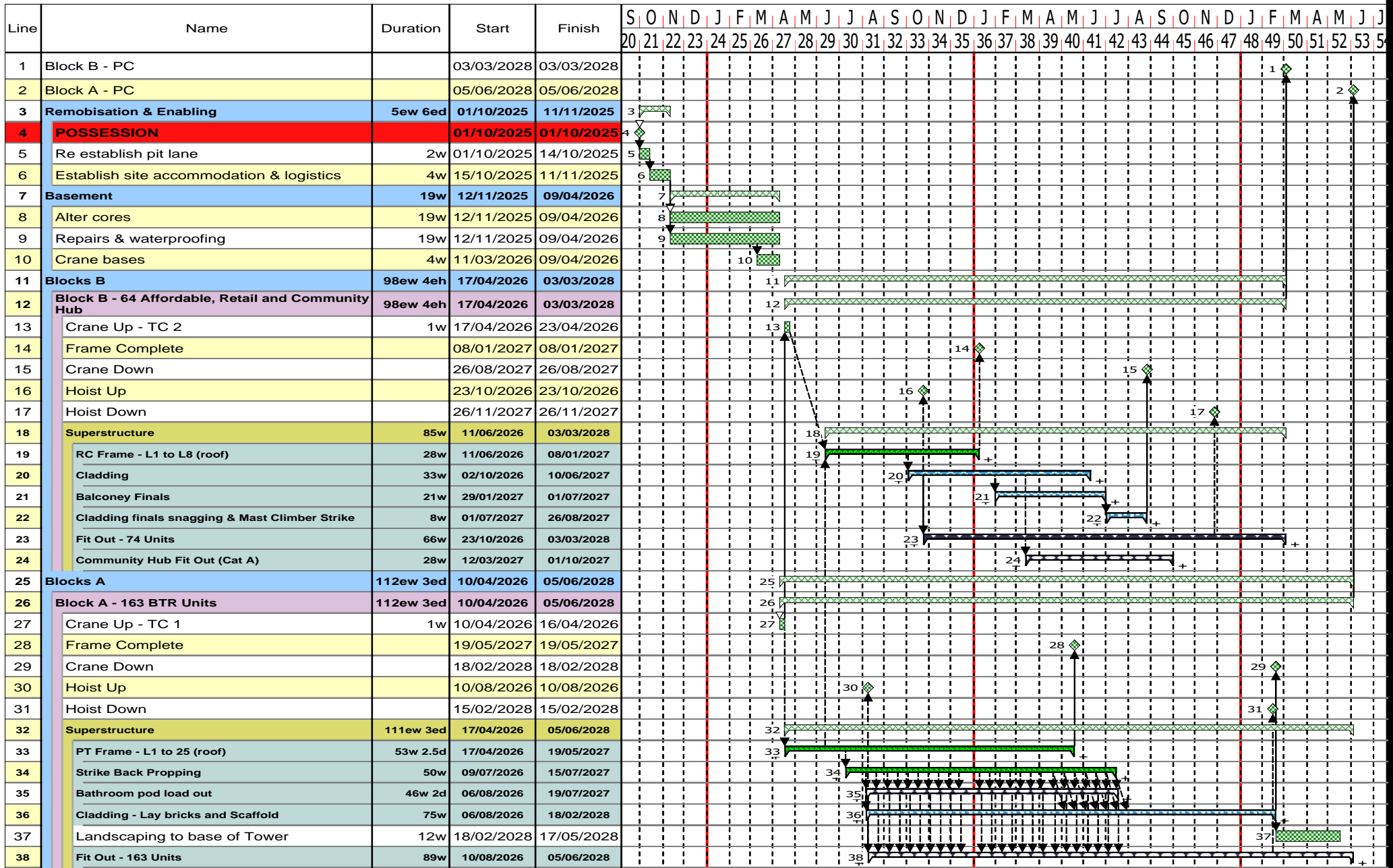
**CANEPARO ASSOCIATES**  
Transport Planning & Highway Design  
21 Little Portland Street • London • W1W 8BT • Tel. 020 3617 8200

Scheme Ref: <b>5352</b>	Drawing No: <b>TR002</b>	Sheet: <b>4 of 4</b>	Rev: <b>A</b>
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P:\2024\5352 - 100 Avenue Road, Camden\Detailed Design\TMAN And Temporary S278\CA\_5352\_TR002 - Vehicle Swept Path Analysis For Proposed General Arrangement (Inside Site) (Rev A).dwg

# Appendix B



# Appendix C



WHITECODE  
CONSULTING



REGAL  
LONDON

# Existing Utilities Report

## 100 Avenue Road

Prepared for Regal London

12077-WCL-ZZ-ZZ-SU-U-0001

Revision P01

*10 September 2024*





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Revisions:

Rev No:	Date:	Status/Comments:	Prepared by:	Checked by:
P01	09/09/2024	S4	WA	WA



## 1. Introduction

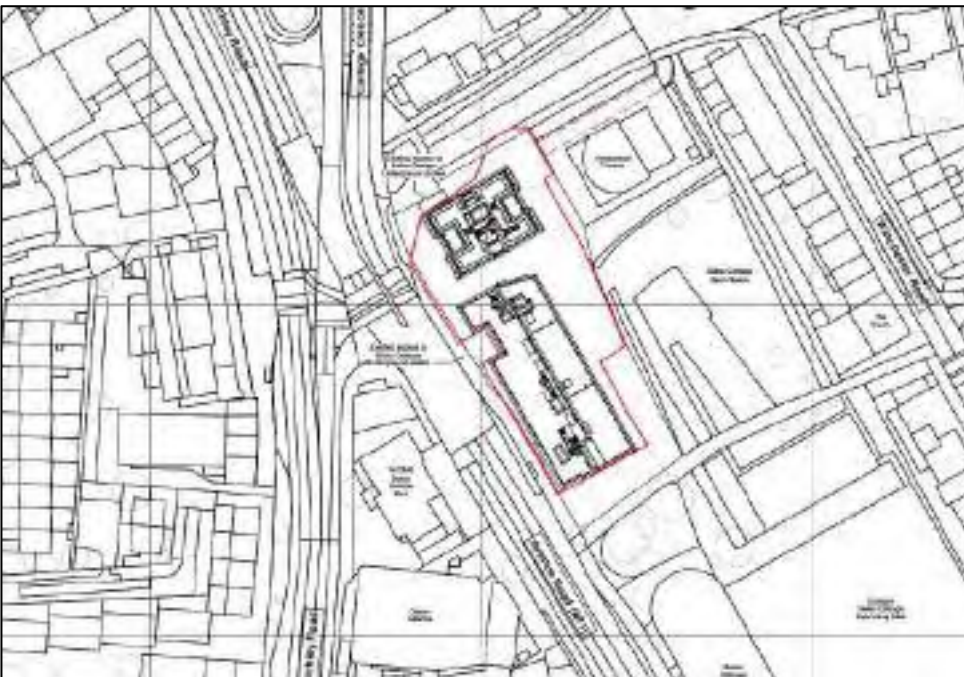
This Utilities Report has been completed to determine if there are any existing utilities on the site and if so, which provider. This utilities assessment has been prepared by Whitecode Consulting at the request of Regal for the proposed development of 100 Avenue Road, NW3 3HF – X526699 Y184317 (the 'Site').

The development comprises the comprehensive development of the Site to provide 236 flats, with a combination of flats from studios to 4 bed flat.

## 1.1. Site – Existing – 100 Avenue Road



## 1.2. Site – Proposed – 100 Avenue Road



## 2. Existing Utilities

### 2.1. Existing Services – Sitewide

The existing infrastructure records for all the utilities in the vicinity of the site boundary were procured. This was to establish the impact that any identified apparatus would have on the proposed new development, or vice versa, both within and outside the site boundary.

### 2.2. Affected Utilities

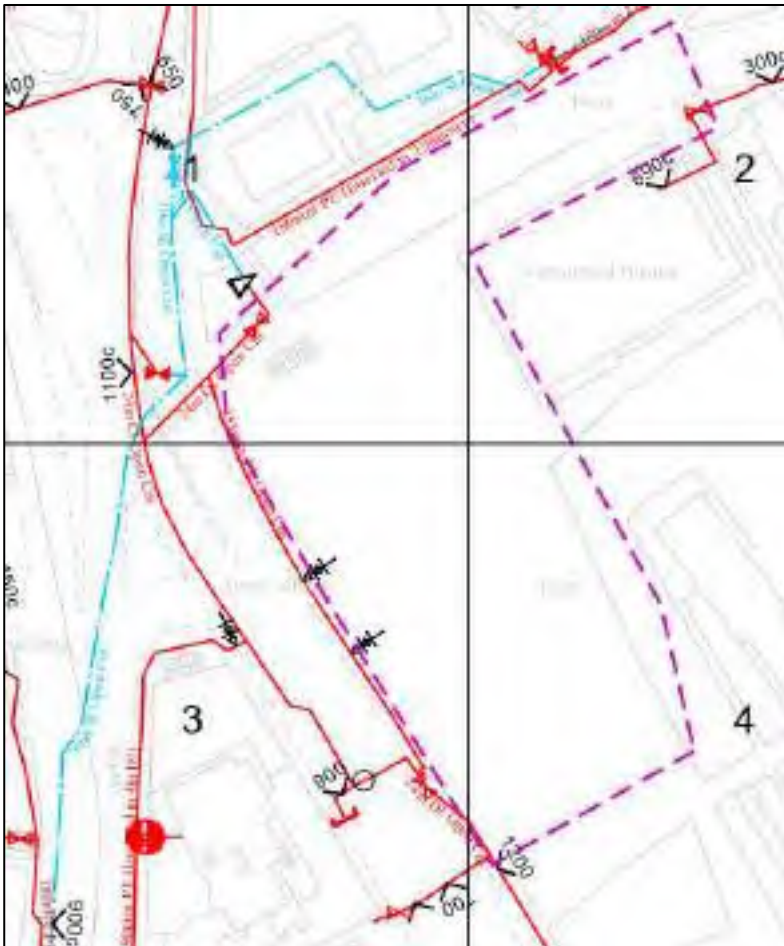
The following utilities have notified us that their apparatus is present in the vicinity:

Data Supplier	Type	Date Received	Map(s)
Cadent Gas	Gas	23.05.2024	Y
UKPN	Electric	24.06.2024	Y
National Grid	Electric	23.05.2024	Y
Thames Water	Water - Clean	29.08.2024	Y
Openreach	Telecommunications	23.05.2024	Y
Verizon	Telecommunications	06.06.2024	Y
Vodafone	Telecommunications	19.06.2024	Y
Zayo	Telecommunications	23.05.2024	Y

An outline of each affected utility is shown in more detail in the following sections.

## 1. Gas – Cadent Gas

The record plans of the existing gas distribution network have been obtained from Cadent Gas, an extract illustration of the location of the mains has been provided below:



As illustrated, there is a connection running at the North and the West of the site. These 2 pipes are close to the site and may affect the build. There is also a 24in pipe that may be in the North West corner of the proposed site.

Trial holes would be required to see where the 24in pipe at the NorthWest corner is and if it requires a diversion. For the other 2 mains, I would recommend to carry out diversion applications and to keep in touch with Cadent Gas to ensure the safety of the pipes.

## 2. Electric – UKPN

Record plans of the existing electrical distribution network have been obtained from UK Power Networks which illustrates that there are high voltage (HV) and low voltage (LV) network as well as two substations, in the vicinity of the site serving existing properties as shown in the plans below.



Both substations are shown to be on the proposed site, although one appears to be disconnected. A site survey will need to be conducted to see their usage. There are also HV lines running on the North and West of the site along the road.

Relocation of the substations are likely, applications to UKPN will be required.

Although the HV cables are not on site, Diversionary works may be required, an application will need to be made to ensure the safety of the cables.

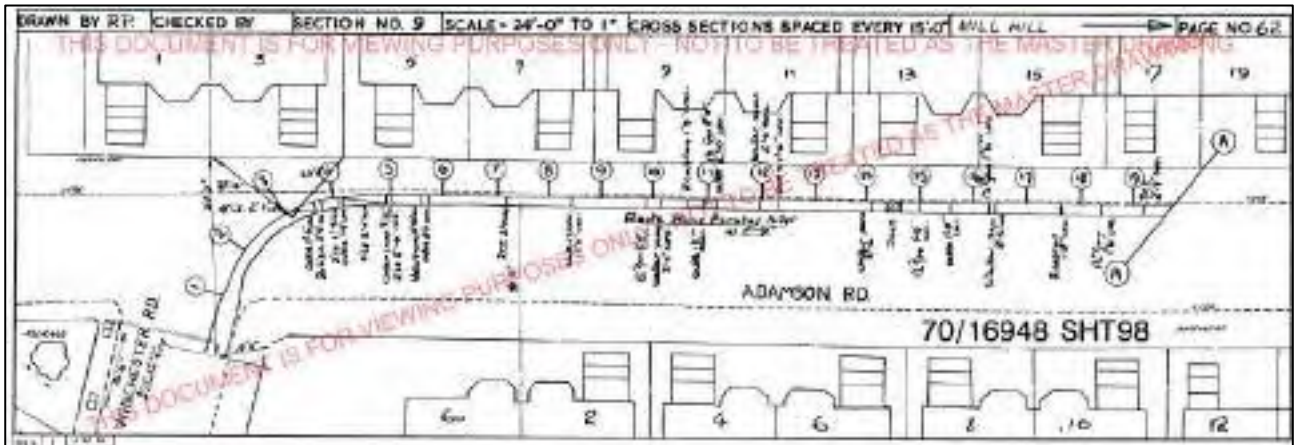
Disconnections may be required.

A budget application has been conducted. UKPN have confirmed that there is enough capacity to serve the site, at a cost of £350,000 + VAT. This was based on an electrical load of 1328kVA.

The Point of Connection is about 20 meters away on Avenue Road.

### 3. Electric – National Grid

Record plans of the existing electrical distribution network have been obtained from National Grid which illustrates that there are network cables in the vicinity of the site as shown in the plans below.



As per the illustration, you will see the location of the cables on Adamson Road and Winchester Road

Although we do not anticipate a diversion to be required, they will need to be contacted to ensure that their cables are safe from construction.



## 4. Water – Clean – Thames Water

The asset record plan provided by Thames Water illustrates existing plant within the proposed site as shown below:



Thames Water has water mains running close to the site. The pipes are shown to be on the West of the site, on the other side of the road, with fire hydrants close by.

Based on the plans and the map, we do not anticipate any diversion works.

Disconnection applications will need to be made for the existing buildings.

## 5. Telecommunications – Openreach

The asset record plan provided by Openreach illustrates existing plant within the proposed site as shown below:



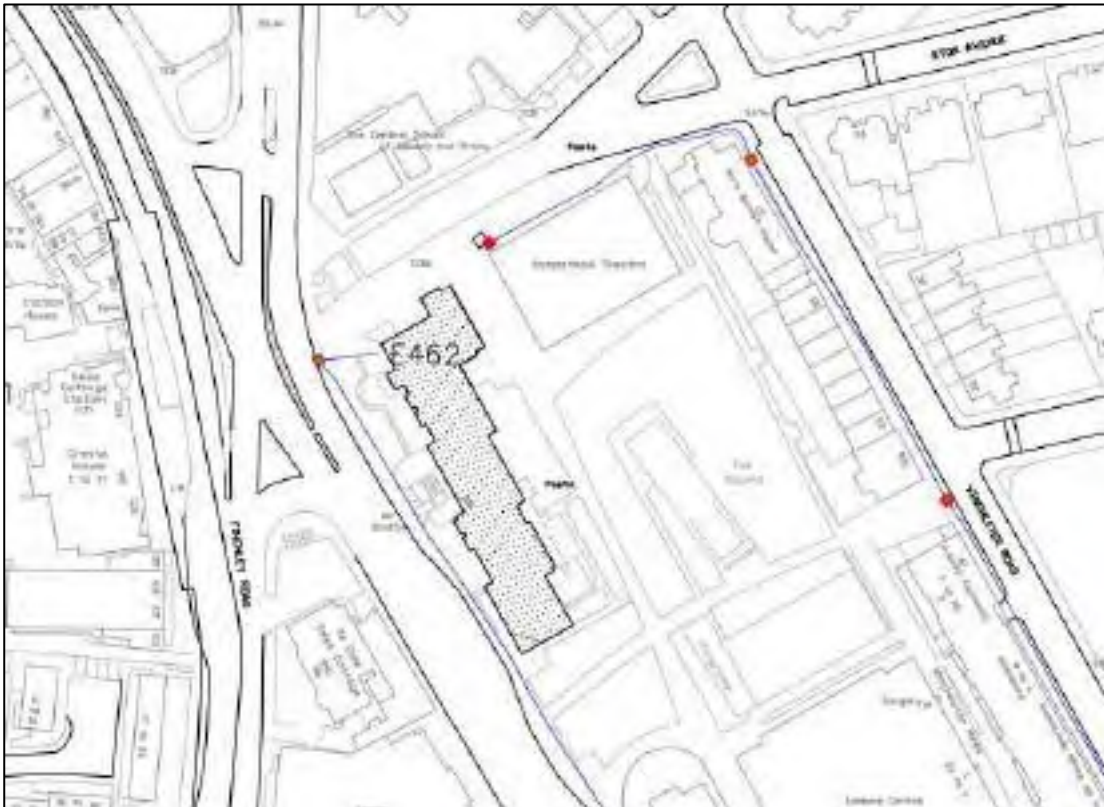
Openreach plant runs along Avenue Road and Adamson Road. They show to serve the buildings on the proposed site as well as Hampstead Theatre.

Diversions and disconnections will need to be applied for once planning has been agreed. A site visit will be required once the applications have been made.

New connections can potentially be taken from the existing mains in the vicinity of the site due to the proximity of the Openreach plant location to the site. These connections will be Giga-Bit ready to satisfy Approved Document Part R.

## 6. Telecommunications – Verizon

The asset record plan provided by Verizon illustrates existing plant around the proposed site as shown below:



Verizon plants run in the road on Avenue Road and into the proposed site at the Northwest corner.

Disconnections will be required as they seem to be serving the existing building.

Diversions works are not anticipated but Verizon may require contact to ensure their cables are safe.

## 7. Telecommunications – Vodafone

The asset record plan provided by Vodafone illustrates existing plant around the proposed site as shown below:

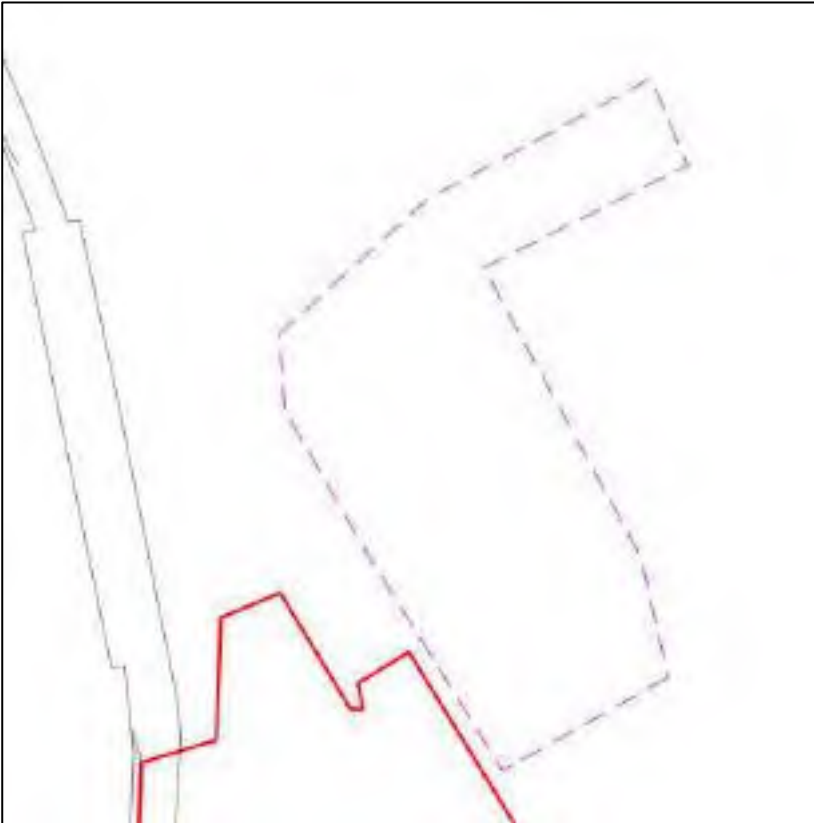


Vodafone plants run in the road on Avenue Road on the West of the proposed site.

Diversions works are not anticipated but Vodafone may require contact to ensure their cables are safe.

## 8. Telecommunications – Zayo

The asset record plan provided by Zayo illustrates existing plant around the proposed site as shown below:



Zayo plants run in the road on Grove Road on the West and Southwest side of the proposed site.

Diversions works are not anticipated but Zayo may require contact to ensure their cables are safe.

## 9. Unaffected Utilities

The following utilities have notified us that their apparatus is not present:

Data Supplier	Date of Search
Angus Energy	23.05.2024
AWE Pipeline	23.05.2024
B & D Energy Limited	23.05.2024
Balfour Beatty Investments Limited	23.05.2024
BOC Limited (A Member of the Linde Group)	23.05.2024
Box Broadband	23.05.2024
BP Exploration Operating Company Limited	23.05.2024
BPA	23.05.2024
CA Telecom	07.06.2024
Cambridge Water	23.05.2024
Cambridgeshire County Council Climate Change and Energy Services	23.05.2024
CATS Pipeline c/o Wood Group PSN	23.05.2024
Cemex	23.05.2024
Centrica Storage Ltd	23.05.2024
City Fibre	03.06.2024
CNG Services Ltd	23.05.2024
Concept Solutions People Ltd	23.05.2024
ConocoPhillips (UK) Teesside Operator Ltd	23.05.2024
D.S.Smith	23.05.2024
Diamond Transmission Corporation	23.05.2024



DIO (MOD Live Pipelines)	23.05.2024
Drax Power Limited	23.05.2024
E.ON UK CHP Limited	23.05.2024
EDF Energy Renewables Ltd	23.05.2024
EirGrid	23.05.2024
Eleclink Limited	23.05.2024
Electricity North West Limited	23.05.2024
Energy Assets Networks	23.05.2024
ENI & Himor c/o Penspen Ltd	23.05.2024
EnQuest NNS Limited	23.05.2024
EP Langage Limited	23.05.2024
Equans	31.05.2024
ESB CCGT Power station (Carrington Gas Pipeline)	23.05.2024
ESP Utilities Group	23.05.2024
ESSAR	23.05.2024
Esso Petroleum Company Limited	23.05.2024
euNetworks Fiber UK Ltd	23.05.2024
EXA Infrastructure	23.05.2024
Exolum Pipeline System	23.05.2024
Fulcrum Electricity Assets Limited	23.05.2024
Fulcrum Pipelines Limited	23.05.2024
Gamma	23.05.2024
Gas Networks Ireland (UK)	23.05.2024
Gateshead Energy Company	23.05.2024



Gigaclear Ltd	23.05.2024
Harbour Energy	23.05.2024
Heathrow Airport LTD	23.05.2024
Humbly Grove Energy	23.05.2024
IGas Energy	23.05.2024
INEOS FPS Pipelines	23.05.2024
INEOS Manufacturing (Scotland and TSEP)	23.05.2024
INOVYN ChlorVinyls Limited	23.05.2024
INOVYN Enterprises Limited	23.05.2024
Intergen (Coryton Energy or Spalding Energy)	23.05.2024
Jurassic Fibre Ltd	23.05.2024
Kensa Utilities	23.05.2024
Last Mile	23.05.2024
Mainline Pipelines Limited	23.05.2024
Manchester Jetline Limited	23.05.2024
Manx Cable Company	23.05.2024
Marchwood Power Ltd (Gas Pipeline)	23.05.2024
Melbourn Solar Limited	23.05.2024
MUA Group Limited	23.05.2024
National Gas Transmission	23.05.2024
National Grid Electricity Distribution	23.05.2024
National Grid Ventures	23.05.2024
Neos Networks	23.05.2024
Northern Gas Networks Limited	23.05.2024





Northumbrian Water Group	23.05.2024
NPower CHP Pipelines	23.05.2024
NTT Global Data Centers EMEA UK Ltd	23.05.2024
NYnet Ltd	23.05.2024
OCU Group	07.06.2024
Ogi	23.05.2024
Oikos Storage Limited	23.05.2024
Ørsted	23.05.2024
Palm Paper Ltd	23.05.2024
Perenco UK Limited (Purbeck Southampton Pipeline)	23.05.2024
Petroineos	23.05.2024
Phillips 66	23.05.2024
Portsmouth Water	23.05.2024
Premier Transmission Ltd (SNIP)	23.05.2024
Redundant Pipelines - LPDA	23.05.2024
RWE - Great Yarmouth Pipeline (Bacton to Great Yarmouth Power Station)	23.05.2024
RWEnpower (Little Barford and South Haven)	23.05.2024
SABIC UK Petrochemicals	23.05.2024
SAS Utility Services Ltd	23.05.2024
Scottish and Southern Electricity Networks	23.05.2024
Scottish Power Generation	23.05.2024
Seabank Power Ltd	23.05.2024
SES Water	23.05.2024
SGN	23.05.2024



Shell	23.05.2024
Shell NOP	23.05.2024
South Staffs Water	23.05.2024
SP Energy Networks	23.05.2024
Spring Fibre Limited	23.05.2024
Squire Energy Networks	23.05.2024
SSE Generation Ltd	23.05.2024
SSE Transmission	23.05.2024
SSE Utility Solutions Limited	23.05.2024
Storengy	23.05.2024
Tata Communications (c/o JSM Construction Ltd)	23.05.2024
TCROW	04.06.2024
TfL – London Underground HV Cables (Road Side Cables)	23.05.2024
toob Limited	23.05.2024
Total Colnbrook Pipelines	23.05.2024
Total Finaline Pipelines	23.05.2024
Transmission Capital	23.05.2024
Trojan Energy Limited	23.05.2024
Uniper UK Ltd	23.05.2024
University of Cambridge Granta Backbone Network	23.05.2024
Vattenfall	23.05.2024
Veolia ES SELCHP Limited	23.05.2024
Veolia ES Sheffield Ltd	23.05.2024
Voneus Limited	23.05.2024



VPI Power Limited	23.05.2024
Wales and West Utilities	23.05.2024
Welsh Power	23.05.2024
West of Duddon Sands Transmission Ltd	23.05.2024
West Sussex OpenNetwork (Cooperative National Infrastructure)	23.05.2024
Westminster City Council	23.05.2024



## 10. Appendices – Utilities Research

### 10.1. Appendix 1 – Cadent Gas

**From:** [plantprotection@cadentgas.com](mailto:plantprotection@cadentgas.com)  
**Sent:** 23 May 2024 10:13  
**To:** [William Auloni](#)  
**Subject:** LSBUD Ref: 33506271 Your Ref: 100 Avenue Road Medium Risk  
**Attachments:** [33506271\\_CadentGas.pdf](#)

---

Date: 23/05/2024  
LinesearchbeforeUdig ref: 33506271  
Your ref: 100 Avenue Road

Dear Sir/Madam,

**Your planned works may proceed with caution.**

We have received a notification from the LinesearchbeforeUdig (LSBUD) platform regarding your plans to carry out works. Based on the information you provided:

**Although there are Cadent gas pipes in the area you're planning to work, you can proceed with your planned work with caution and in line with the guidance. This outcome is based on the information you gave us. If your plans change, or you are unable to work to the guidance please let us know as soon as possible so we can work with you to find a solution.**

**If your works are being carried out in an easement, please ensure that you contact us to obtain formal, signed written consent which will only be provided following consultation with our plant protection team.**

Please find enclosed a copy of our plans for the area and ensure you follow our key guidance document [Specification for Safe Working in the Vicinity of Cadent Assets](#).

**What you need to do**

**BEFORE** carrying out any work you must:

- Carefully read the guidance documents and maps showing the location of our gas apparatus.
- Contact the landowner and ensure any proposed works in private land do not infringe on Cadent's legal rights (i.e. easements or other rights). If the works are in the road or footpath, the relevant local authority should be contacted.
- Ensure that all persons, including direct labour and contractors, working for you on or near Cadent apparatus follow the requirements of the HSE Guidance Notes HSG47 'Avoiding danger from underground services'. This guidance can be downloaded free of charge at [www.hse.gov.uk](http://www.hse.gov.uk)
- In line with the guidance, verify and establish the actual position of gas mains, pipes, services and other apparatus on site before any activities are undertaken.

**DURING** any work you must:

- Comply with all guidance and any further guidance from our specialist team.
- Ensure that access to Cadent apparatus is maintained at all times.
- Prevent the placing of heavy construction plant, equipment, materials or the passage of heavy vehicles over Cadent apparatus unless specifically agreed with us in advance.
- Exercise extreme caution if slab (mass) concrete is encountered during excavation works as this may be protecting or supporting Cadent apparatus.
- Maintain appropriate clearances between gas apparatus and the position of other buried plant.

This outcome is valid for 28 days from the date of this letter. If your work, or the location, date or nature of the work you're doing changes, you must submit another enquiry.

If you need any further information or have any questions about this letter, please contact us at [plantprotection@cadentgas.com](mailto:plantprotection@cadentgas.com) or on **0800 688 588** quoting your reference at the top of this letter.

### **Your responsibilities and obligations**

It is your responsibility to ensure that the information you have given us is accurate. You must also share all relevant documents, including the guidance notes, with anyone who carries out work on your behalf.

This assessment solely relates to Cadent gas pipes. It doesn't include:

- Cadent's legal interest (easements or other rights) in the land which restricts activity near Cadent's pipes in private land. You must get details of any such restrictions from the landowner in the first instance and if in doubt contact us on **0800 688 588** or at
- Gas service pipes and related apparatus.
- Recently installed apparatus.
- Apparatus owned by other organisations such as other gas distribution networks, local electricity companies and other utilities.

It is your responsibility to consider whether the points above are relevant to you and whether they could be affected by your proposed work.

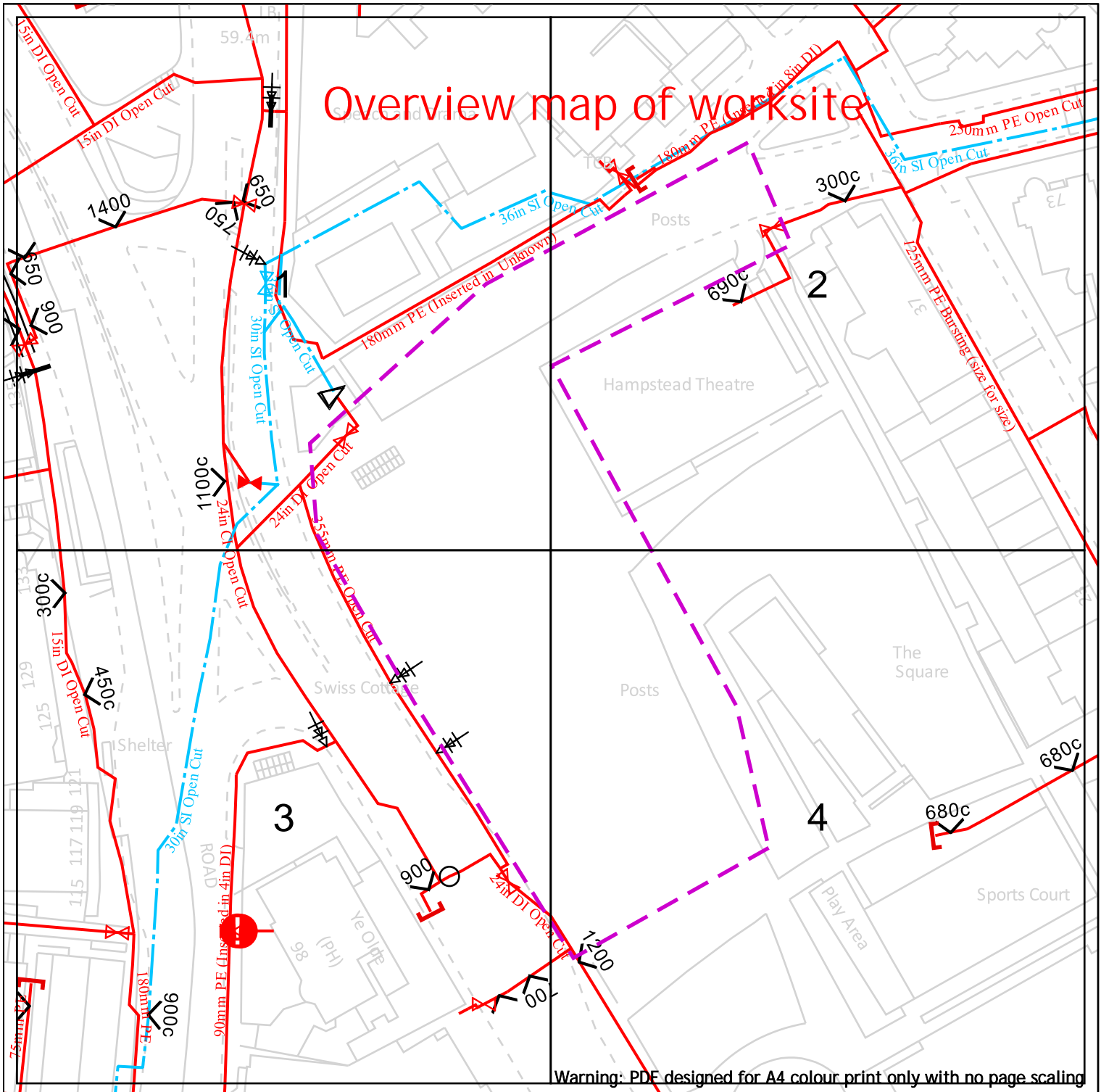
This letter does not constitute any formal agreement or consent for any proposed development work either generally or related to Cadent's easements or other rights, or any planning or building regulations applications.

Cadent Gas Ltd or their agents, servants or contractors do not accept any liability for any losses arising under or in connection with this information. This limit on liability applies to all and any claims in contract, tort (including negligence), misrepresentation (excluding fraudulent misrepresentation), breach of statutory duty or otherwise. This limit on liability does not exclude or restrict liability where prohibited by the law nor does it supersede the express terms of any related agreements.

**Kind Regards,**  
Plant Protection Team  
T: 0800 688 588  
[plantprotection@cadentgas.com](mailto:plantprotection@cadentgas.com)  
[cadentgas.com](http://cadentgas.com)

**We work together | We take responsibility | We drive performance | We shape the future**  
**Keeping people warm, *while protecting the planet***

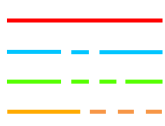
# Overview map of worksite



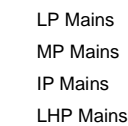
Warning: PDF designed for A4 colour print only with no page scaling



### Dig Sites



### Area:



### Line:



Where an out-of-standard service is present please contact plant protection 0800 688 588 for further information.

Out-of-standard service symbology may be used to indicate one or more of the following:

- Identification of shallow services
  - Identification of dual services
  - Recording of non-perpendicular services
- A perpendicular service/connection is laid in a straight line from the entry point at the property to the connection at the main – this includes a pipe that has a perpendicular connection and is not straight but does not deviate more than 1m off centre line.

Date Requested: 23/05/2024  
 Job Reference: 33506271  
 Site Location: 526713 184304  
 Requested by: Mr William Auloni

Your Scheme/Reference: 100 Avenue Road

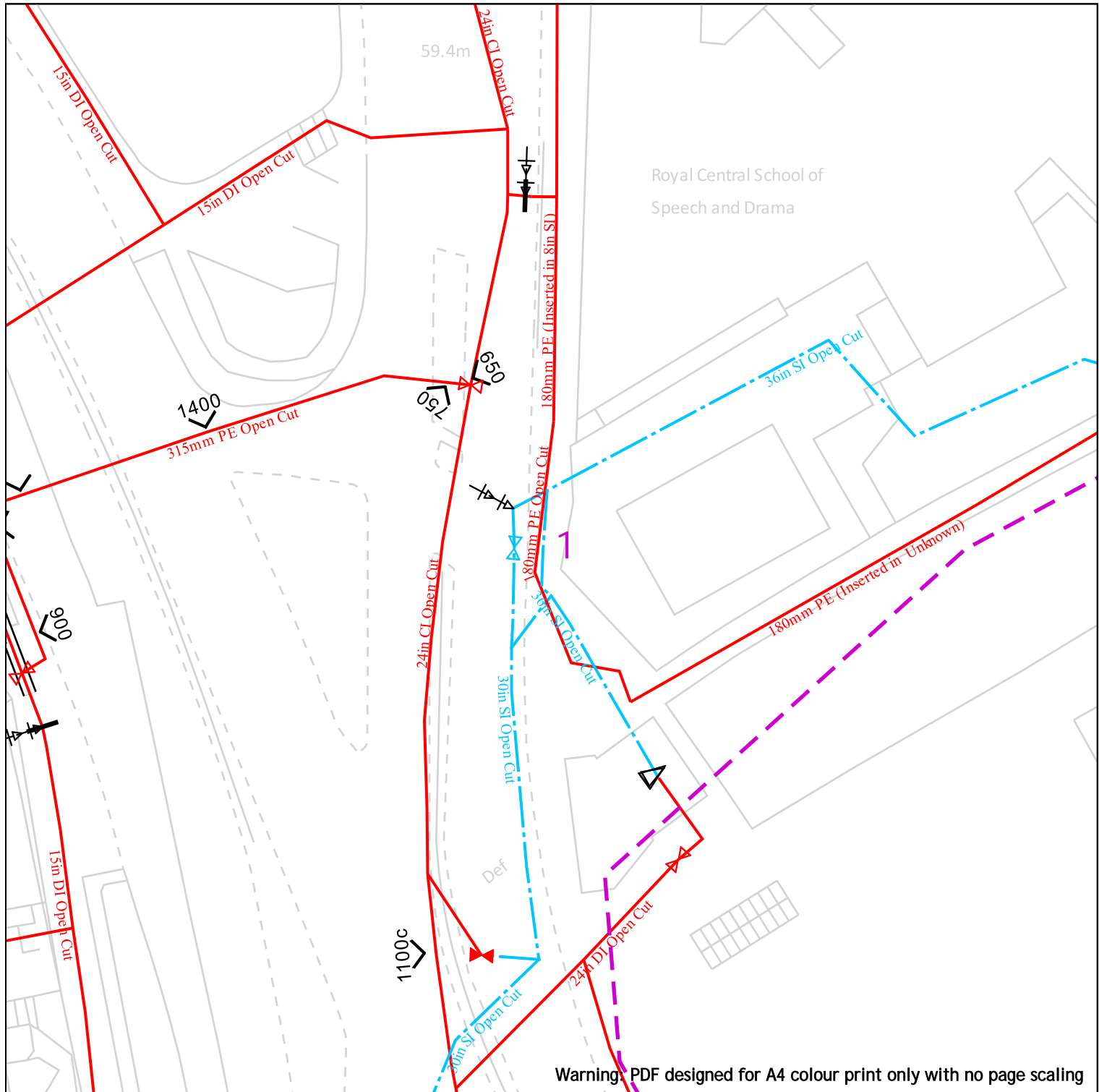
View extent: 100m, 100m

### IMPORTANT NOTICES

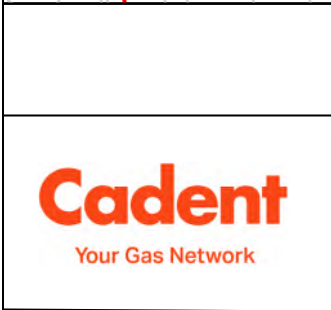
This plan shows these pipes owned by Cadent Gas Limited in its role as a Licensed Gas Transporter (GT). Gas pipes owned by other GT's or otherwise privately owned may be present in this area. Information with regards to such pipes should be obtained from the relevant owners. The information shown on this plan is given without warranty, the accuracy thereof cannot be guaranteed. Service pipes, valves, syphons, stub connections etc. are not shown but their presence should be anticipated. No liability of any kind whatsoever is accepted by Cadent Gas Limited or their agents, servants or contractors for any errors or omission. Safe digging practices, in accordance with HS(G)47, must be used to verify and establish the actual position of mains, pipes, services and other apparatus on site before any mechanical plant is used. It is your responsibility to ensure that this information is provided to all persons (either direct labour or contractors) working for you on or near gas apparatus. The information included on this plan should not be referred to beyond a period of 28 days from the date of issue.

**In case of emergency call - 0800 111 999**

Scale: 1:1025 (When plotted at A4)



Warning: PDF designed for A4 colour print only with no page scaling



Date Requested: 23/05/2024  
 Job Reference: 33506271  
 Site Location: 526713 184304  
 Requested by: Mr William Auloni  
 Your Scheme/Reference: 100 Avenue Road  
 View extent: 100m, 100m

Scale: 1:500 (When plotted at A4)

25m

<b>Dig Sites</b>	<b>Area:</b>	<b>Line:</b>
	LP Mains	
	MP Mains	
	IP Mains	
	LHP Mains	

Where an out-of-standard service is present please contact plant protection 0800 688 588 for further information.  
 Out-of-standard service symbology may be used to indicate one or more of the following:

- Identification of shallow services
- Identification of dual services
- Recording of non-perpendicular services

A perpendicular service/connection is laid in a straight line from the entry point at the property to the connection at the main – this includes a pipe that has a perpendicular connection and is not straight but does not deviate more than 1m off centre line.

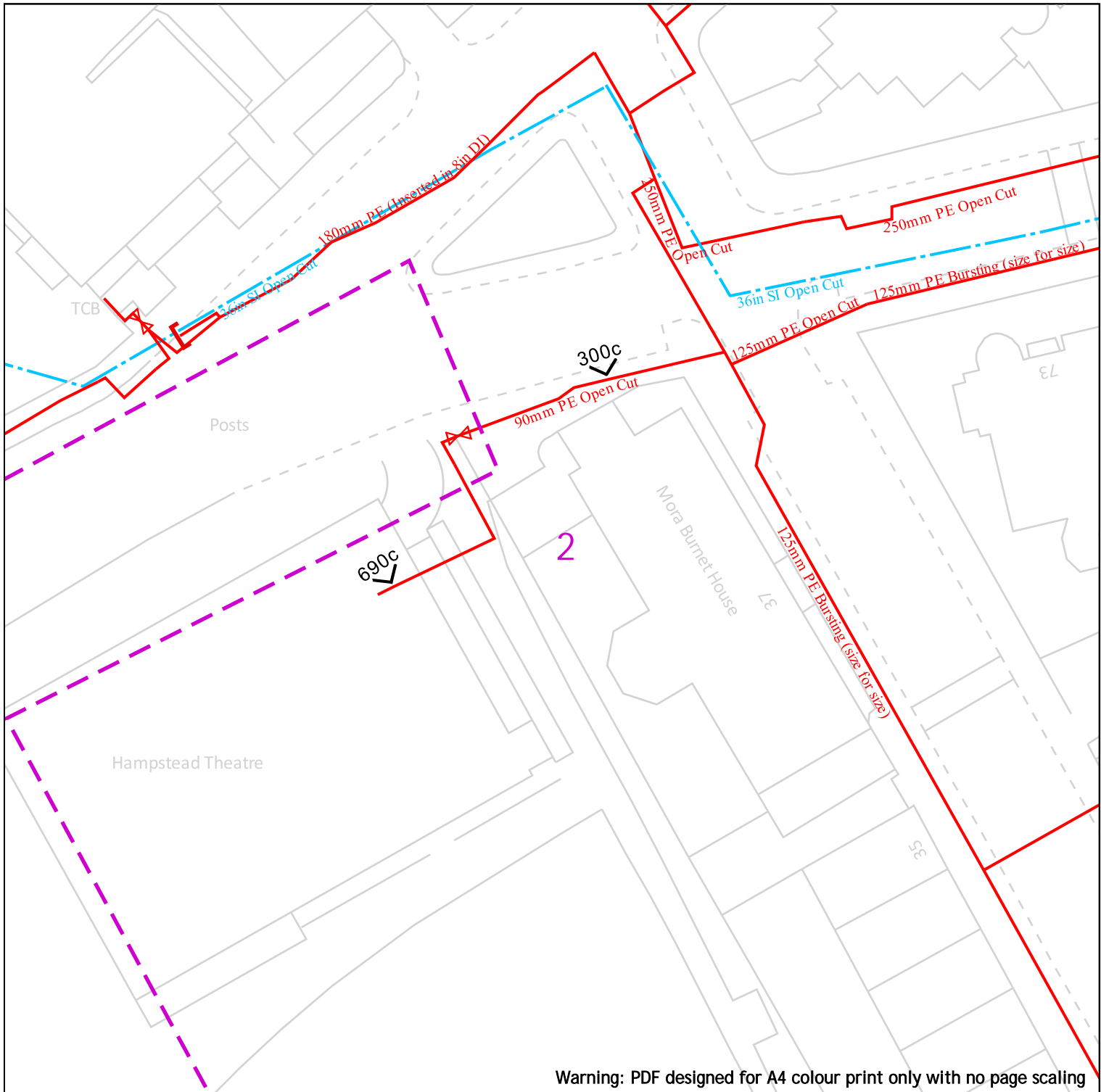
**IMPORTANT NOTICES**

This plan shows these pipes owned by Cadent Gas Limited in its role as a Licensed Gas Transporter (GT). Gas pipes owned by other GT's or otherwise privately owned may be present in this area. Information with regards to such pipes should be obtained from the relevant owners. The information shown on this plan is given without warranty, the accuracy thereof cannot be guaranteed. Service pipes, valves, syphons, stub connections etc. are not shown but their presence should be anticipated. No liability of any kind whatsoever is accepted by Cadent Gas Limited or their agents, servants or contractors for any errors or omission. Safe digging practices, in accordance with HS(G)47, must be used to verify and establish the actual position of mains, pipes, services and other apparatus on site before any mechanical plant is used. It is your responsibility to ensure that this information is provided to all persons (either direct labour or contractors) working for you on or near gas apparatus. The information included on this plan should not be referred to beyond a period of 28 days from the date of issue.

**In case of emergency call - 0800 111 999**

Crown Copyright © - This plan is reproduced from or based on the OS map by Cadent Gas Limited, with the sanction of the controller of HM Stationary Office. Crown Copyright Reserved. Ordnance Survey Licence number 100024886





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Dig Sites

Area:

Line:

**Cadent**  
Your Gas Network

LP Mains  
 MP Mains  
 IP Mains  
 LHP Mains

Valve

Depth of cover

Syphon

Diameter Change

Material Change

Out of Standard Service

Where an out-of-standard service is present please contact plant protection 0800 688 588 for further information.

Out-of-standard service symbology may be used to indicate one or more of the following:

- Identification of shallow services
- Identification of dual services
- Recording of non-perpendicular services

A perpendicular service/connection is laid in a straight line from the entry point at the property to the connection at the main – this includes a pipe that has a perpendicular connection and is not straight but does not deviate more than 1m off centre line.

Date Requested: 23/05/2024  
Job Reference: 33506271  
Site Location: 526713 184304  
Requested by: Mr William Auloni  
Your Scheme/Reference: 100 Avenue Road

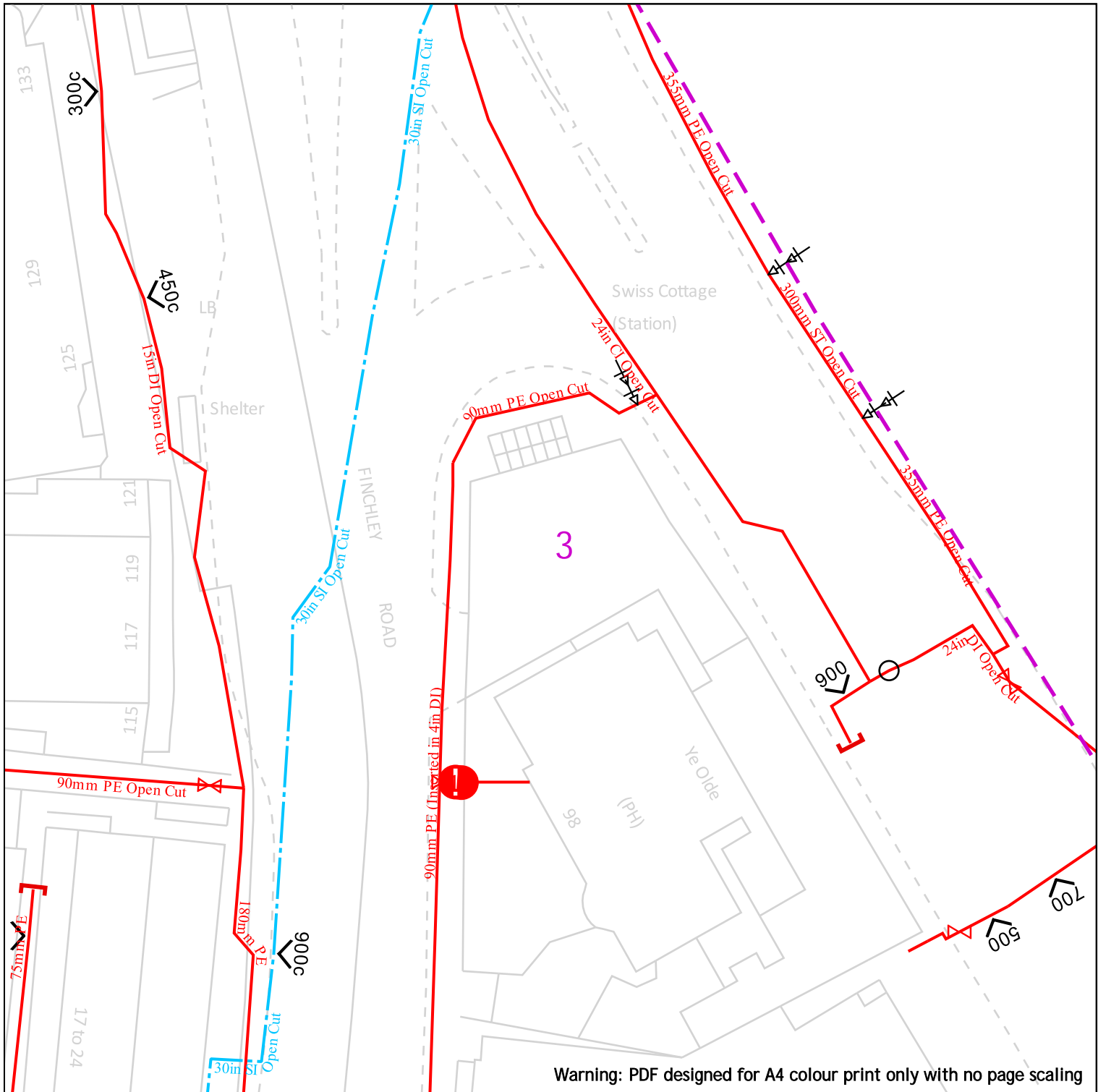
**IMPORTANT NOTICES**

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View extent: 100m, 100m

**In case of emergency call - 0800 111 999**

Scale: 1:500 (When plotted at A4)



Warning: PDF designed for A4 colour print only with no page scaling



<b>Dig Sites</b>	<b>Area:</b>	<b>Line:</b>
	LP Mains	Valve
	MP Mains	Depth of cover
	IP Mains	Syphon
	LHP Mains	Diameter Change
		Material Change
		Out of Standard Service

Where an out-of-standard service is present please contact plant protection 0800 688 588 for further information.

Out-of-standard service symbology may be used to indicate one or more of the following:

- Identification of shallow services
- Identification of dual services
- Recording of non-perpendicular services

A perpendicular service/connection is laid in a straight line from the entry point at the property to the connection at the main – this includes a pipe that has a perpendicular connection and is not straight but does not deviate more than 1m off centre line.

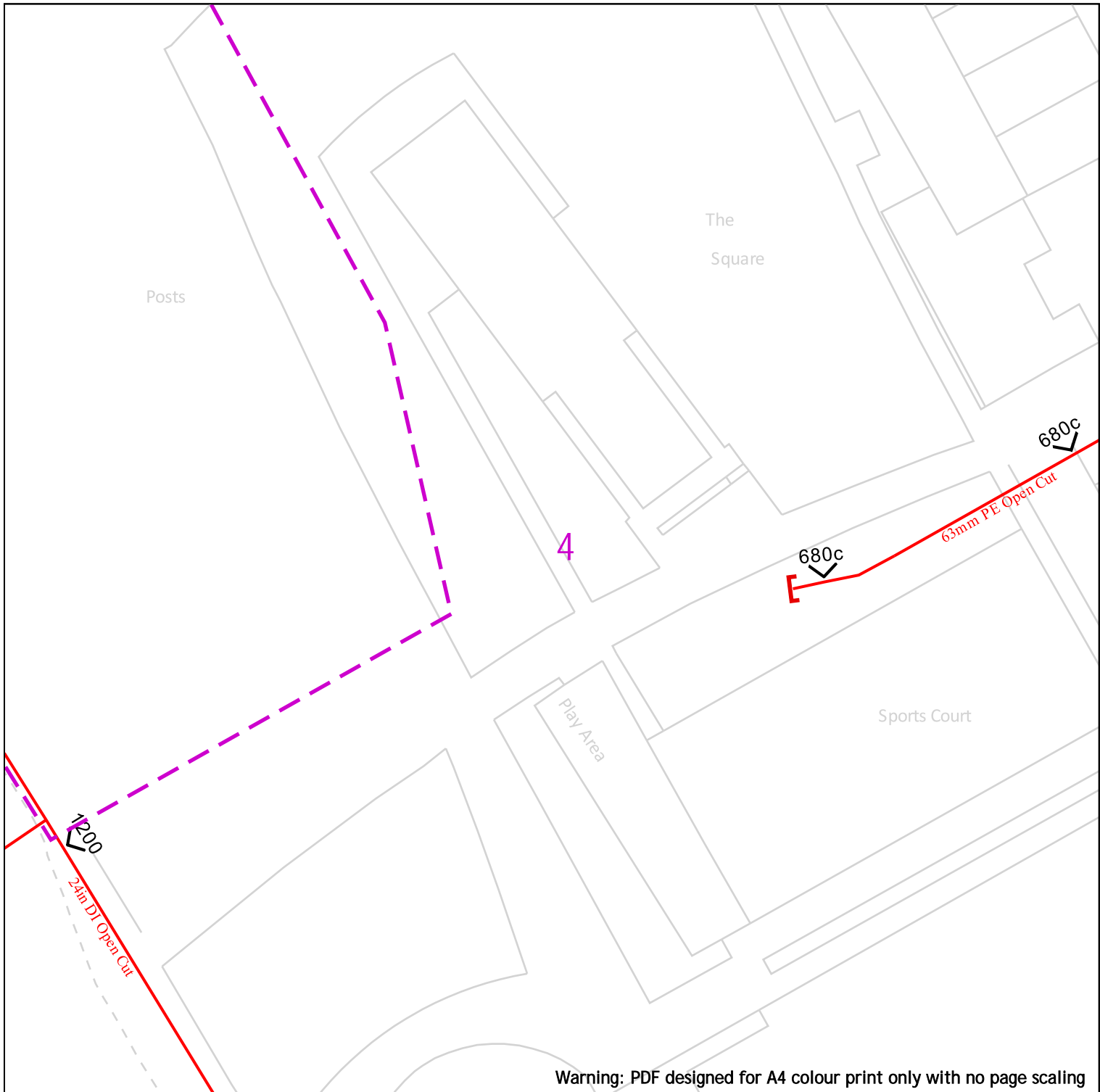
Date Requested: 23/05/2024  
 Job Reference: 33506271  
 Site Location: 526713 184304  
 Requested by: Mr William Auloni  
 Your Scheme/Reference: 100 Avenue Road  
 View extent: 100m, 100m

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**In case of emergency call - 0800 111 999**

Scale: 1:500 (When plotted at A4)



Warning: PDF designed for A4 colour print only with no page scaling



Dig Sites

Area:

Line:



LP Mains  
MP Mains  
IP Mains  
LHP Mains



Valve  
Depth of cover  
Syphon



Diameter Change  
Material Change  
Out of Standard Service

Where an out-of-standard service is present please contact plant protection 0800 688 588 for further information.

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Date Requested: 23/05/2024  
Job Reference: 33506271  
Site Location: 526713 184304  
Requested by: Mr William Auloni  
Your Scheme/Reference: 100 Avenue Road

View extent: 100m, 100m

**IMPORTANT NOTICES**

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**In case of emergency call - 0800 111 999**

Scale: 1:500 (When plotted at A4)



## 10.2. Appendix 2 – UKPN



Registered Office:  
Newington House  
237 Southwark Bridge Road  
London SE1 6NP

Company:  
UK Power Networks (Operations)  
Limited

Registered in England and Wales No: 3870728

Our Ref: 33506271      Your Ref: 100 Avenue Road

Tuesday, 18 June 2024

William Auloni  
26-27 The Hill Northfleet  
Gravesend  
KEN  
DA11 9EU

Dear William Auloni

Thank you for contacting us regarding UK Power Networks equipment at the above site. I have enclosed a copy of our records which show the electrical lines and/or electrical plant. I hope you find the information useful.

I have also enclosed a fact sheet which contains important information regarding the use of our plans and working around our equipment. Safety around our equipment is our number one priority so please ensure you have completed all workplace risk assessments before you begin any works.

Should your excavation affect our Extra High Voltage equipment (6.6 KV, 22 KV, 33 KV or 132 KV), please contact us to obtain a copy of the primary route drawings and associated cross sections.

If you have any further queries do not hesitate to contact us.

Plan Provision  
0800 056 5866



This information is made available to you on the terms set out below. If you do not accept the terms of use set out in this fact sheet please do not use the plans and return them to UK Power Networks.

1. UK Power Networks does not warrant that the information provided to you is correct. You rely upon it at your own risk.
2. UK Power Networks does not exclude or limit its liability if it causes the death of any person or causes personal injury to a person where such death or personal injury is caused by its negligence.
3. Subject to paragraph 2 UK Power Networks has no liability to you in contract, in tort (including negligence), for breach of statutory duty or otherwise how for any loss, damage, costs, claims, demands, or expenses that you or any third party may suffer or incur as a result of using the information provided whether for physical damage to property or for any economic loss (including without limitation loss of profit, loss of opportunity, loss of savings, loss of goodwill, loss of business, loss of use) or any special or consequential loss or damage whatsoever.
4. The information about UK Power Networks electrical plant and/or electric lines provided to you belongs to and remains the property of UK Power Networks. You must not alter it in any respect.
5. The information provided to you about the electrical plant and/or electric lines depicted on the plans may NOT be a complete record of such apparatus belonging to UK Power Networks. The information provided relates to electric lines and/or electrical plant belonging to UK Power Networks that it believes to be present but the plans are not definitive: other electric lines and/or electrical plant may be present and that may or may not belong to UK Power Networks.
6. Other apparatus not belonging to UK Power Networks is not shown on the plan. It is your responsibility to make your own enquiries elsewhere to discover whether apparatus belonging to others is present. It would be prudent to assume that other apparatus is present.
7. You are responsible for ensuring that the information made available to you is passed to those acting on your behalf and that all such persons are made aware of the contents of this letter.
8. Because the information provided to you may not be accurate, you are recommended to ascertain the presence of UK Power Networks electric lines and/or electrical plant by the digging of trial holes. Trial holes should be dug by hand only.

Excavations must be carried out in line with the Health and Safety Executive guidance document HSG 47. We will not undertake this work. A copy of HSG 47 can be obtained from the Health and Safety Executives website.

All electric lines discovered must be considered LIVE and DANGEROUS at all times and must not be cut, resited, suspended, bent or interfered with unless specially authorised by UK Power Networks.

The electric line and electrical plant belonging to UK Power Networks remains so even when made dead and abandoned and any such electric line and/or electrical plant exposed shall be reported to UK Power Networks.

Where your works are likely to affect our electric lines and/or electrical plant an estimate of the price of any protective /diversionary works can be prepared by UK Power Networks Branch at Metropolitan House, Darkes Lane, Potters Bar, Herts. , EN6 1AG, telephone no. 0845 2340040



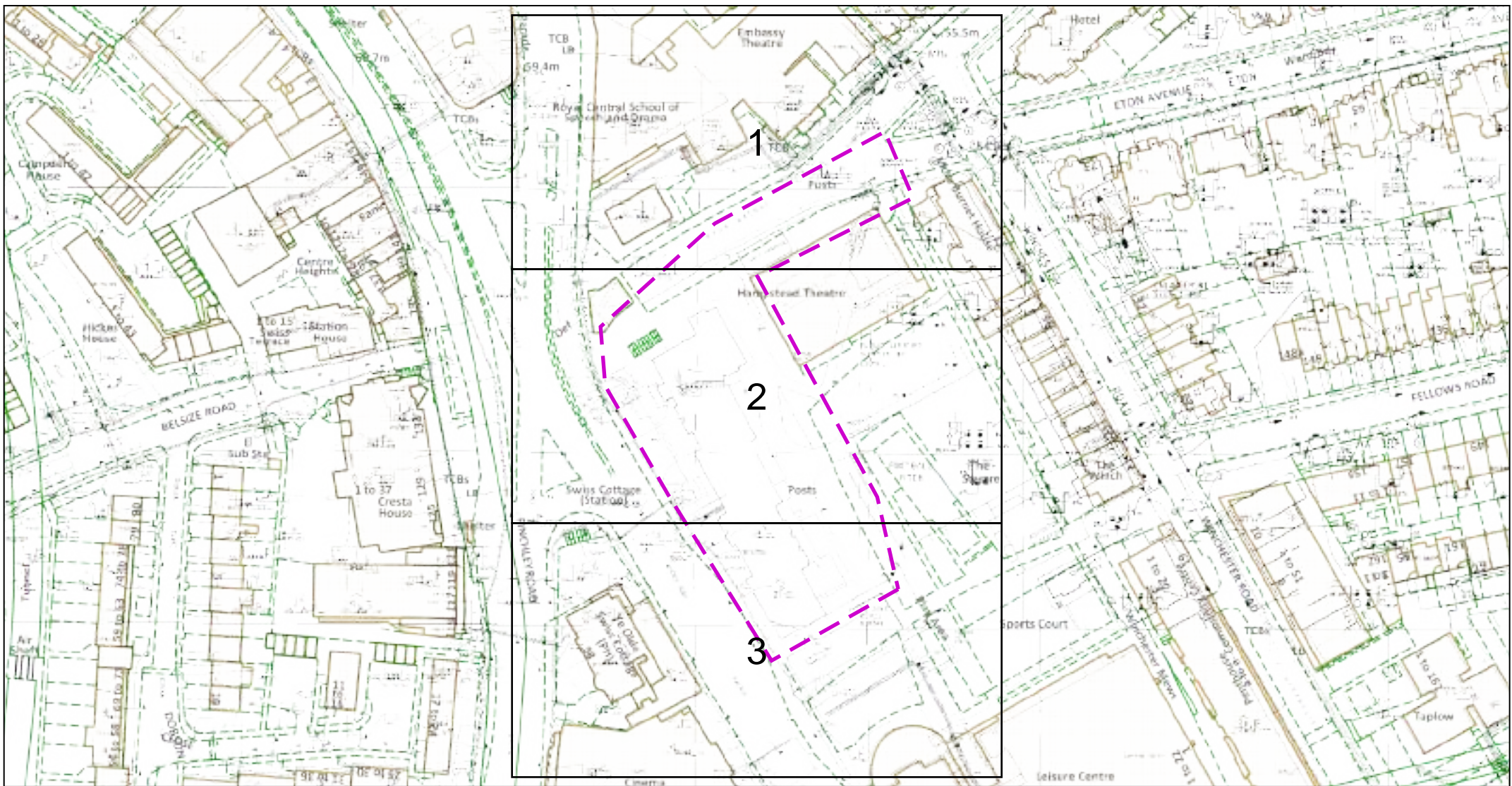
- 9 Any work near to any overhead electricity lines must be carried out by you in accordance with the Health and Safety Executive guidance document GS6 and the Electricity at Work Regulations.

The GS6 Recommendations may be purchased from HSE Books or downloaded from the Energy Networks Association's website.

If given a reasonable period of prior notice UK Power Networks will attend on site without charge to advise how and where "goal posts" should be erected. If you wish to use this service, in the first instance please telephone: 0845 6014516 between 08:30 and 17:00 Monday to Friday.

10. You are responsible for the security of the information provided to you. It must not be given, sold or made available upon payment of a fee to a third party.
11. If in carrying out work on land in, on, under or over which is installed an electric line and/or electrical plant that belongs to UK Power Networks you and/or anyone working on your behalf damages (however slightly) that apparatus you must inform immediately UK Power Networks by our emergency 24 hour three digit telephone number **105** providing;
- your name, address and telephone number;
  - the date, time and place at which such damage was caused;
  - a description of the electric line and/or electrical plant to which damage was caused;
  - the name of the person whom it appears to you is responsible for that damage;
  - the nature of the damage.
12. The expression "UK Power Networks" includes UK Power Networks (EPN) plc, UK Power Networks (LPN) plc, UK Power Networks (SEPN) plc, UK Power Networks and any of their successors and predecessors in title.





Dig Sites Area:  Line: 

The quality and accuracy of any print will depend on your printer, your computer and its print settings. Measurements scaled from this plan may not match measurements between the same points on the ground.



This plan must be used with the attached 'Symbols' document.

Date Requested: 18/06/2024

Job Reference: 33506271

Site Location: 526673 184239

Requested by:

Mr William Auloni

Your Scheme/Reference: 100

Avenue Road

Scale: 1:1538 (When plotted at A4)

1. The position of the apparatus shown on this drawing is believed to be correct but the original landmarks may have been altered since the apparatus was installed.
2. The exact position of the apparatus should be verified - use approved cable avoidance tools prior to excavation using suitable hand tools.
3. It is essential that trial holes are carefully made avoiding the use of mechanical tools or picks until the exact location of all the cables have been determined.
4. It must be assumed that there is a service cable into each property, lamp column and street sign, etc.
5. All cables must be treated as being live unless proved otherwise by UK Power Networks.
6. The information proved must be given to all people working near UK Power Networks plant and equipment. Do not use plans more than 3 months after the issue date for excavation purposes.
7. Please be aware that electric cables/lines belonging to other owners of licensed electricity distribution systems may be present and it is your responsibility to identify their location.
8. Please be aware the Low Voltage Overhead power lines are not currently displayed for the Eastern Region via this service, if you require records on the location of these please contact our Plan Provision team directly via plans@ukpowernetworks.co.uk.

1. UK Power Networks does not warrant that the information provided to you is correct. You rely upon it at your own risk.
2. UK Power Networks does not exclude or limit its liability if it causes the death of any persons or causes personal injury to a person.
3. Subject to paragraph 2 UK Power Networks has no liability to you in contract, in tort (including negligence), for breach of statutory duty or otherwise for any loss, damage, cost, claims, demands, or expenses that you or any third party may suffer or incur as a result of using the information provided whether for physical damage to property or for any economic loss (including without limitation loss of profit, loss of opportunity, loss of savings, loss of goodwill, loss of business, loss of use) or any special or consequential loss or damage whatsoever.
4. This plan has been provided to you on the basis of the terms of use set out in the covering letter that accompanies this plan. If you do not accept and/or do not understand the terms of use set out in the covering letter you must not use the plan and must return it to the sender of the letter.
5. You are responsible for the security of the information provided to you. It must not be given, sold or made available upon payment of a fee to a third party.
6. Please Note: The Overview map does not display UK Power Networks electricity network and should not be used for the location of UK Power Networks assets. For detail of the electricity network please view the relevant page as highlighted in the Overview map.

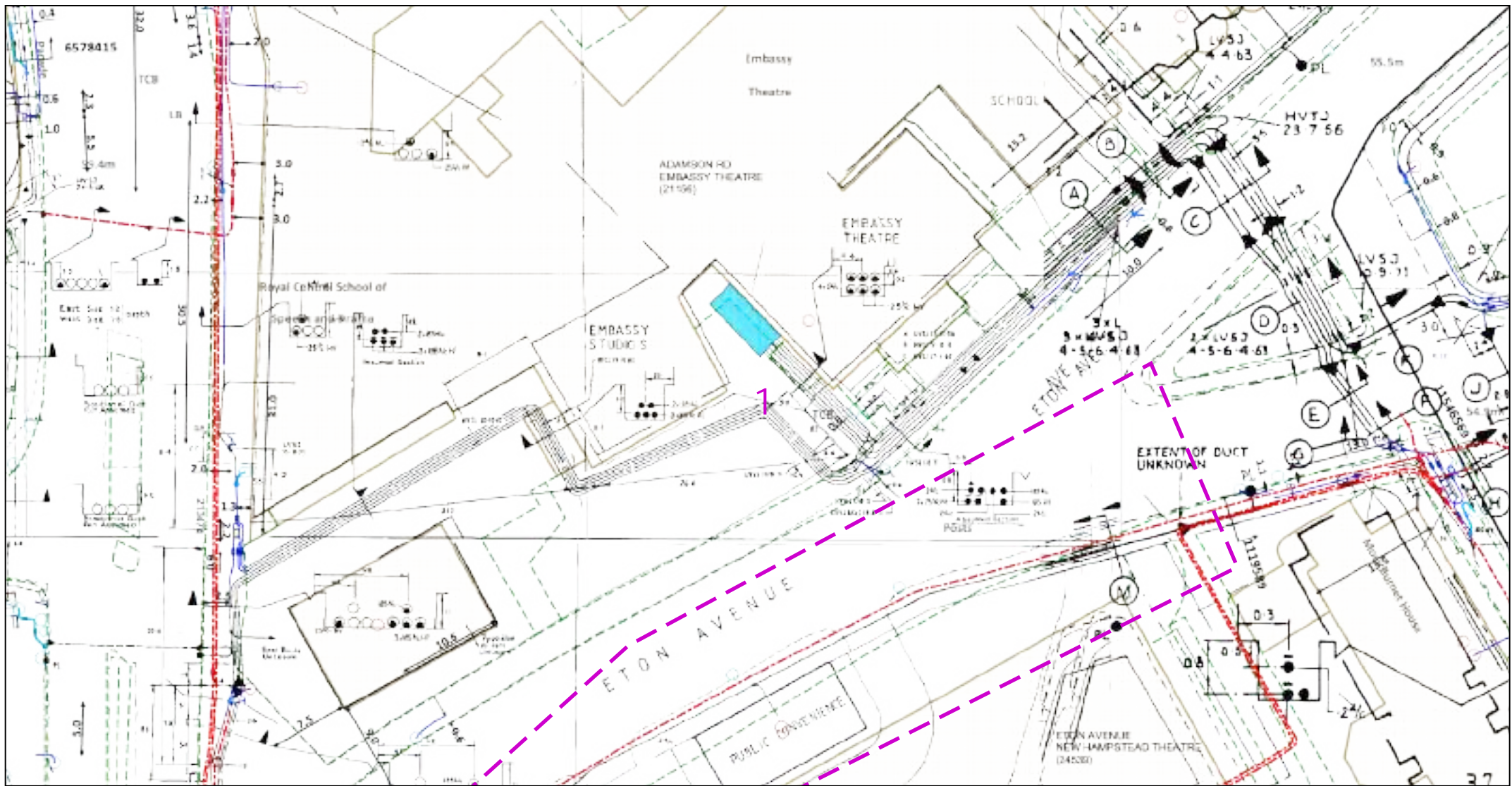
IF IN DOUBT - ASK! PHONE  
0800 056 5866  
EMERGENCY - If you damage a  
cable or line  
Phone 0800 783 8838 (24hrs)  
URGENTLY



ALWAYS LOOK UP  
BEFORE  
YOU START WORK  
Refer to HSE Guidance  
note GS6

Maps produced at 1:2500 scale are Geo-Schematics which show LV mains cables and overhead lines (in some cases all voltages). Prior to carrying out excavations you must refer to the 1:500 records to determine the location of all known underground plant and equipment.





Dig Sites Area: Line:

The quality and accuracy of any print will depend on your printer, your computer and its print settings. Measurements scaled from this plan may not match measurements between the same points on the ground.

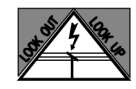


This plan must be used with the attached 'Symbols' document.  
 Date Requested: 18/06/2024  
 Job Reference: 33506271  
 Site Location: 526673 184239  
 Requested by: Mr William Auloni  
 Your Scheme/Reference: 100 Avenue Road  
 Scale: 1:500 (When plotted at A4)

1. The position of the apparatus shown on this drawing is believed to be correct but the original landmarks may have been altered since the apparatus was installed.
2. The exact position of the apparatus should be verified - use approved cable avoidance tools prior to excavation using suitable hand tools.
3. It is essential that trial holes are carefully made avoiding the use of mechanical tools or picks until the exact location of all the cables have been determined.
4. It must be assumed that there is a service cable into each property, lamp column and street sign, etc.
5. All cables must be treated as being live unless proved otherwise by UK Power Networks.
6. The information provided must be given to all people working near UK Power Networks plant and equipment. Do not use plans more than 3 months after the issue date for excavation purposes.
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8. Please be aware the Low Voltage Overhead power lines are not currently displayed for the Eastern Region via this service, if you require records on the location of these please contact our Plan Provision team directly via plans@ukpowernetworks.co.uk.

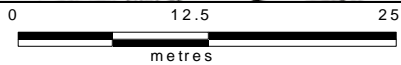
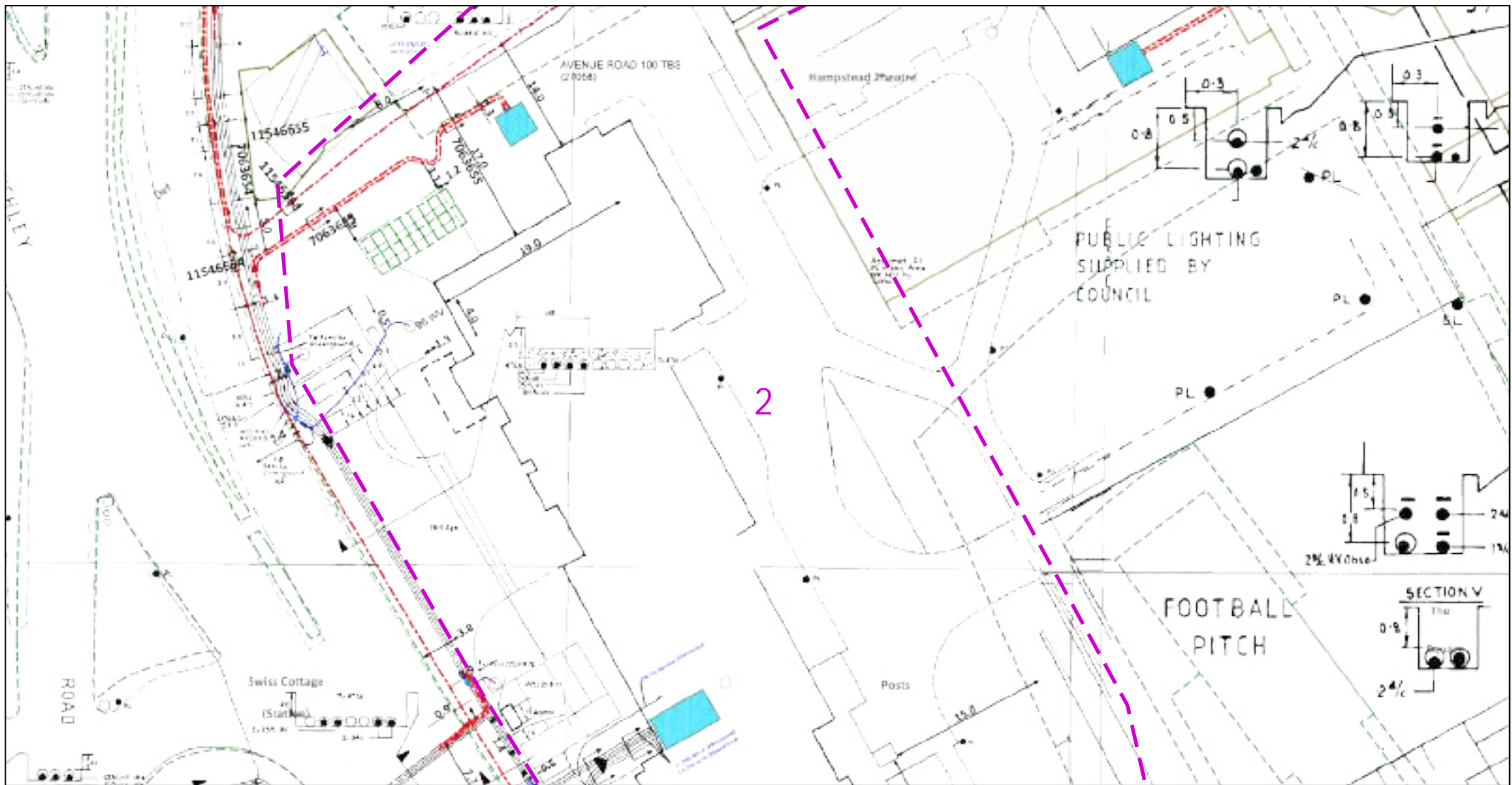
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IF IN DOUBT - ASK! PHONE  
 0800 056 5866  
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Dig Sites Area: Line:

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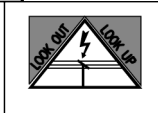


This plan must be used with the attached 'Symbols' document.  
 Date Requested: 18/06/2024  
 Job Reference: 33506271  
 Site Location: 526673 184239  
 Requested by: Mr William Auloni  
 Your Scheme/Reference: 100  
 Avenue Road  
 Scale: 1:500 (When plotted at A4)

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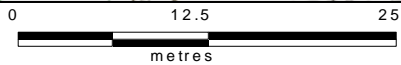
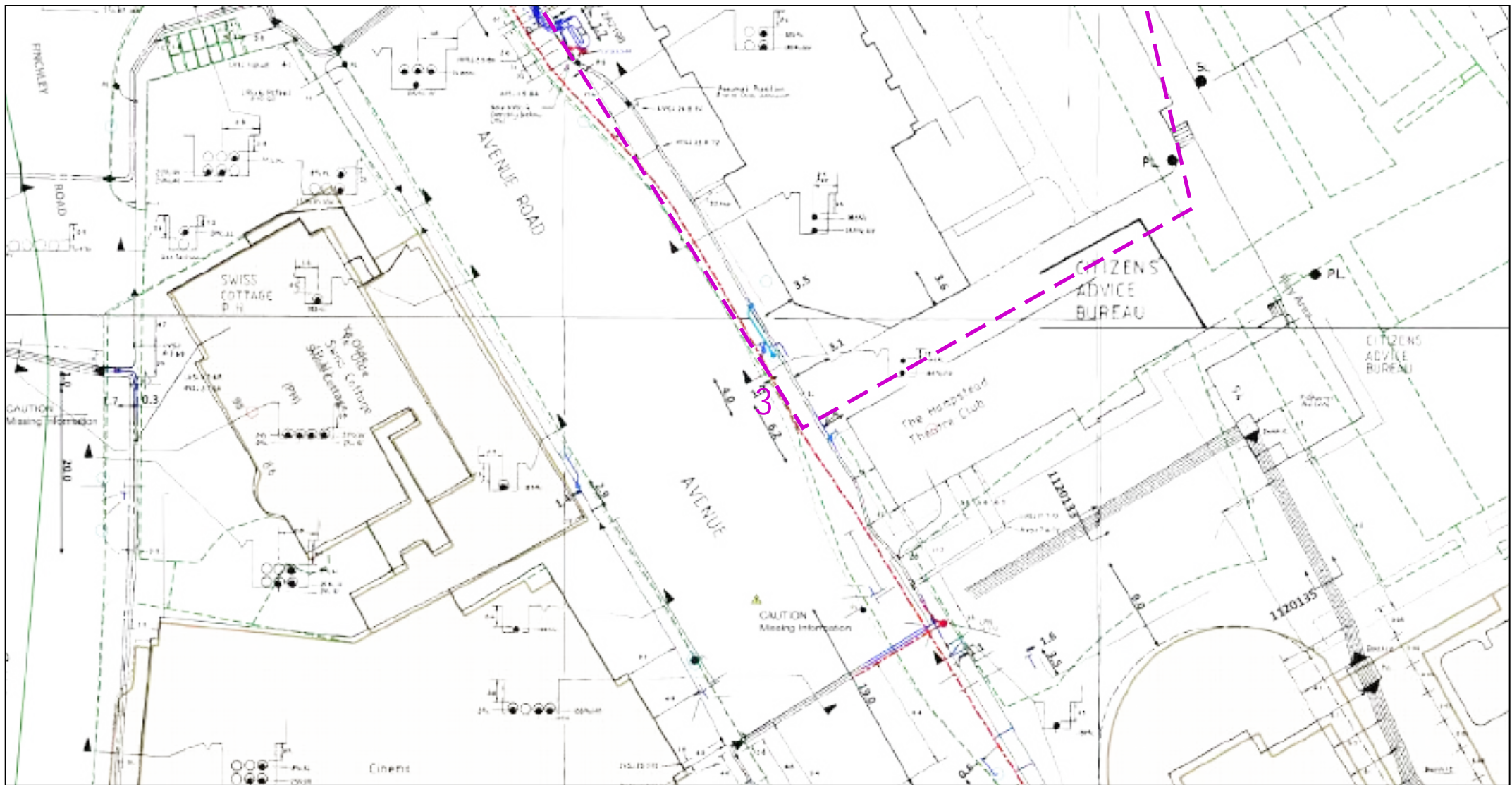
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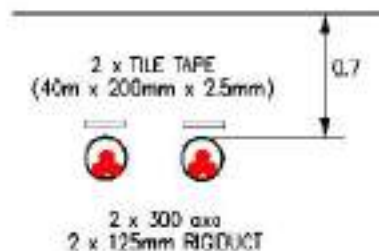
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# Cross Section : 7063655



Cross Section  
7063655

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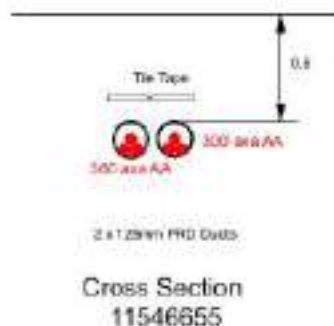
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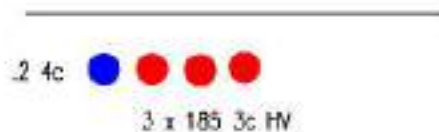
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Cross Section  
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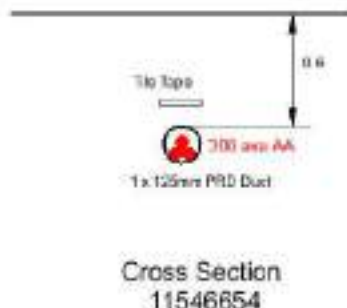
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# Cross Section : 11546568



Cross Section  
11546568

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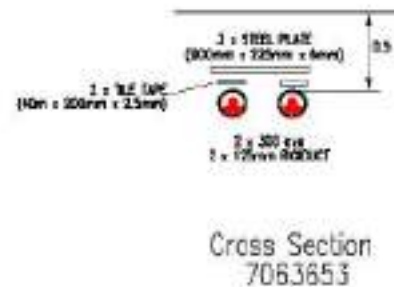


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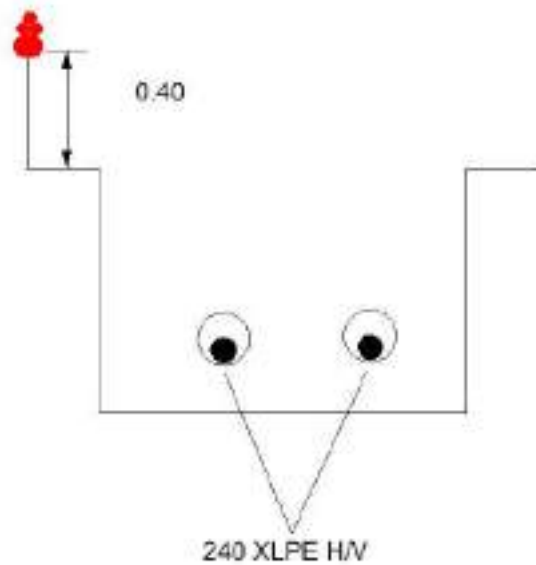
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# Cross Section : 1119589



ASSUMED 0.24/C PRESENT  
N/R OF POSITION

240 XLPE H/V

Cross Section  
1119589

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Cross Section  
11546571

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 7. Please be aware that electric cables/lines belonging to other owners of licensed electricity distribution systems may be present and it is your responsibility to identify their location.

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 2. UK Power Networks does not exclude or limit its liability if it causes the death of any persons or causes personal injury to a person.  
 3. Subject to paragraph 2 UK Power Networks has no liability to you in contract, in tort (including negligence), for breach of statutory duty or otherwise for any loss, damage, cost, claims, demands, or expenses that you or any third party may suffer or incur as a result of using the information provided whether for physical damage to property or for any economic loss (including without limitation loss of profit, loss of opportunity, loss of savings, loss of goodwill, loss of business, loss of use) or any special or consequential loss or damage whatsoever.  
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IF IN DOUBT - ASK! PHONE  
 0800 836 5816  
 EMERGENCY - If you damage  
 a cable or line  
 Please 0800 732 6830 (24hrs)  
 UNIDENTIFY



ALWAYS LOOK UP BEFORE  
 YOU START WORK  
 Refer to HSE Guidance note G66

Maps produced at 1:2500 scale are best-suitings which show domestic cables and air field lines (or cable ducts or sockets) prior to starting an excavation, you must refer to the 1:500 records to determine the location of all known underground plant and equipment.

# Cross Section : 11546570



Cross Section  
11546568

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Cross Section  
11546569

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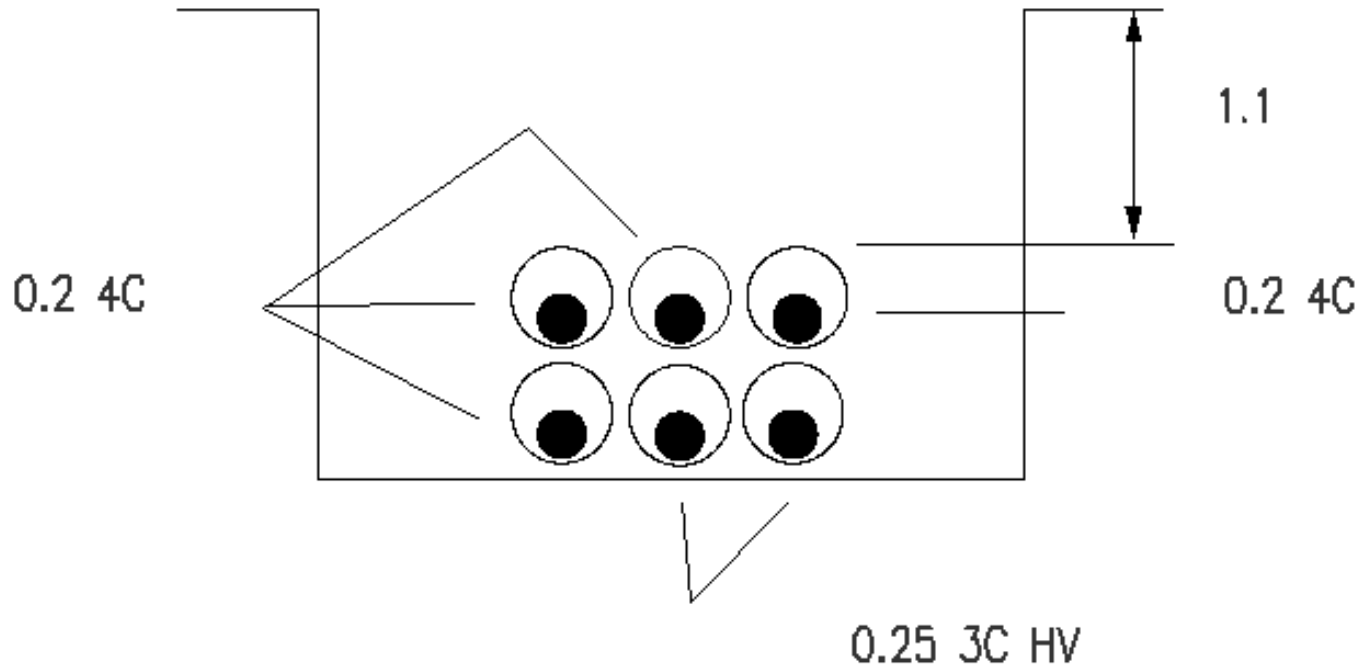
IF IN DOUBT - ASK! PHONE  
 0800 836 5866  
 EMERGENCY - If you damage  
 a cable or line  
 Phone 0800 732 6839 (24hrs)  
 UK POWER



ALWAYS LOOK UP BEFORE  
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Cross Section : 1120133  
**ALL CABLES IN THIS  
 CROSS SECTION ARE  
 IDLE**



**Cross Section  
 1120133**

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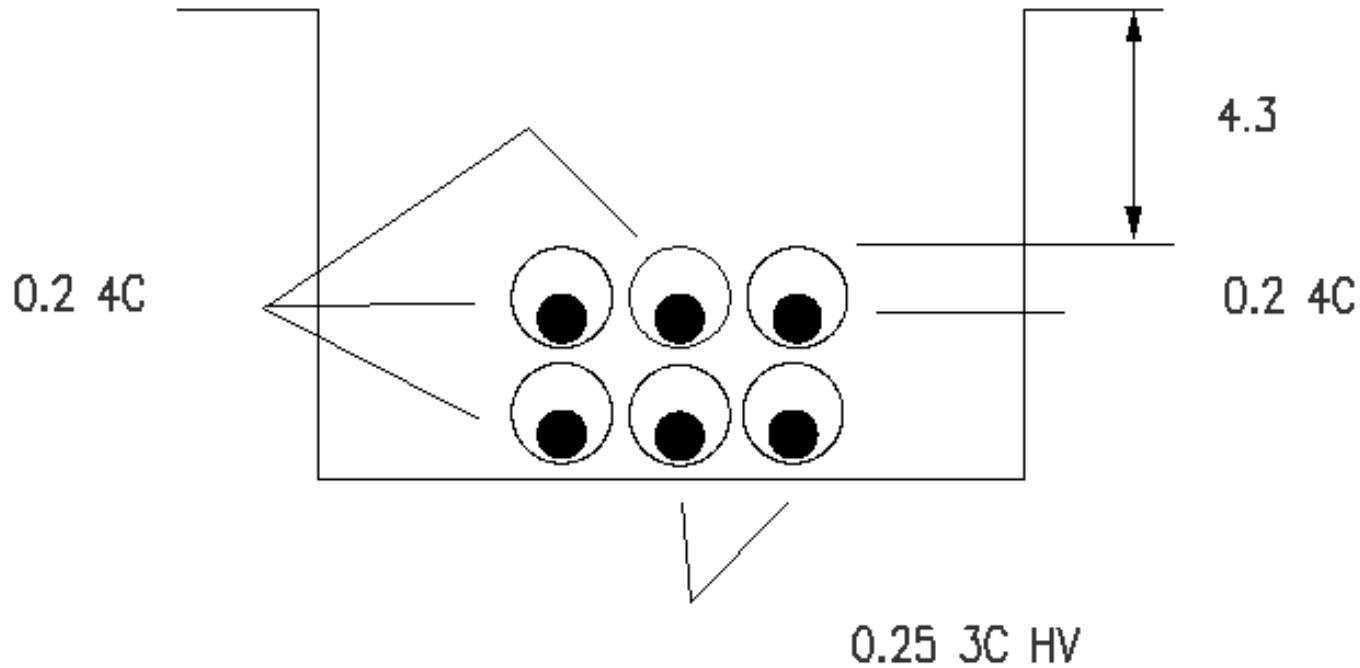
**IF IN DOUBT - ASK! PHONE  
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 EMERGENCY - If you damage  
 a cable call the  
 Freeflow 0800 732 6830 (24hrs)  
 UNIDENTIFY**



**ALWAYS LOOK UP BEFORE  
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 Refer to HSE Guidance note G66

Maps produced at 1:2500 scale are best practice which show domestic cables and air duct lines (or cable ducts & sockets). Prior to starting an excavation, you must refer to the 1:500 records to determine the location of all known underground plant and equipment.

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 CROSS SECTION ARE  
 IDLE**



**Cross Section  
 1120135**

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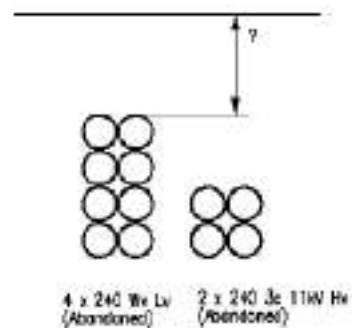
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# Cross Section : 6501130



position of Cables in Section Shown

Cross Section  
6501130

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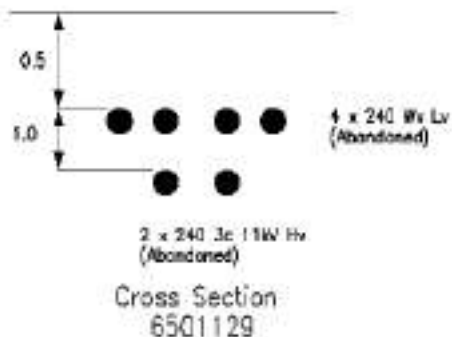


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# Cross Section : 6501129



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## UK Power Networks Feedback Tool

Please help UK Power Networks improve the accuracy of their network records and help make it safer for all those working around them in future.

All you need to do is:

1. Use your phone camera to scan the QR code:
2. Provide feedback on what you have found on site (good or bad)
3. Upload a photo if needed



Thank you for making the area a safer place to dig.

**UK Power Networks, working with LSBUD**



## 10.3. Appendix 3 – UKPN – Budget Quote

Mr. William Auloni  
Whitecode Consulting Ltd  
26-27, The Hill  
Gravesend  
DA11 9EU

Date: 06 June 2024

Our Ref: 8500301095 / QID 3000050481

Dear Mr Auloni

**Site Address: 100 Avenue Road, London, NW3 3HF**

### Budget estimate

I am writing to you on behalf of London Power Networks plc the licensed distributor of electricity for the above address trading as and referred to in this Quote as "UK Power Networks". Thank you for your recent enquiry regarding the above premises.

I am pleased to be able to provide you with a budget estimate for the Works.

It is important to note that this budget estimate is intended as a guide only. It may have been prepared without carrying out a site visit or system studies. No enquiry has been made as to the availability of consents or the existence of any ground conditions that may affect the ground works and only a cursory assessment of any Reinforcement costs that may be applicable has been undertaken. A detailed evaluation of any potential Reinforcement costs, including the applicability of the High-Cost Project Threshold and associated Cost Apportionment Factor will be undertaken upon receipt of a formal application for a Connection Offer. This Budget Estimate is not an offer to provide the connection and nor does it reserve any capacity on UK Power Networks' electricity distribution system.

### **Description of work included:**

#### **Description for 1328kVA at HV/LV**

To supply the demand of 1328kVA to the above site it will be required to install a two HV/LV substations in proposed position provided by the customer. It is proposed to supply the customer from Lithos Road MSS S3 feeder which is approx. 20m away on Avenue Road.

### **High Voltage Point Of Connection**

**£350,000.00** (exclusive of VAT) if the Point Of Connection (POC) is as per above.

### **Please take note**

- **As it is a budget referral request no capacity or switch positions has reserved and Point of Connection might be changed or will be made interactive with other active referrals at Lithos Road MSS for the formal application.**
- **If this were a formal offer it is possible this PoC could be made interactive with other applications**
- **Existing services to be removed prior to new connection.**

## Assumptions

This budget estimate is based on the following assumptions:

- The most appropriate Point of Connection (POC) is as described above.
- A viable cable or overhead line route exists along the route we have assumed between the Point of Connection (POC) and your site
- In cases where the Point of Connection (POC) is to be at High Voltage, that a substation can be located on your premises at or close to the position we have assumed
- Where electric lines are to be installed in private land UK Power Networks will require an easement in perpetuity for its electric lines and in the case of electrical plant the freehold interest in the substation site, on UK Power Networks terms, without charge and before any work commences
- You will carry out, at no charge to UK Power Networks, all the civil works within the site boundary, including substation bases, substation buildings where applicable and the excavation/reinstatement of cable trenches
- Unless stated in your application, all loads are assumed to be of a resistive nature. Should you intend to install equipment that may cause disturbances on UK Power Networks' electricity distribution system (e.g. motors; welders; etc.) this may affect the estimate considerably
- All UK Power Networks' work is to be carried out as a continuous programme of work that can be completed substantially within 12 months from the acceptance of the Quote.

Please note that if any of the assumptions prove to be incorrect, this may have a significant impact on the price in any subsequent Quote. You should note also that UK Power Networks' formal Quote may vary considerably from the budget estimate. If you place reliance upon the budget estimate for budgeting or other planning purposes, you do so at your own risk.

## If you would like to proceed

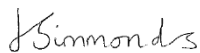
If you would like to proceed to a formal offer of connection then you must apply for a Quote. Please refer to our website [click here](#) to complete application process.

To help us progress any future enquiry as quickly as possible please quote the UK Power Networks Reference Number from this letter on all correspondence.

## Any Questions?

If you have any questions about your budget estimate or need more information, please do not hesitate to contact me. The best time to call is between the hours of 9am and 4pm, Monday to Friday. If the person you need to speak to is unavailable or engaged on another call when you ring, you may like to leave a message or call back later.

Yours sincerely



Jennifer Simmonds

Mobile: 07812 262 852  
Email: JENNIFER.SIMMONDS@UKPOWERNETWORKS.CO.UK



To download your free safety leaflets and resources visit  
[UK Power Networks - Safety Page](#)



## 10.4. Appendix 4 – National Grid

Our Ref: 33506271 100 Avenue Road

Thursday, 23 May 2024

William Auloni  
26-27 The Hill Northfleet

KEN  
DA11 9EU

National Grid House  
Warwick Technology Park  
Gallows Hill, Warwick  
CV34 6DA

**Electricity Emergency Number:**  
**0800 40 40 90\***  
\*Available 24 hours, 7 days/week.  
Calls may be recorded and monitored.  
[www.nationalgrid.com](http://www.nationalgrid.com)

Asset Protection  
Gas Transmission  
National Grid  
Warwick  
Email: [assetprotection@nationalgrid.com](mailto:assetprotection@nationalgrid.com)

## National Grid Electricity – High Risk Response Letter

Dear Sir/ Madam,

An assessment has been carried out with respect to National Grid Electricity Transmission plc's apparatus and the proposed work location. Based on the location entered into the system for assessment the area has been found to be within the High Risk zone from National Grid Electricity Transmission plc's apparatus and **Must Not Proceed** without further assessment by Asset Protection.

**You must send through a works description (including how deep you are excavating) and any plans including any commencement dates to the following email address [assetprotection@nationalgrid.com](mailto:assetprotection@nationalgrid.com) including the LSBUD enquiry reference number.**

**Failure to provide this information will result in a delay to your works being assessed.**

**Once you have contacted the above email address to provide the additional information requested, please do not commence with any works until you have been contacted by NGET.**

Your works may require access to National Grid Non-Operational Land, you must contact BNP PRE at [operationalsupport.nationalgrid@realestate.bnpparibas](mailto:operationalsupport.nationalgrid@realestate.bnpparibas) to confirm and agree land access requirements. BNP PRE hold site specific information that you will be required to review and include within your risk assessments and method statements where access to Non-Operational land is required to undertake the works. This information is mandatory to operate safely and receive authorisation to commence works.

Please note this response and any attached map(s) are valid for 28 days from date of this letter. A new enquiry will need to be submitted into LSBUD if the requested information above is not supplied within 28 days.

Yours sincerely,

Asset Protection



## Your Responsibilities and Obligations

The "Assessment" Section below outlines the detailed requirements that must be followed when planning or undertaking your activities at this location.

It is your responsibility to ensure that the information you have submitted is accurate and that all relevant documents including links are provided to all persons (either direct labour or contractors) working for you near National Grid Electricity Transmission plc's apparatus, e.g. as contained within the Construction (Design and Management) Regulations.

This assessment solely relates to National Grid Electricity Transmission plc (NGET)

This assessment does **NOT** include:

- National Grid's legal interest (easements or wayleaves) in the land which restricts activity in proximity to National Grid's assets in private land. You must obtain details of any such restrictions from the landowner in the first instance and if in doubt contact Asset Protection.
- Recently installed apparatus
- National Grid Gas plc.
- Apparatus owned by other organisations, e.g. Cadent, other gas distribution operators, local electricity companies, other utilities, etc.

It is **YOUR** responsibility to take into account whether the items listed above may be present and if they could be affected by your proposed activities.

This communication does not constitute any formal agreement or consent for any proposed development work; either generally or with regard to National Grid Electricity Transmission plc easements or wayleaves nor any planning or building regulations applications.

National Grid Electricity Transmission plc or their agents, servants or contractors do not accept any liability for any losses arising under or in connection with this information. This limit on liability applies to all and any claims in contract, tort (including negligence), misrepresentation (excluding fraudulent misrepresentation), breach of statutory duty or otherwise. This limit on liability does not exclude or restrict liability where prohibited by the law nor does it supersede the express terms of any related agreements.

If you require further assistance please contact the Asset Protection team via e-mail ([assetprotection@nationalgrid.com](mailto:assetprotection@nationalgrid.com)) or via the contact details at the top of this response.

## Assessment

### Affected Apparatus

The apparatus that has been identified as being in the vicinity of your proposed works is:

- Electricity Transmission Underground Cables and associated apparatus.

## Requirements

### Over Head Lines

BEFORE carrying out any work you must:

(N.B. Works only to be undertaken when contact has been made and response received from Asset Protection)

- Carefully read these requirements including the attached guidance documents and maps showing the location of apparatus.
- Contact the landowner and ensure any proposed works in private land do not infringe National Grid Electricity Transmission plc's legal rights (i.e. easements or wayleaves). If the works are in the road or footpath the relevant local authority should be contacted.
- Ensure that all persons, including direct labour and contractors, working for you on or near National Grid Electricity Transmission plc's apparatus follow the requirements of the HSE Guidance Notes HSG47 - 'Avoiding Danger from Underground Services' and GS6 – 'Avoidance of danger from overhead electric power lines'. This guidance can be downloaded free of charge at [www.hse.gov.uk](http://www.hse.gov.uk)
- In line with the above guidance, verify and establish the actual position of mains, pipes, cables, services and other apparatus on site before any activities are undertaken.

DURING any work you must:

- Comply with all guidance for working within the vicinity of overhead lines as detailed within the guidance documents listed below.
- Comply with all guidance relating to general activities and any specific guidance for each asset type as specified in the Guidance Section below.
- Ensure that access to National Grid Electricity Transmission plc's apparatus is maintained at all times.
- Prevent the placing of heavy construction plant, equipment, materials or the passage of heavy vehicles over National Grid Electricity Transmission plc's apparatus unless specifically agreed with National Grid Electricity Transmission plc's apparatus in advance.
- Exercise extreme caution if slab (mass) concrete is encountered during excavation works as this maybe protecting or supporting National Grid Electricity Transmission plc's apparatus.

### Underground Cables

BEFORE carrying out any work you must:

(N.B. Works only to be undertaken when contact has been made and response received from Asset Protection)

- Refer to the attached cable profile drawings which provide details about the location of National Grid's high voltage underground cables.
- Carefully read these requirements including the attached guidance documents and maps showing the location of apparatus.
- Contact the landowner and ensure any proposed works in private land do not infringe National Grid Electricity Transmission plc's legal rights (i.e. easements or wayleaves). If the works are in the road or footpath the relevant local authority should be contacted.
- Ensure that all persons, including direct labour and contractors, working for you on or near infringe National Grid Electricity Transmission plc's follow the requirements of the HSE Guidance Notes HS(G)47 "Avoiding Danger From Underground Services". This guidance can be downloaded free of charge at [www.hse.gov.uk](http://www.hse.gov.uk)
- In line with the above guidance, verify and establish the actual position of mains, pipes, cables,

National Grid is a trading name for:

National Grid Electricity Transmission plc

Registered Office: 1-3 Strand, London WC2N 5EH

Registered in England and Wales, No 2366977

services and other apparatus on site before any activities are undertaken.

DURING any work you must:

- Comply with all guidance for working within the vicinity of Electricity Underground Cables as detailed within the guidance documents listed below.
- Comply with all guidance relating to general activities and any specific guidance for each asset type as specified in the Guidance Section below.
- Ensure that access to National Grid Electricity Transmission plc's apparatus is maintained at all times.
- Prevent the placing of heavy construction plant, equipment, materials or the passage of heavy vehicles over National Grid Electricity Transmission plc's apparatus unless specifically agreed with National Grid Electricity Transmission plc's in advance.
- Exercise extreme caution if slab (mass) concrete is encountered during excavation works as this maybe protecting or supporting National Grid Electricity Transmission plc's apparatus.

## GUIDANCE

### National Grid Electricity Transmission Network data

- The Network map for National Grid Electricity Transmission assets can be downloaded at the following link in GIS format.

<https://www.nationalgrid.com/uk/electricity-transmission/network-and-infrastructure/network-route-maps>

### Buried Cables

- National Grid's underground cables are protected by a Deed of grant; Easement; Wayleave Agreement or the provisions of the New Roads and Street Works Act. Which provide full right of access to retain, maintain, repair and inspect our asset. Hence we require that no permanent structures are to be built over our cables or within the easement strip.
- Ground levels above our cables must not be altered in any way. Any alterations to the depth of our cables will subsequently alter the rating of the circuit and can compromise the reliability, efficiency and safety of our electricity network.
- The supplied detailed route records should not be taken as positive indication of the cable location. This can only be determined by digging trial holes. Where trial holes are dug the concrete and / or polymeric cable protection tiles must not be disturbed.
- The normal recommendation is to keep 2 metres clear either side of the 1 metre wide trench containing the AC cable.
- National Grid should be made aware of the works starting and may put in place a representative to monitor the works. The works should be carried out in accordance with the recommendations in document HS (G) 47 available from the HSE web site.
- The relocation of existing underground cables is not normally feasible on grounds of cost, operation and maintenance and environmental impact and we believe that successful development can take place in their vicinity.
- The information supplied is given in good faith and only as a guide to the location of our underground cables. The accuracy of this information cannot be guaranteed. The physical presence of such cables may also be evident from physical protection measures such as ducts or concrete protection tiles. The person(s) responsible for planning, supervising and carrying out work in proximity to our cable(s) shall be liable to us, as cable(s) owner, as well as to any third party who may be affected in any way by any loss or damage resulting from their failure to locate and avoid any damage to such a cable(s).
- The relevant guidance in relation to working safely near to existing underground cables is contained within the Health and Safety Executive's ([www.hse.gov.uk](http://www.hse.gov.uk)) Guidance HS(G)47 "Avoiding Danger From Underground Services" and all relevant site staff should make sure that they are both aware of and understand this guidance.
- Our cables are normally buried to a depth of 0.9 metres to the protective covers but this depth can vary depending on location (bridges may have cables 100mm below surface level)..The specific cable route record drawings show further details along the route of the particular cable. The cable route drawings are attached to this letter for your information.
- Cables installed in cable tunnels, whilst less likely to be affected by surface or shallow works may be affected by activities such as piling.
- National Grid assets can be adversely affected by vibration and can provide guidance on the acceptable levels of vibration due to piling and other construction related operations in the vicinity of national grid assets
- Ground cover above our cables should not be reduced or increased without consultation with National Grid.

National Grid is a trading name for:

National Grid Electricity Transmission plc

Registered Office: 1-3 Strand, London WC2N 5EH

Registered in England and Wales, No 2366977

- If a landscaping scheme is proposed as part of the works, we request that no trees and shrubs are planted either directly above or within 3 metres of the existing underground cable, as ultimately the roots may grow to cause damage to the cable.

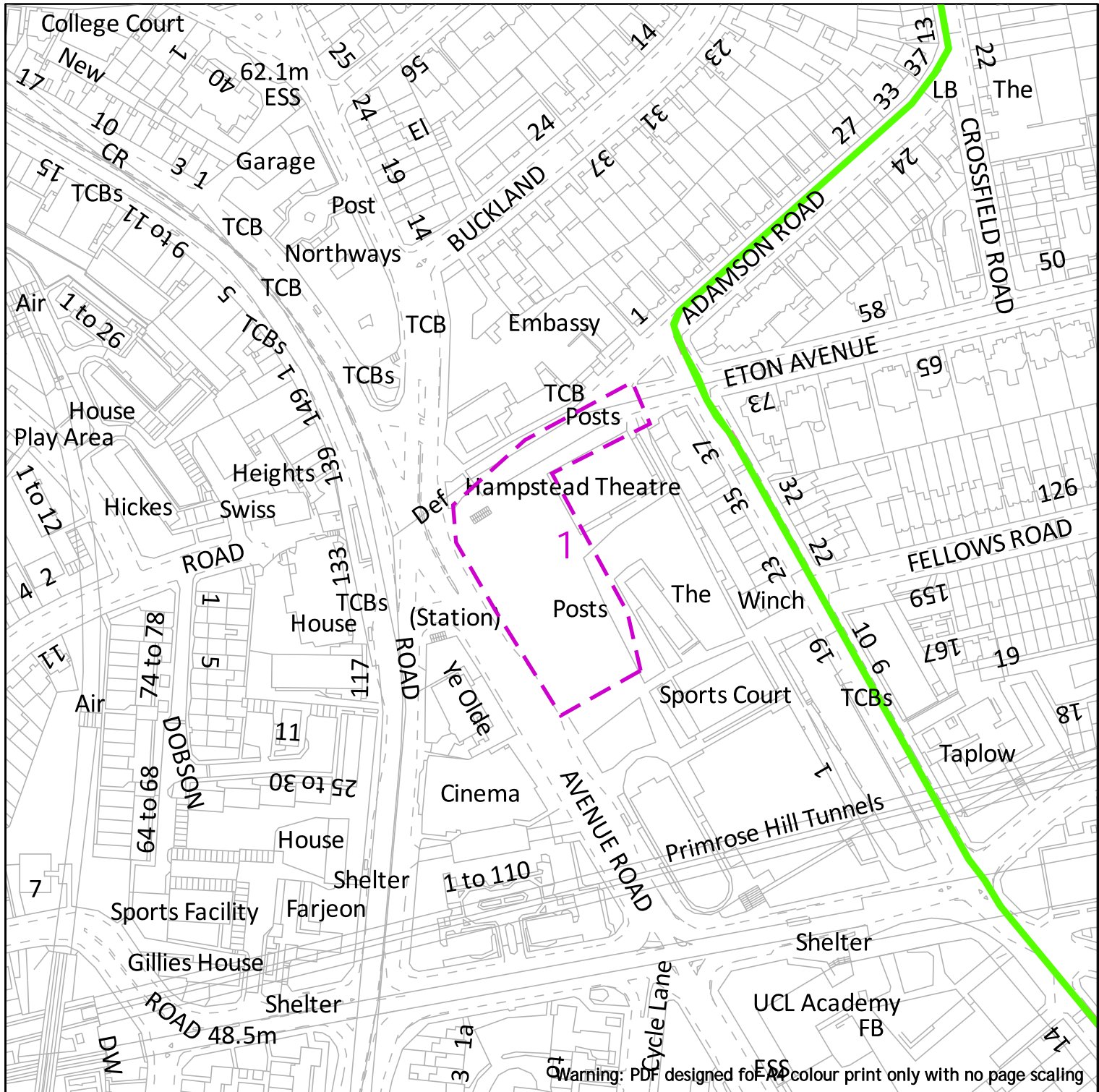
## **Cables Tunnels**

- National Grid's underground cable tunnels are protected by the provisions of a Wayleave/Easement or the New Roads and Street Works Act. This provides full right of access to retain, maintain, repair and inspect our asset.
- Further information and guidance regarding National Grid Cable Tunnels is available on our website at <https://www.nationalgrid.com/uk/electricity-transmission/document/121061/download>
- National Grid requires that the clearances indicated in the guidance document linked above are maintained at all times. Should it be necessary to enter the exclusion zone discussion/ agreement with National Grid is required to ensure the integrity of their tunnel asset and the high voltage cables is not compromised.
- Ground levels above our cable tunnels must not be altered in any way without prior agreement with National Grid; any alterations to the depth of our tunnels can compromise its integrity which could have an adverse impact on the reliability, efficiency and safety of our electricity network.
- Any works which would create high levels of vibration such as piling would need to be assessed by National Grid to ensure it has limited impact on our cable tunnels. National Grid can advise on acceptable vibration limits.

## **Overhead Lines**

- National Grid's Overhead Line/s is protected by a Deed of Easement/Wayleave Agreement which provides full right of access to retain, maintain, repair and inspect our asset.
- The submitted proposed design does not change after the date of this letter. Any design change that moves the proposal closer to National Grid Overhead Line Assets shall be submitted for assessment.
- Statutory electrical safety clearances shall be maintained at all times. National Grid recommends that no permanent structures are built directly beneath our overhead lines. These distances are set out in EN 43 – 8 Technical Specification for "Overhead Line Clearances Issue 5 (2019). To purchase & download EN 43 – 8 Technical Specification for "Overhead Line Clearances Issue 5 (2019) go to <https://www.ena-eng.org/ENA-Docs/>
- Any changes in ground levels which are proposed either beneath or in close proximity to our existing overhead lines would serve to reduce safety clearances. Safety clearances to existing overhead lines must be maintained in all circumstances.
- To view the Development Near Overhead Lines Document.  
<https://www.nationalgridet.com/document/130626/download>
- To view the National Grid Third Party Guidance for working near National Grid Electricity Transmission equipment.  
<https://www.nationalgridet.com/sites/et/files/documents/Working%20near%20electricity%20equipment.pdf>

- The relevant guidance in relation to working safely near to existing overhead lines is contained within the Health and Safety Executive's ([www.hse.gov.uk](http://www.hse.gov.uk)) Guidance Note GS 6 "Avoidance of Danger from Overhead Electric Lines."
- If a landscaping scheme is proposed as part of the proposal, only slow and low growing species of trees and shrubs shall be planted beneath and adjacent to the existing overhead line to reduce the risk of growth to a height which compromises statutory safety clearances.
- Drilling or excavation works shall not be undertaken if they have the potential to disturb or adversely affect the foundations or "pillars of support" of our towers. These foundations extend beyond the base of the tower. Pillar of Support drawings shall be obtained using the contact details above.
- Due to the scale, bulk and cost of the transmission equipment required to operate at 275kV or 400kV we only support proposals for the relocation of existing high voltage overhead lines where such proposals directly facilitate a major development or infrastructure project of national importance which has been identified as such by government.
- To promote the successful development of sites crossed by existing overhead lines, and the creation of well-designed places, National Grid has produced 'Design guidelines for development near pylons and high voltage overhead power lines', which look at how to create high quality development near overhead lines and offer practical solutions which can assist in avoiding the unnecessary sterilisation of land in the vicinity of high voltage overhead lines.
- Further information regarding our undergrounding policy and development near transmission overhead lines is available on our website at: <https://www.nationalgridet.com/network-and-assets/planning-and-development>



Warning: PDF designed for A4 colour print only with no page scaling



**EXTREME CAUTION - HIGH VOLTAGE**  
**\*\*RISK OF DEATH OR SERIOUS INJURY\*\***

- Dig Sites
- Area:
- Line:
- Underground Cables
- Overhead Lines
- Fibre Cables

**NationalGrid House**  
 Warwick Technology Park  
 Gallows Hill  
 Warwick  
 CV34 6DA  
 AssetProtection@NationalGrid.com

Date Requested: 23/05/2024  
 Job Reference: 33506271  
 Site Location: 526713 184304  
 Requested by: Mr William Auloni  
 Your Scheme/Reference: 100 Avenue Road

**IMPORTANT NOTICES**  
 This plan shows those pipes owned by National Grid Electricity Transmission plc in its role as a Licensed Electricity Transporter (ET). Electricity cables owned by other ETs, or otherwise privately owned, may be present in this area. Information with regards to such cables should be obtained from the relevant owners. The information shown on this plan is given without warranty, the accuracy thereof cannot be guaranteed. Ancillary equipment such as cooling systems and communication cables are not shown but their presence should be anticipated. No liability of any kind whatsoever is accepted by National Grid Electricity Transmission plc or their agents, servants or contractors for any error or omission. Safe digging practices, in accordance with HS(G)47, must be used to verify and establish the actual position of cables and other apparatus on site before any mechanical plant is used. It is your responsibility to ensure that this information is provided to all persons (either direct labour or contractors) working for you on or near electricity apparatus. The information included on this plan should not be referred to beyond a period of 28 days from the date of issue.

**National Grid Electricity Emergency Number: 0800 40 40 90**  
 Available 24 hours, 7 days/week. Calls may be recorded and monitored

Scale: 1:2500 (When plotted at A4)

# ENQUIRY SUMMARY

## Received Date

23/05/2024 9:10

## Work Start Date

03/03/2025

## Your Reference

100 Avenue Road

## Location

Centre Point: 526713 184304

X Extent:

Y Extent:

Postcode: NW3 3HF

## Map Options

Paper Size: A4

Orientation: PORTRAIT

Scale: 1:2500

Real World Extents: 90m x 152m

## Enquirer Details

Organisation Name: Whitecode Consulting

Contact Name: William Auloni

Email Address: william.auloni@whitecode.co.uk

Telephone: 01322476684

Address: 26-27 The Hill Northfleet, , KEN, DA11 9EU

## Enquiry Type

Planned Works

## Activity Type

Development Projects

## Work Types

Housing

## Notes/Works Description (if supplied)

100 Avenue Road

## Site Contact Name (if supplied)

William Auloni

## Site Contact Number (if supplied)

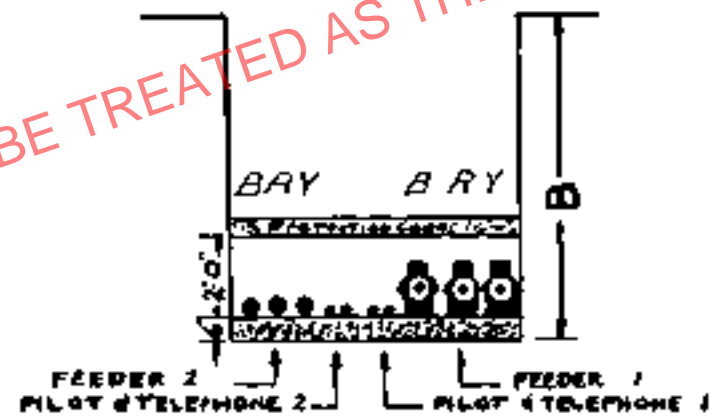
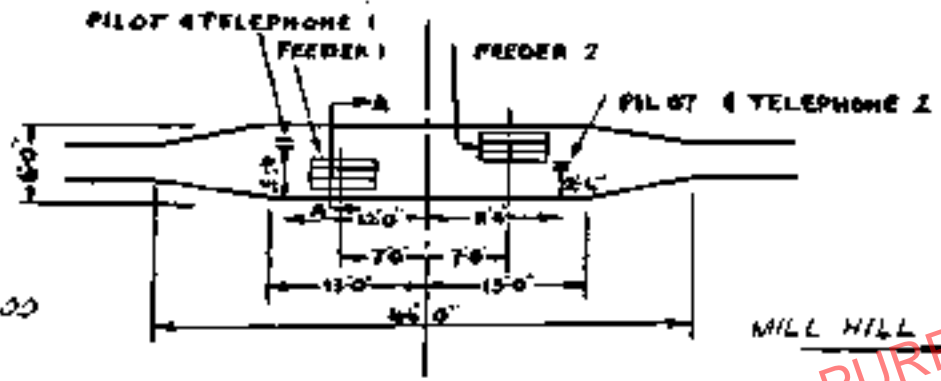
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JOINTS REMADE MAY 71  
 JOINTERS FILCH & THOMAS

FACING MILL HILL



B = 7' 0"

SCALE 4" = 1'  
 CROSS SECTION ON A-A

JOINTERS NAMES FORS 1 & 2 PILOT TELE  
 F. FADWICK.  
 WEATHER - FAIR

32 JOINTERS WOOD

MILL HILL

1/8 8/9

SCALE = 1/8" = 1'

70/16948 SHT96

REV	1	1	2
		25-6-70	24-7-70

DRAWN BY J.M.

CHECKED BY

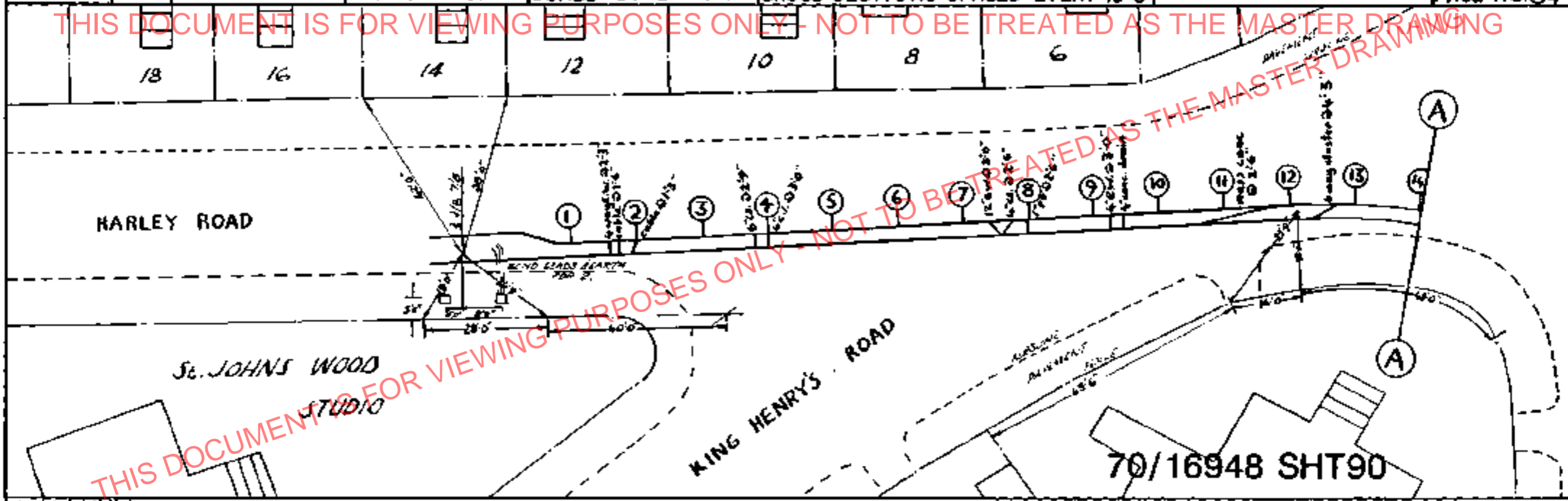
SECTION NO. 8

SCALE - 24'-0" TO 1"

CROSS SECTIONS SPACED EVERY 15'-0" MILL HILL

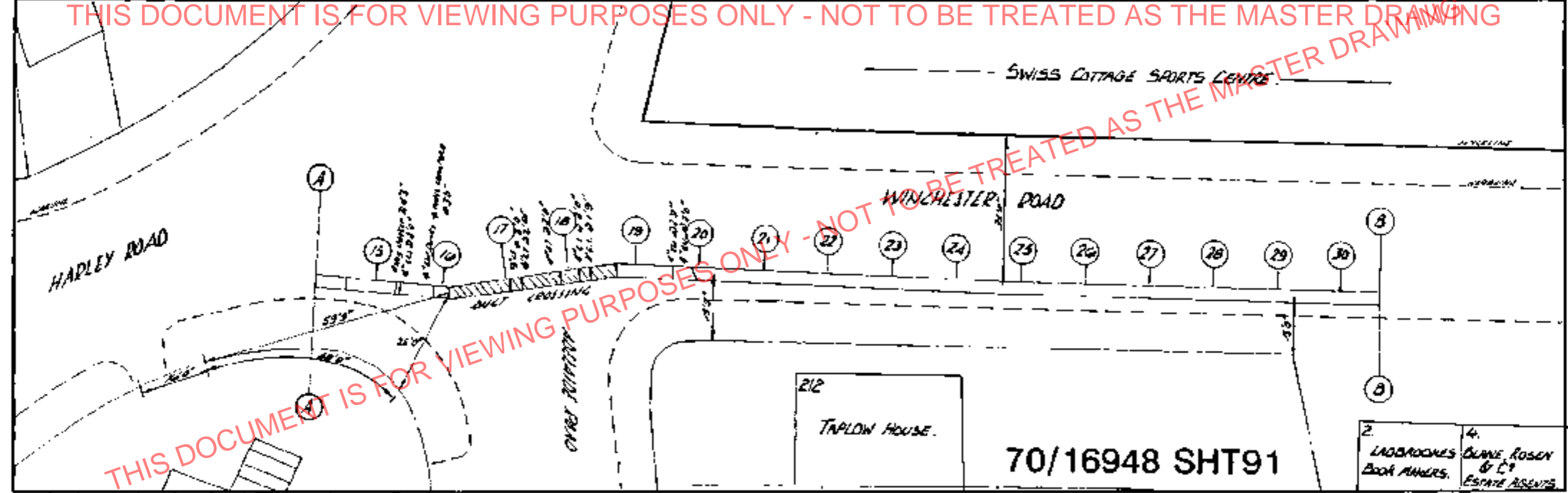
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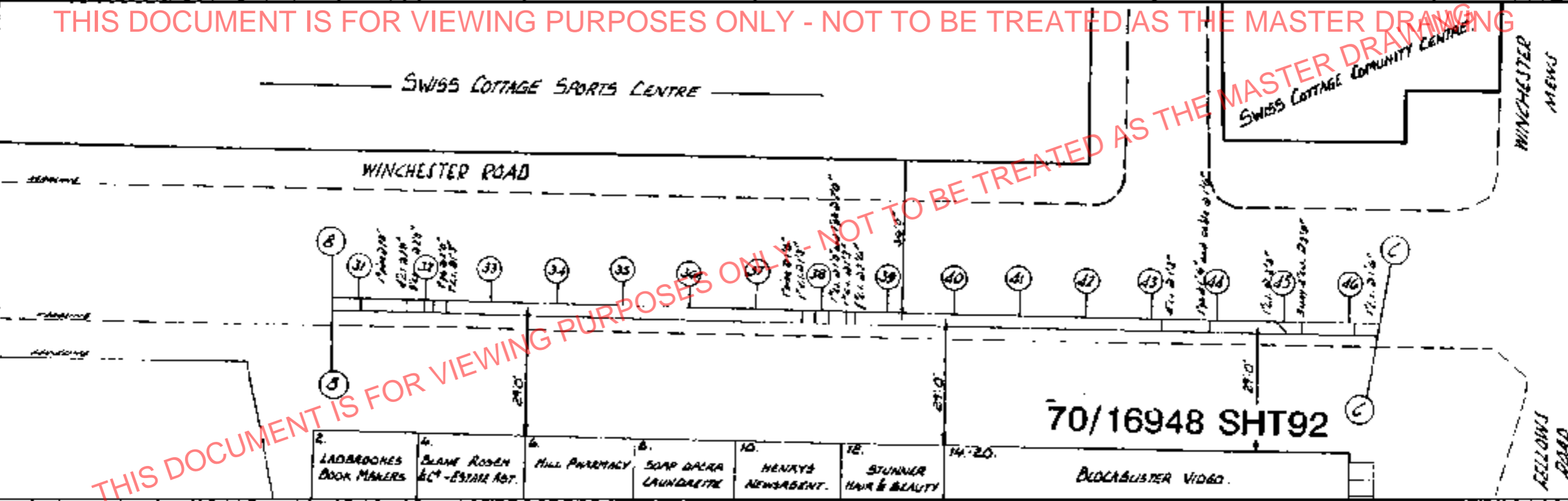


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REV. 1	J.M.M.	REV. 2	N.M.
23-6-90		26-4-99	

2.	4.
LADBROOKES	BLANE ROSEN
BOOK MAKERS.	& CO
	ESTATE AGENTS.

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		25.6.70			26.4.77

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SWISS COTTAGE COMMUNITY CENTRE

WINCHESTER MENS

THE WINCHESTER PROJECT / COMMUNITY SCHEME.

SUGAR CANE MINI-MARKET

BRADLEY'S DISTRO.

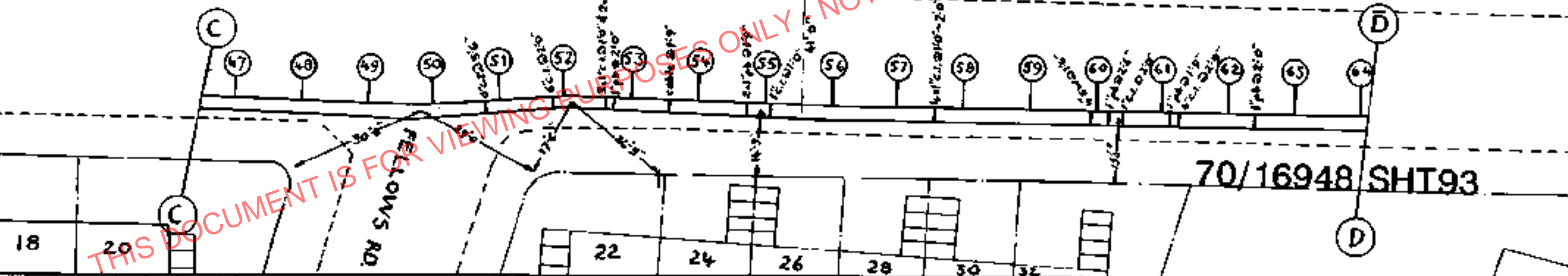
SUGAR CANE MINI-MARKET

SUGAR CANE MINI-MARKET

DANIC'S BRASS ARCHITECTURAL IRONMONGERS

MORA BURNET HOUSE.

WINCHESTER ROAD



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MORA BURNET HOUSE

NEW LENGTHS OF CABLE LET IN  
OWING TO DAMAGE BY GAS  
BOARD CONTRACTORS MAY 71

ISLAND

WINCHESTER RD

40'-0"

ADJUSTMENT

5'-10"

6'-0"

6'-0"

6'-0"

6'-0"

6'-0"

6'-0"

6'-0"

14'-6" VERTICAL CLEARANCE  
OVER RAILROAD

40'-0"

24'-0"

ETON AVE

ADAMSON ROAD

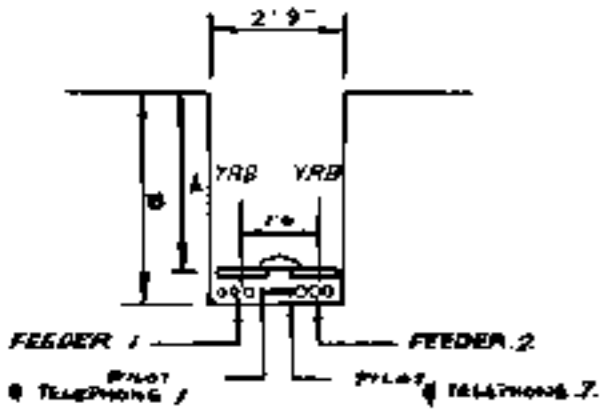
60

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FACING ST. JOHNS WOOD.



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B	6'6"	6'6"	6'3"	5'6"	5'3"	5'3"	6'0"	6'0"	6'0"	6'0"	6'0"	6'3"	6'9"	6'6"	6'3"	6'3"	6'6"	6'0"	5'9"	5'9"	5'9"	5'9"
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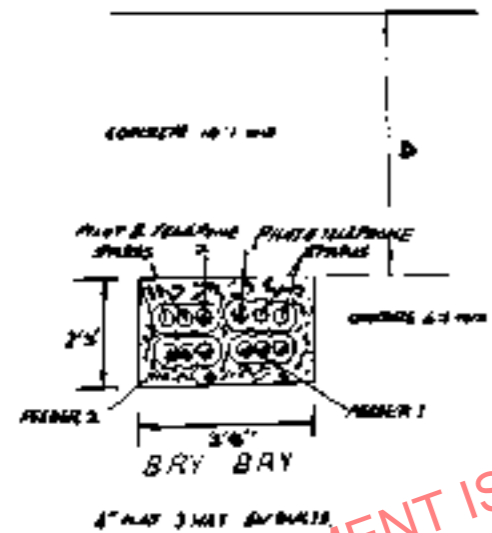
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FACING MILL HILL

C/S	17	18
D	4'3"	4'3"



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CHECKED BY

SECTION NO. 9

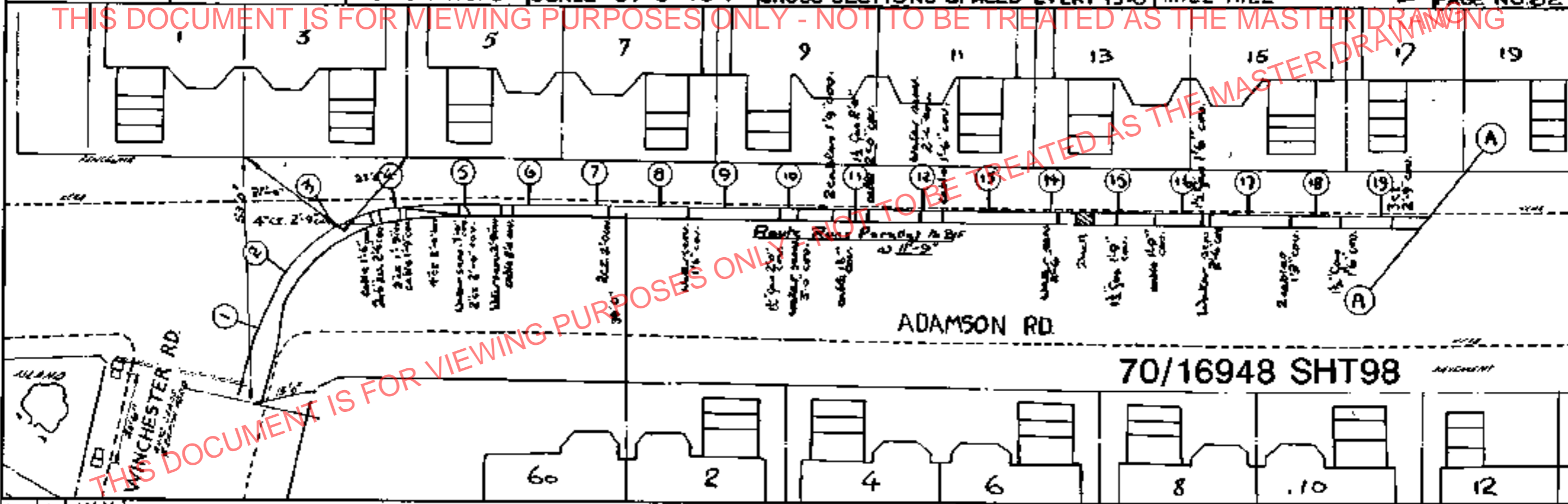
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CROSS SECTIONS SPACED EVERY 15'-0"

MILL HILL

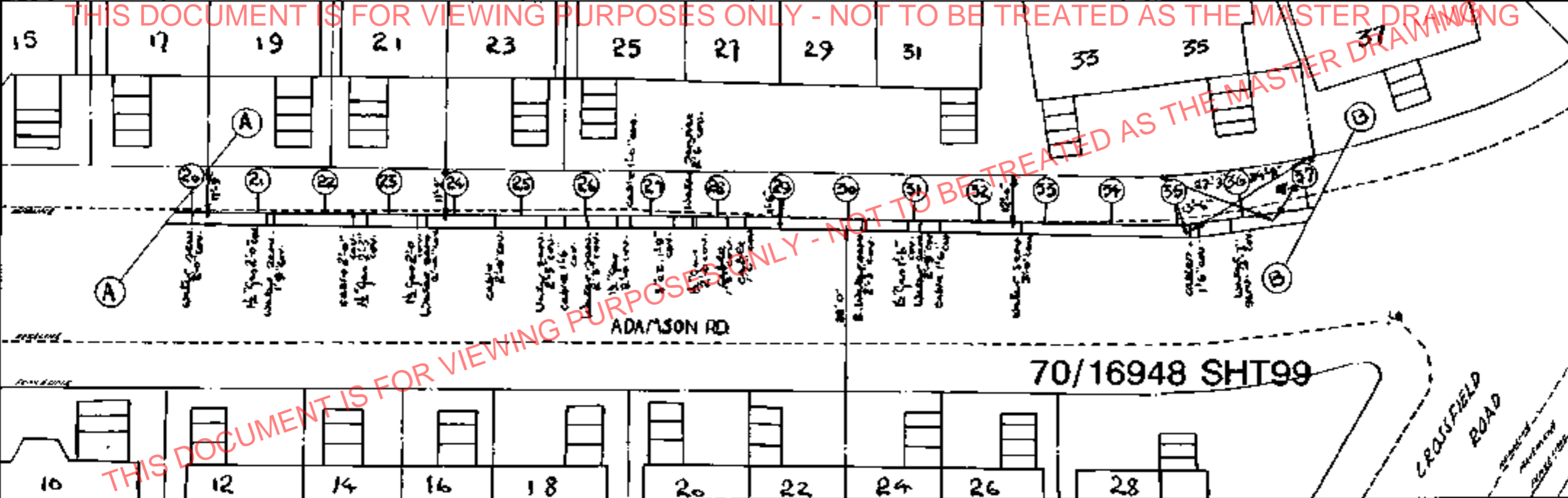
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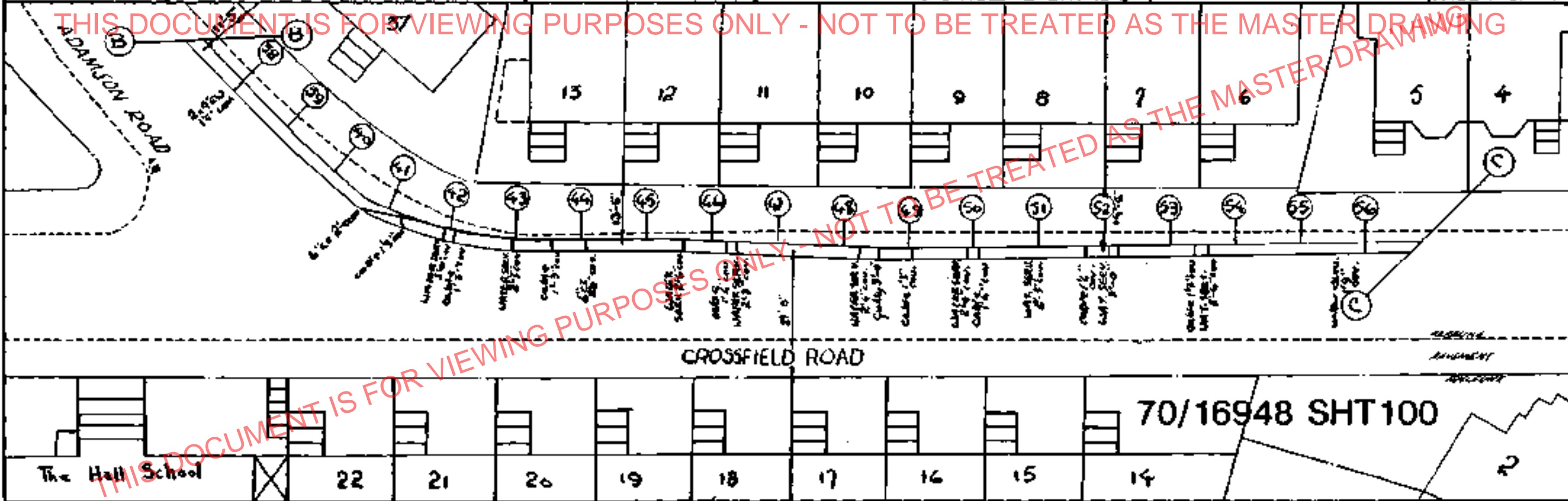


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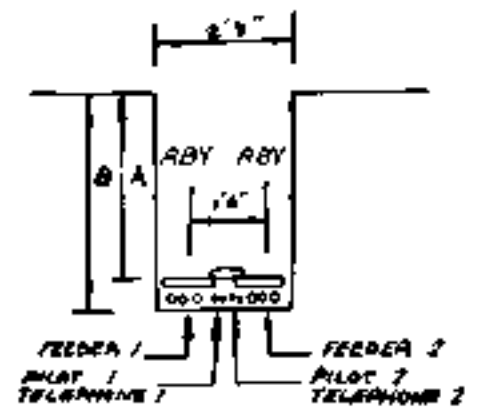
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NORMAL TRENCH CROSS SECTION

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9/3	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60
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B	4'6"	4'6"	4'6"	4'6"	4'6"	4'6"	4'6"	4'6"	4'6"	4'6"	4'6"	4'6"	4'6"	4'6"	4'6"	4'6"	4'6"	4'6"	4'6"	4'6"
9/3	61	62	63	64	65	66	67	68	69	70	71	72	73							
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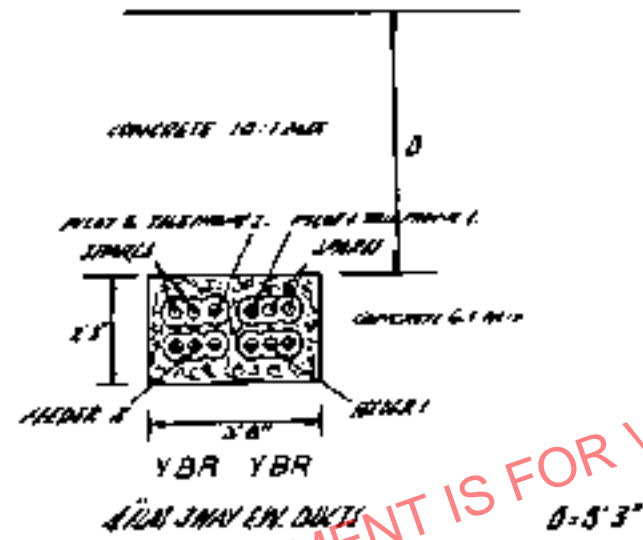
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FACING MILL HILL

DUCT LENGTH 4'-0" OUTSIDE N° 13 ADAMSON RD.



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## 10.5. Appendix 5 – Thames Water

Whitecode Design Associates  
Highfield House, 2-4 Highfield House

DARTFORD  
DA1 2EW

**Search address supplied** 100  
Avenue Road  
London  
NW3 3HF

**Your reference** 100 Avenue Road

**Our reference** ALS/ALS Standard/2024\_5041916

**Search date** 29 August 2024

## Notification of Price Changes

From 1<sup>st</sup> April 2024 Thames Water Property Searches will be increasing the prices of its CON29DW Residential and Commercial searches along with the Asset Location Search. Costs will rise in line with RPI as per previous years, which is set at 6%.

Customers will be emailed with the new prices by February 28<sup>th</sup> 2024.

Any orders received with a higher payment prior to the 1<sup>st</sup> April 2024 will be non-refundable. For further details on the price increase please visit our website at [www.thameswater-propertysearches.co.uk](http://www.thameswater-propertysearches.co.uk).



Thames Water Utilities Ltd  
Property Searches, PO Box 3189, Slough SL1 4WW



[searches@thameswater.co.uk](mailto:searches@thameswater.co.uk)  
[www.thameswater-propertysearches.co.uk](http://www.thameswater-propertysearches.co.uk)



0800 009 4540



**Search address supplied:** 100, Avenue Road, London, NW3 3HF

Dear Sir / Madam

**An Asset Location Search is recommended when undertaking a site development.** It is essential to obtain information on the size and location of clean water and sewerage assets to safeguard against expensive damage and allow cost-effective service design.

The following records were searched in compiling this report: - the map of public sewers & the map of waterworks. Thames Water Utilities Ltd (TWUL) holds all of these.

This search provides maps showing the position, size of Thames Water assets close to the proposed development and also manhole cover and invert levels, where available.

Please note that none of the charges made for this report relate to the provision of Ordnance Survey mapping information. The replies contained in this letter are given following inspection of the public service records available to this company. No responsibility can be accepted for any error or omission in the replies.

You should be aware that the information contained on these plans is current only on the day that the plans are issued. The plans should only be used for the duration of the work that is being carried out at the present time. Under no circumstances should this data be copied or transmitted to parties other than those for whom the current work is being carried out.

Thames Water do update these service plans on a regular basis and failure to observe the above conditions could lead to damage arising to new or diverted services at a later date.

## Contact Us

If you have any further queries regarding this enquiry please feel free to contact a member of the team on 0800 009 4540, or use the address below:

Thames Water Utilities Ltd  
Property Searches  
PO Box 3189  
Slough  
SL1 4WW

Email: [searches@thameswater.co.uk](mailto:searches@thameswater.co.uk)

Web: [www.thameswater-propertysearches.co.uk](http://www.thameswater-propertysearches.co.uk)

## Waste Water Services

**Please provide a copy extract from the public sewer map.**

Enclosed is a map showing the approximate lines of our sewers. Our plans do not show sewer connections from individual properties or any sewers not owned by Thames Water unless specifically annotated otherwise. Records such as "private" pipework are in some cases available from the Building Control Department of the relevant Local Authority.

Where the Local Authority does not hold such plans it might be advisable to consult the property deeds for the site or contact neighbouring landowners.

This report relates only to sewerage apparatus of Thames Water Utilities Ltd, it does not disclose details of cables and or communications equipment that may be running through or around such apparatus.

The sewer level information contained in this response represents all of the level data available in our existing records. Should you require any further Information, please refer to the relevant section within the 'Further Contacts' page found later in this document.

For your guidance:

- The Company is not generally responsible for rivers, watercourses, ponds, culverts or highway drains. If any of these are shown on the copy extract they are shown for information only.
- Any private sewers or lateral drains which are indicated on the extract of the public sewer map as being subject to an agreement under Section 104 of the Water Industry Act 1991 are not an 'as constructed' record. It is recommended these details be checked with the developer.

## Clean Water Services

**Please provide a copy extract from the public water main map.**

Enclosed is a map showing the approximate positions of our water mains and associated apparatus. Please note that records are not kept of the positions of individual domestic supplies.

For your information, there will be a pressure of at least 10m head at the outside stop valve. If you would like to know the static pressure, please contact our Customer Centre on 0800 316 9800. The Customer Centre can also arrange for a full flow and pressure test to be carried out for a fee.

For your guidance:

- Assets other than vested water mains may be shown on the plan, for information only.
- If an extract of the public water main record is enclosed, this will show known public water mains in the vicinity of the property. It should be possible to estimate the likely length and route of any private water supply pipe connecting the property to the public water network.

## **Payment for this Search**

A charge will be added to your suppliers account.

## Further contacts:

### Waste Water queries

Should you require verification of the invert levels of public sewers, by site measurement, you will need to approach the relevant Thames Water Area Network Office for permission to lift the appropriate covers. This permission will usually involve you completing a TWOSA form. For further information please contact our Customer Centre on Tel: 0845 920 0800. Alternatively, a survey can be arranged, for a fee, through our Customer Centre on the above number.

If you have any questions regarding sewer connections, budget estimates, diversions, building over issues or any other questions regarding operational issues please direct them to our service desk. Which can be contacted by writing to:

Developer Services (Waste Water)  
Thames Water  
Clearwater Court  
Vastern Road  
Reading  
RG1 8DB

Tel: 0800 009 3921  
Email: [developer.services@thameswater.co.uk](mailto:developer.services@thameswater.co.uk)

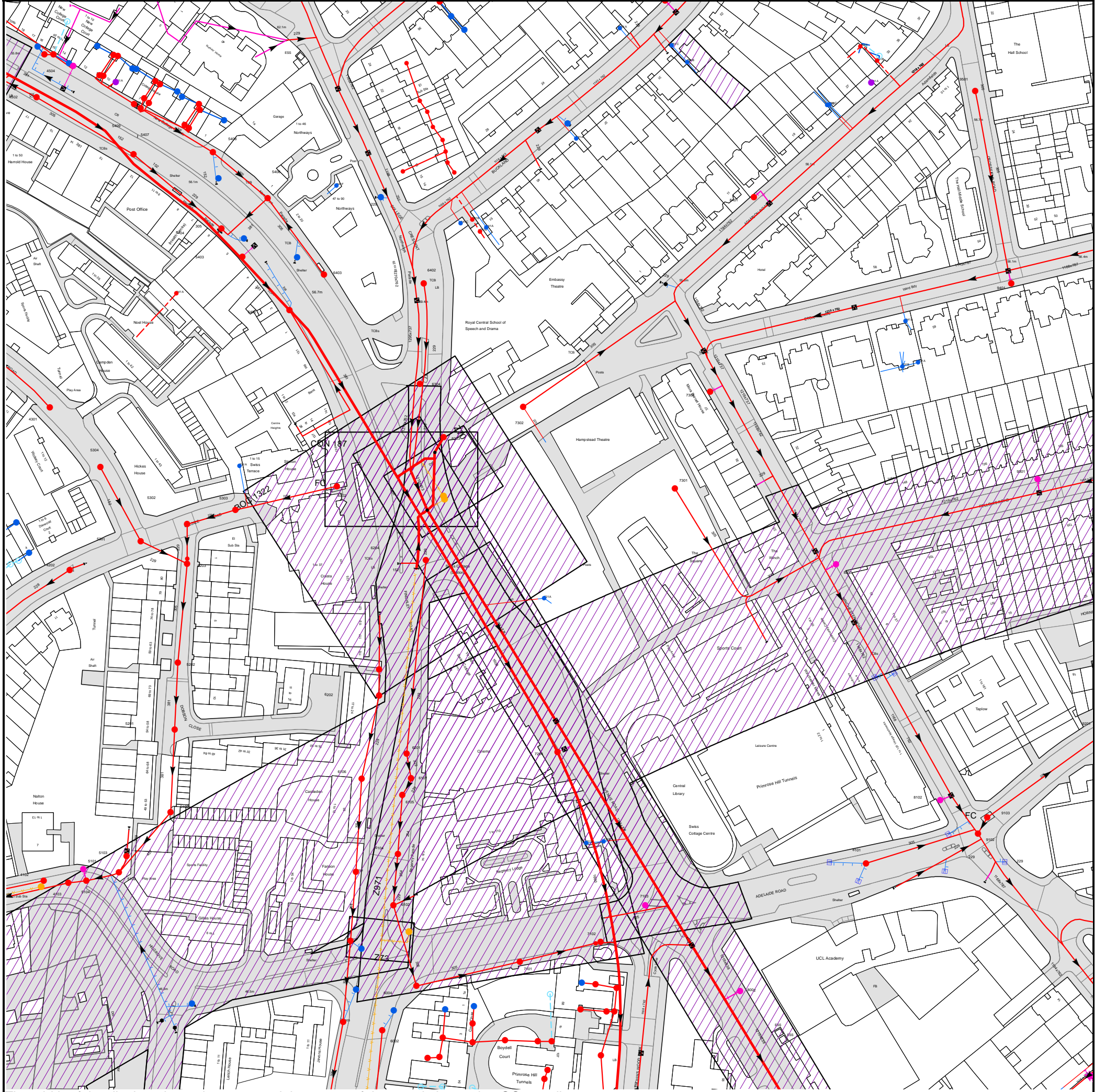
### Clean Water queries

Should you require any advice concerning clean water operational issues or clean water connections, please contact:

Developer Services (Clean Water)  
Thames Water  
Clearwater Court  
Vastern Road  
Reading  
RG1 8DB

Tel: 0800 009 3921  
Email: [developer.services@thameswater.co.uk](mailto:developer.services@thameswater.co.uk)

**Asset Location Search Sewer Map - ALS/ALS Standard/2024 5041916**



The width of the displayed area is 500 m and the centre of the map is located at OS coordinates 526717,184299

The position of the apparatus shown on this plan is given without obligation and warranty, and the accuracy cannot be guaranteed. Service pipes are not shown but their presence should be anticipated. No liability of any kind whatsoever is accepted by Thames Water for any error or omission. The actual position of mains and services must be verified and established on site before any works are undertaken.

Based on the Ordnance Survey Map (2020) with the Sanction of the controller of H.M. Stationery Office, License no. 100019345 Crown Copyright Reserved.

NB. Levels quoted in metres Ordnance Newlyn Datum. The value -9999.00 indicates that no survey information is available

Manhole Reference	Manhole Cover Level	Manhole Invert Level
9401	n/a	n/a
9301	n/a	n/a
70CJ	n/a	n/a
70DA	n/a	n/a
70CE	n/a	n/a
70DB	n/a	n/a
70DC	n/a	n/a
60BF	n/a	n/a
70BJ	n/a	n/a
70CA	n/a	n/a
70CB	n/a	n/a
70DE	n/a	n/a
8002	n/a	n/a
70BI	n/a	n/a
70CC	n/a	n/a
70CD	n/a	n/a
7101	54.11	48.39
7102	53.66	n/a
7103	53.21	51.36
8101	50.39	47.68
711A	n/a	n/a
711B	n/a	n/a
9102	50.45	45.16
9103	50.44	47.47
8102	n/a	n/a
7204	54.97	45.5
721A	n/a	n/a
8203	n/a	n/a
7301	54.25	52.42
7302	57.15	54.52
7304	n/a	n/a
831B	n/a	n/a
831A	n/a	n/a
841A	n/a	n/a
641A	n/a	n/a
74BA	0	0
741A	n/a	n/a
751A	n/a	n/a
7501	n/a	n/a
751C	n/a	n/a
751B	n/a	n/a
8401	n/a	n/a
841B	n/a	n/a
841C	n/a	n/a
851C	n/a	n/a
851A	n/a	n/a
851B	n/a	n/a
85BA	n/a	n/a
9501	57.16	52.81
60BB	0	0
6104	53.86	50.12
6105	54.22	50.48
6107	n/a	50.57
6004	53.74	48.7
60CE	n/a	n/a
60CC	n/a	n/a
60CB	n/a	n/a
60CD	n/a	n/a
60CA	n/a	n/a
60BJ	n/a	n/a
60AJ	n/a	n/a
60BH	n/a	n/a
60BG	n/a	n/a
60BI	n/a	n/a
6303	n/a	n/a
6304	58.13	54
6402	59.47	55.33
6403	56.96	54.83
54CF	n/a	n/a
641E	n/a	n/a
641C	n/a	n/a
641B	n/a	n/a
64BA	n/a	n/a
641F	n/a	n/a
64CG	n/a	n/a
64DC	n/a	n/a
64CE	n/a	n/a
64DA	n/a	n/a
64DB	n/a	n/a
64DE	n/a	n/a
64CH	n/a	n/a
65DE	n/a	n/a
65DF	n/a	n/a
65CD	n/a	n/a
65CE	n/a	n/a
65CB	n/a	n/a
5401	56.57	49.26
541A	n/a	n/a
5403	56.39	46.12
54CE	n/a	n/a
5404	56.38	48.51
















Manhole Reference	Manhole Cover Level	Manhole Invert Level
5406	56.76	n/a
54CC	n/a	n/a
5407	56.07	49.98
5408	56.66	51.46
54DF	n/a	n/a
54DB	n/a	n/a
54DD	n/a	n/a
54DE	n/a	n/a
5409	56.19	51.97
54DA	n/a	n/a
5402	n/a	52.79
55CH	n/a	n/a
55CF	n/a	n/a
55CJ	n/a	n/a
55CI	n/a	n/a
55CG	n/a	n/a
551A	n/a	n/a
55CE	n/a	n/a
55CB	n/a	n/a
55CD	n/a	n/a
55BJ	n/a	n/a
55CA	n/a	n/a
55BG	n/a	n/a
42BA	n/a	n/a
43AC	n/a	n/a
42AJ	n/a	n/a
42AI	n/a	n/a
4202	45.5	42.73
5304	47.14	42.87
5301	47.48	41.82
5201	45.7	40.98
5202	45.74	41.18
5302	49.29	46.07
5203	48.13	41.67
5303	51.07	47.51
531A	n/a	n/a
6302	54.65	50.4
6202	54.71	n/a
6203	54.9	52.31
6201	54.43	50.66
6204	55.82	52.78
4102	43.27	n/a
4103	38.24	35.86
5101	n/a	n/a
5104	44.07	36.65
5102	44.13	40.36
5103	44.13	40.44
5105	45.28	40.68
50AJ	0	0
6003	53.43	49.9
6101	52.97	50.02
6103	53.52	50.22
60BC	n/a	n/a
61BH	n/a	n/a
6106	54.21	50.42
6002	53.18	50.18
6102	53.36	49.71
4502	55.61	49.91
45DC	n/a	n/a
45DA	n/a	n/a
4301	46.94	42.94
45CJ	n/a	n/a
45CI	n/a	n/a
45DB	n/a	n/a
4504	56.29	51.57
55BF	n/a	n/a
55BH	n/a	n/a

The position of the apparatus shown on this plan is given without obligation and warranty, and the accuracy cannot be guaranteed. Service pipes are not shown but their presence should be anticipated. No liability of any kind whatsoever is accepted by Thames Water for any error or omission. The actual position of mains and services must be verified and established on site before any works are undertaken.









# Asset Location Search - Sewer Key

## Public Sewer Types (Operated and maintained by Thames Water)

-  **Foul Sewer:** A sewer designed to convey waste water from domestic and industrial sources to a treatment works.
-  **Surface Water Sewer:** A sewer designed to convey surface water (e.g. rain water from roofs, yards and car parks) to rivers or watercourses.
-  **Combined Sewer:** A sewer designed to convey both waste water and surface water from domestic and industrial sources to a treatment works.
-  Storm Sewer
-  Sludge Sewer
-  Foul Trunk Sewer
-  Surface Trunk Sewer
-  Combined Trunk Sewer
-  Foul Rising Main
-  Surface Water Rising Main
-  Combined Rising Main
-  Vacuum
-  Thames Water Proposed
-  Vent Pipe
-  Gallery

## Other Sewer Types (Not operated and maintained by Thames Water)

-  Sewer
-  Culverted Watercourse
-  Proposed
-  Decommissioned Sewer
-  Content of this drainage network is currently unknown
-  Ownership of this drainage network is currently unknown

### Notes:

- 1) All levels associated with the plans are to Ordnance Datum Newlyn.
- 2) All measurements on the plan are metric.
- 3) Arrows (on gravity fed sewers) or flecks (on rising mains) indicate the direction of flow.
- 4) Most private pipes are not shown on our plans, as in the past, this information has not been recorded.

## Sewer Fittings

A feature in a sewer that does not affect the flow in the pipe. Example: a vent is a fitting as the function of a vent is to release excess gas.

-  Air Valve
-  Meter
-  Dam Chase
-  Vent
-  Fitting

## Operational Controls

A feature in a sewer that changes or diverts the flow in the sewer. Example: A hydrobrake limits the flow passing downstream.

-  Ancillary
-  Drop Pipe
-  Control Valve
-  Weir




## End Items

End symbols appear at the start or end of a sewer pipe. Examples: an Undefined End at the start of a sewer indicates that Thames Water has no knowledge of the position of the sewer upstream of that symbol. Outfall on a surface water sewer indicates that the pipe discharges into a stream or river.

-  Inlet
-  Outfall
-  Undefined End




## Other Symbols

Symbols used on maps which do not fall under other general categories.

-  Change of Characteristic Indicator
-  Public / Private Pumping Station
-  Invert Level
-  Summit

## Areas

Lines denoting areas of underground surveys, etc.

-  Agreement
-  Chamber
-  Operational Site

## Ducts or Crossings

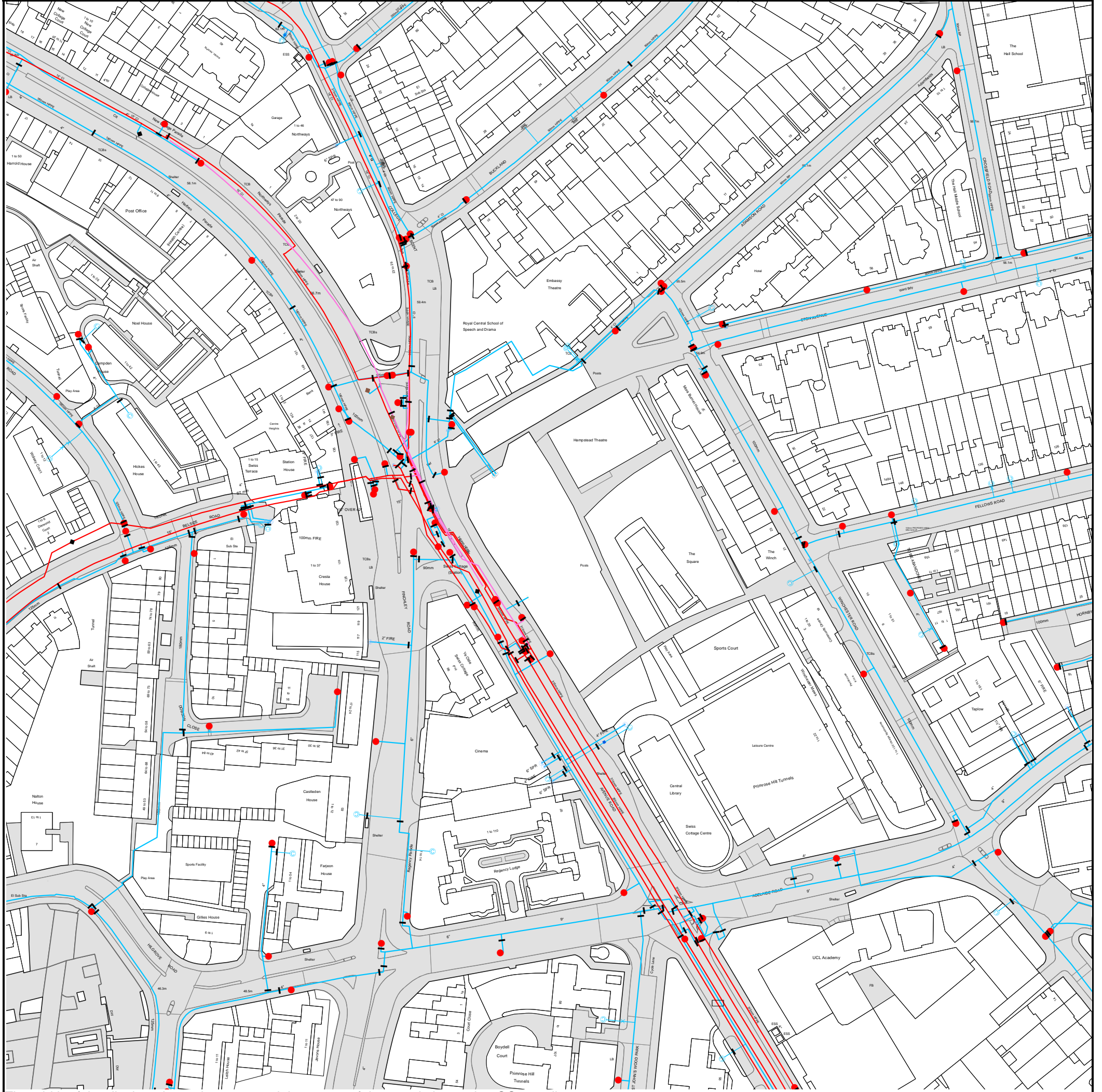
-  Casement
  -  Conduit Bridge
  -  Subway
  -  Tunnel
- Ducts may contain high voltage cables. Please check with Thames Water.

5) 'ns' or '0' on a manhole indicates that data is unavailable.

6) The text appearing alongside a sewer line indicates the internal diameter of the pipe in millimeters. Text next to a manhole indicates the manhole reference number and should not be taken as a measurement. If you are unsure about any text or symbology, please contact Property Searches on 0800 009 4540.



**Asset Location Search Water Map - ALS/ALS Standard/2024 5041916**



The width of the displayed area is 500 m and the centre of the map is located at OS coordinates 526717, 184299.








The position of the apparatus shown on this plan is given without obligation and warranty, and the accuracy cannot be guaranteed. Service pipes are not shown but their presence should be anticipated. No liability of any kind whatsoever is accepted by Thames Water for any error or omission. The actual position of mains and services must be verified and established on site before any works are undertaken.

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# Asset Location Search - Water Key

## Water Pipes (Operated & Maintained by Thames Water)


- 
**4" Distribution Main:** The most common pipe shown on water maps. With few exceptions, domestic connections are only made to distribution mains.
- 
**16" Trunk Main:** A main carrying water from a source of supply to a treatment plant or reservoir, or from one treatment plant or reservoir to another. Also a main transferring water in bulk to smaller water mains used for supplying individual customers.
- 
**3" SUPPLY Supply Main:** A supply main indicates that the water main is used as a supply for a single property or group of properties.
- 
**3" FIRE Fire Main:** Where a pipe is used as a fire supply, the word FIRE will be displayed along the pipe.
- 
**3" METERED Metered Pipe:** A metered main indicates that the pipe in question supplies water for a single property or group of properties and that quantity of water passing through the pipe is metered even though there may be no meter symbol shown.
- 
**Transmission Tunnel:** A very large diameter water pipe. Most tunnels are buried very deep underground. These pipes are not expected to affect the structural integrity of buildings shown on the map provided.
- 
**Proposed Main:** A main that is still in the planning stages or in the process of being laid. More details of the proposed main and its reference number are generally included near the main.

PIPE DIAMETER	DEPTH BELOW GROUND
Up to 300mm (12")	900mm (3')
300mm - 600mm (12" - 24")	1100mm (3' 8")
600mm and bigger (24" plus)	1200mm (4')

## Valves

-  General Purpose Valve
-  Air Valve
-  Pressure Control Valve
-  Customer Valve

## Hydrants

-  Single Hydrant

## Meters










-  Meter

## End Items



Symbol indicating what happens at the end of a water main.

-  Blank Flange
-  Capped End
-  Emptying Pit
-  Undefined End
-  Manifold
-  Customer Supply
-  Fire Supply



## Operational Sites

-  Booster Station
-  Other
-  Other (Proposed)
-  Pumping Station
-  Service Reservoir
-  Shaft Inspection
-  Treatment Works
-  Unknown
-  Water Tower

## Other Symbols

-  Data Logger
-  **Casement:** Ducts may contain high voltage cables. Please check with Thames Water.

## Other Water Pipes (Not Operated or Maintained by Thames Water)

-  **Other Water Company Main:** Occasionally other water company water pipes may overlap the border of our clean water coverage area. These mains are denoted in purple and in most cases have the owner of the pipe displayed along them.
-  **Private Main:** Indicates that the water main in question is not owned by Thames Water. These mains normally have text associated with them indicating the diameter and owner of the pipe.

## Payment Terms and Conditions

All sales are made in accordance with Thames Water Utilities Limited (TWUL) standard terms and conditions unless previously agreed in writing.

1. All goods remain in the property of Thames Water Utilities Ltd until full payment is received.
2. Provision of service will be in accordance with all legal requirements and published TWUL policies.
3. All invoices are strictly due for payment within 14 days of the date of the invoice. Any other terms must be accepted/agreed in writing prior to provision of goods or service or will be held to be invalid.
4. Penalty interest may be invoked by TWUL in the event of unjustifiable payment delay. Interest charges will be in line with UK Statute Law 'The Late Payment of Commercial Debts (Interest) Act 1998'.
5. Interest will be charged in line with current Court Interest Charges, if legal action is taken.
6. A charge may be made at the discretion of the company for increased administration costs.

A copy of Thames Water's standard terms and conditions are available from the Commercial Billing Team (cashoperations@thameswater.co.uk).

We publish several Codes of Practice including a guaranteed standards scheme. You can obtain copies of these leaflets by calling us on 0800 980 8800.

If you are unhappy with our service, you can speak to your original goods or customer service provider. If you are still not satisfied with the outcome provided, we will refer the matter to a Senior Manager for resolution who will provide you with a response.

If you are still dissatisfied with our final response, and in certain circumstances such as you are buying a residential property or commercial property within certain parameters, The Property Ombudsman will investigate your case and give an independent view. The Ombudsman can award compensation of up to £25,000 to you if he finds that you have suffered actual financial loss and/or aggravation, distress, or inconvenience because of your search not keeping to the Code. Further information can be obtained by visiting [www.tpos.co.uk](http://www.tpos.co.uk) or by sending an email to [admin@tpos.co.uk](mailto:admin@tpos.co.uk).

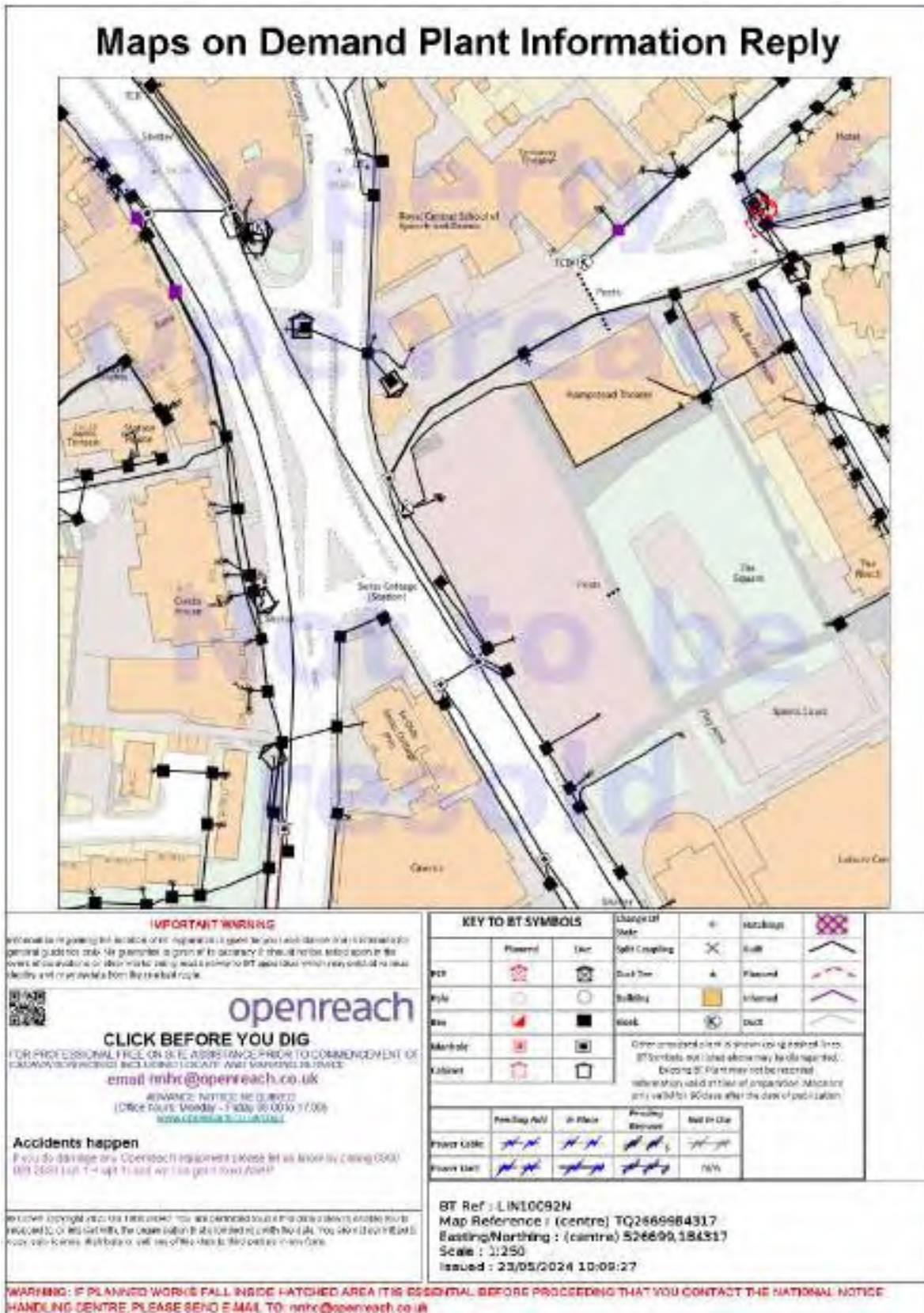
If the Goods or Services covered by this invoice falls under the regulation of the 1991 Water Industry Act, and you remain dissatisfied you can refer your complaint to Consumer Council for Water on 0300 034 2222 or write to them at Consumer Council for Water, 1st Floor, Victoria Square House, Victoria Square, Birmingham, B2 4AJ.

### Ways to pay your bill

Credit Card	BACS Payment	Telephone Banking
Please Call <b>0800 009 4540</b> quoting your invoice number starting CBA or ADS	Account number <b>90478703</b> Sort code <b>60-00-01</b> A remittance advice must be sent to: <b>Thames Water Utilities Ltd., PO Box 3189, Slough SL1 4WW.</b> or email <a href="mailto:ps.billing@thameswater.co.uk">ps.billing@thameswater.co.uk</a>	By calling your bank and quoting: Account number <b>90478703</b> Sort code <b>60-00-01</b> and your invoice number

Thames Water Utilities Ltd Registered in England & Wales No. 2366661 Registered Office Clearwater Court, Vastern Rd, Reading, Berks, RG1 8DB.

## 10.6. Appendix 6 – Openreach





## 10.7. Appendix 7 – Verizon

**From:** [UK OSP-Team](#) .  
**Sent:** 06 June 2024 08:59  
**To:** [William Auloni; UK OSP-Team](#)  
**Subject:** Re: Asset Enquiry  
**Attachments:** [C2 Yes Letter.doc](#)  
[TQ2684SE.pdf](#)

---

Dear Sir/Madam

Verizon is a licensed Statutory Undertaker.

We have reviewed your plans for 100 Avenue Road and have determined that Verizon (Formally known as MCI WorldCom, MFS) does have apparatus in the areas concerned.  
Please see attached location map/maps  
If you have any further queries please do not hesitate to call.

Yours faithfully  
Plant Protection Officer (GGS) Email [osp-team@uk.verizon.com](mailto:osp-team@uk.verizon.com)

On Fri, 31 May 2024 at 10:18, William Auloni <[william.auloni@whitecode.co.uk](mailto:william.auloni@whitecode.co.uk)> wrote:

Hi,

We are consulting for a developer, and we would like to know if you have any apparatus in the following area:

Byron Quarter  
Harrow  
HA3 5BD  
X516120 Y189515

Thank you.



**William Auloni**  
Utilities Manager  
[01322476684](tel:01322476684)  
[07540627638](tel:07540627638)  
[william.auloni@whitecode.co.uk](mailto:william.auloni@whitecode.co.uk)





**Verizon**

Outside Plant & Infrastructure Dept,  
1<sup>st</sup> Floor Office

**Unit 8**

101 Stephenson Street

London

E16 4SA

United Kingdom

Tel: +44 (0) 20 76759465

Email: [osp-team@intl.verizon.com](mailto:osp-team@intl.verizon.com)

Dear Sir/Madam,

**C2 Plant Enquiry**

We have reviewed your proposed plan and have enclosed maps of the areas in which Verizon (Formally known as MCI WorldCom, MFS) has apparatus in the area concerned. Please note these maps are for approximate location only. To determine the exact location a trial hole must be dug using extreme caution.

Where U/G plant is affected and requires rerouting; you must submit draft details of the proposed scheme with a request for a 'C3 Budget Estimate' to email [osp-team@intl.verizon.com](mailto:osp-team@intl.verizon.com). These should be returned to you within 30 working days on receipt of your request.

If you have any further queries, please do not hesitate to call us on the above numbers.

Yours faithfully

**Plant Protection Officer**

**C3 Budget Estimates Controller**

**Verizon**

1<sup>st</sup> floor Office

Unit 8

101 Stephenson Street

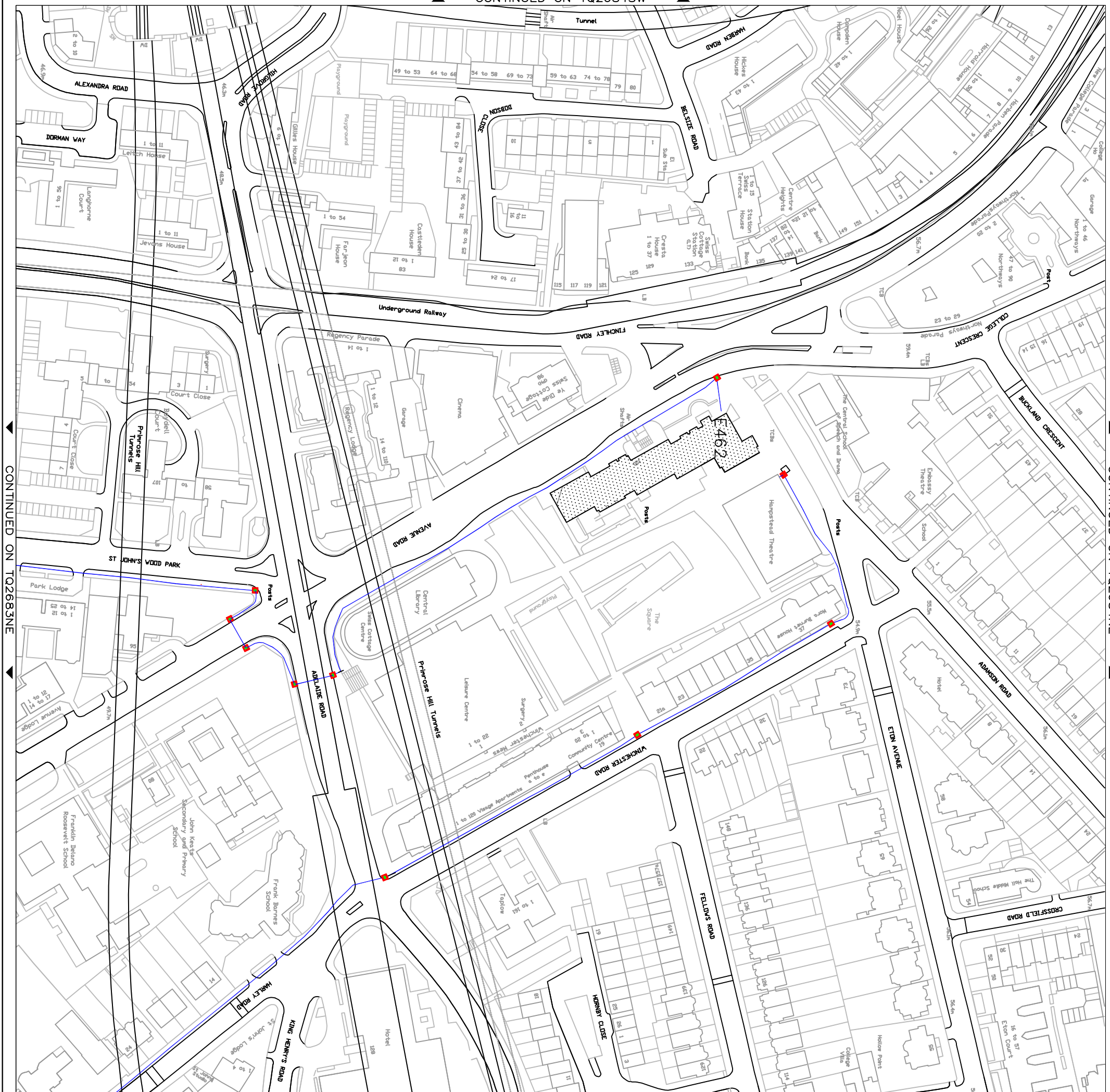
London

E16 4SA

Tel: +44 (0) 20 76759465

---

**Please Note.** To enable us to process your enquiry as quickly as possible, please ensure that you include a post code and/or an Ordnance Survey Grid Reference. Chambers may also be marked as MCI, W-Com, MFS. Thank you for your co-operation



REVISION No.	AMENDMENTS AND ADDITIONS	DRAWN	DATE
A	LATERAL FOR BUILDING E4621 RECORDED INFORMATION SUPPLIED BY BOA	GMB	04/03/09

### URBAN LANDLINE MAP TILE 500X500m



#### LEGEND:

- VZ BACKBONE DUCT IN CARRIAGEWAY/FOOTWAY
- VZ LATERAL DUCT IN CARRIAGEWAY/FOOTWAY
- VZ DUCT IN UTILITY TUNNEL
- VZ ACCESS CHAMBERS
- VZ CABLE IN 3rd PARTY DUCT/CHAMBER



Basingstoke Road  
 Reading, BERKS  
 RG26DA GBR  
 Tel: +44 118 9055000

INFORMATION SHOWN ON THIS PLAN IS FOR INDICATION ONLY. NO WARRANTY IS MADE AS TO ITS ACCURACY. THIS PLAN MUST NOT BE RELIED UPON IN THE EVENT OF EXCAVATION OR OTHER WORKS CARRIED OUT IN THE VICINITY OF VERIZON PLANT. NO LIABILITY OF ANY KIND WHATSOEVER IS ACCEPTED BY VERIZON BUSINESS, ITS SERVANTS OR AGENTS FOR ANY ERROR OR OMISSION IN RESPECT OF THIS PLAN. THE ACTUAL POSITION OF UNDERGROUND SERVICES MUST BE VERIFIED ON SITE BEFORE ANY MECHANICAL PLANT IS USED

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O.S. MAP REFERENCE		TQ2684SE	
DRAWN	DATE	APPROVED	DATE
CCC	SEP 99		
		SCALE	SHEET SIZE
		1:1875	A3
			REVISION
			A





## 10.8. Appendix 8 – Vodafone