

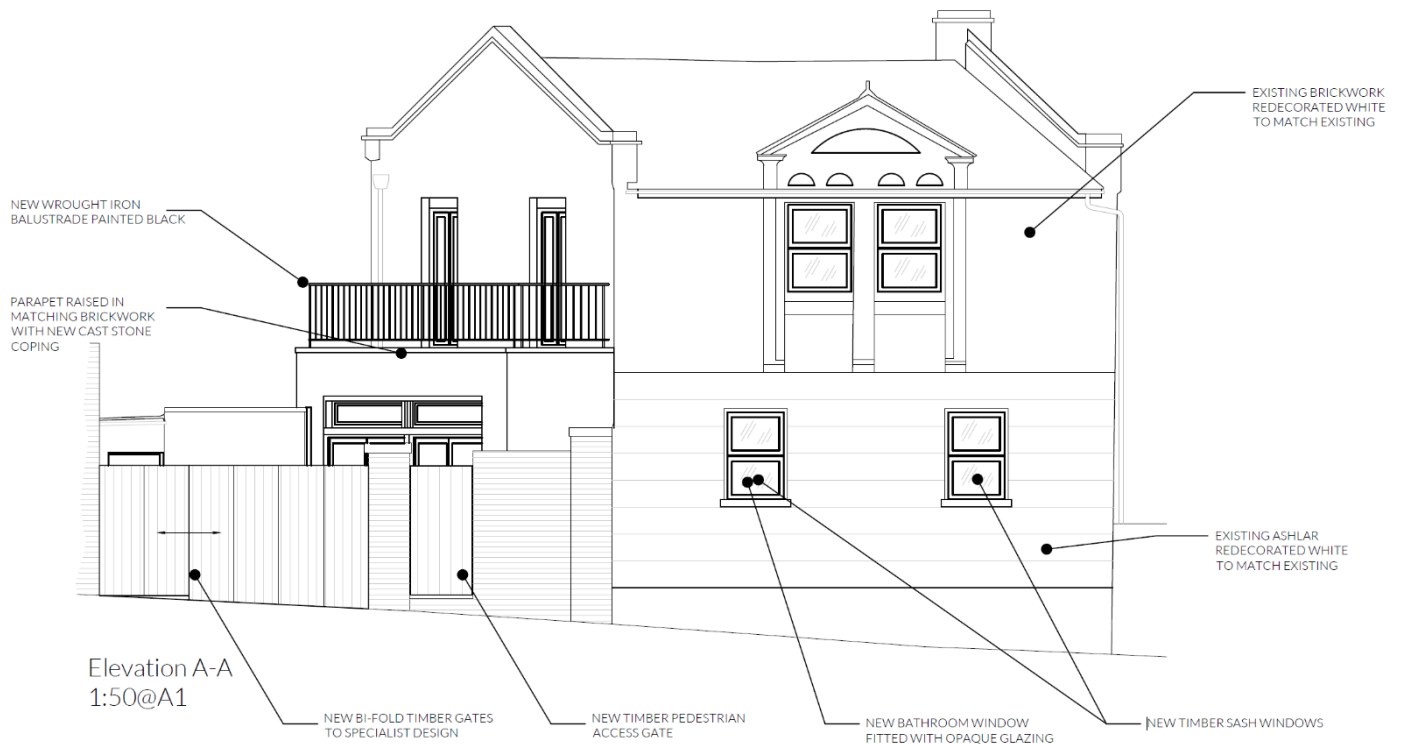
Design and Access Statement

In support of planning application for:

External Alterations to dwelling including 2 new ground floor windows,
larger balcony and vehicular gate.

At:

11A Lyndhurst Road, Camden, London NW3 5PX



This Design and Access Statement supplements the Full Planning Application for External Alterations to dwelling including 2 new ground floor windows, replacement balcony and vehicular gate at 11A Lyndhurst Road, Camden.

1.0 Introduction

1.1 Site Assessment

11A Lyndhurst Road is located within the Borough of Camden, London.

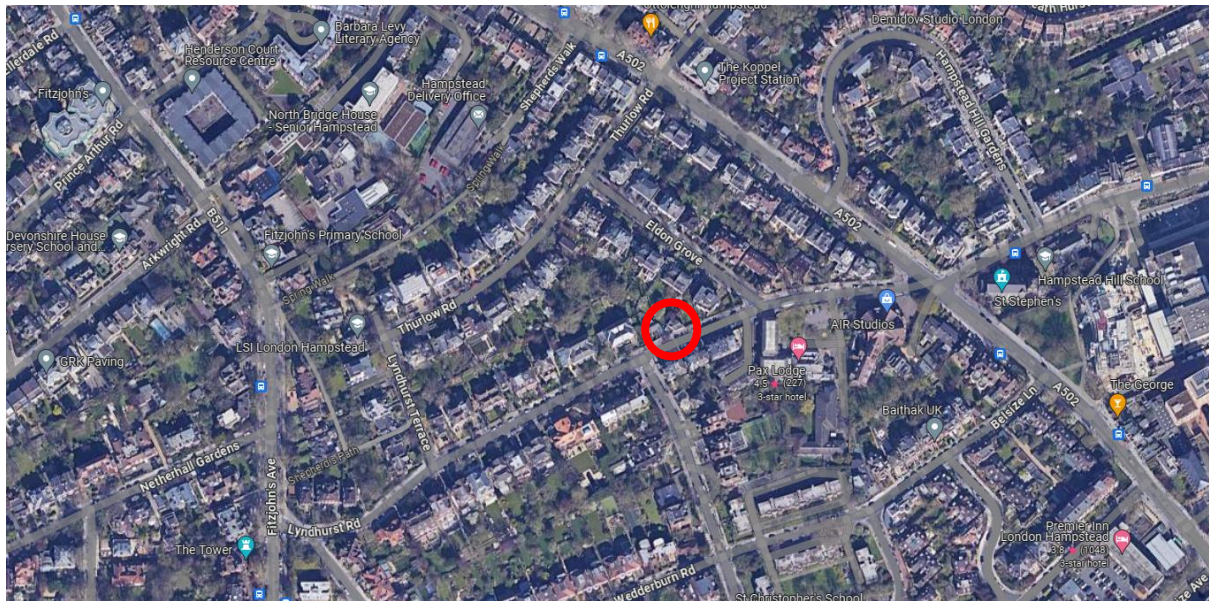


Figure 1: Site Location

1.2 Local Area Character

The site is located on an urban block bounded by Lyndhurst Road, Lyndhurst Terrace, Thurlow Road and Eldon Grove. The properties are typically 3-4 storeys, set back from the road and originally detached or semi-detached with narrow gaps between properties. A number of these gaps have since been filled with smaller subservient buildings which are either extensions to the original dwellings or form new dwellings, such as 11A. The bounding roads including Lyndhurst Gardens are all lined with large trees on the pavements as well as planted front gardens.

A number of properties along these roads are identified as making a positive contribution to the area, including 11A Lyndhurst Road, a very limited number are identified as causing harm. The predominant character of Lyndhurst Road and Lyndhurst Gardens is described in the NHC area as Queen Anne Houses, while there are examples of Gothic Revival at the junction and along Lyndhurst Terrace.

1.3 Planning History

There is no planning history for this site.

1.4 Flood Risk

The property is located within Flood Zone 1 (low probability) and is not vulnerable to flooding.

2.0 Proposals

It is proposed to make external alterations to the existing dwelling, including two new windows on the ground floor, a larger balcony and vehicular gate.

2.1 Design, Layout & Appearance

The existing dwelling comprises of an open plan lounge/kitchen/dining room with bathroom on the ground floor and two bedrooms and a bathroom on the first floor.

It is proposed to reconfigure the internal layout of the ground floor to create a separate kitchen, shower room and open plan lounge/diner.

The new timber windows on Elevation A-A will match the existing style and material of the sash windows on the first floor and are sympathetic to the existing buildings, they have been laid out symmetrically either side of the existing first floor openings. On Elevation B-B the existing French windows have been reconfigured to include fixed lower solid panels. On Elevation D-D the existing window (serving the current ground floor bathroom) will be replaced with a new frame slot window.

The two existing wrought iron Juliet balconies on the front elevation will be replaced with a single larger wrought iron balustrade (painted black) to create a larger balcony area. This will not protrude any further beyond the principal elevation of the building than the existing parapet of the ground floor window.



Figure 2: Existing Juliet Balconies at 11A Lyndhurst Road

The black wrought iron balustrade style of the existing Juliet balconies has been retained as part of the proposed balcony design. We have extended the balcony arrangement so that it is usable

amenity space for the occupiers, as the property has no rear garden and limited external space (most of which is taken up by the car parking).

Due to tree screening and the orientation of the surrounding units this arrangement will not increase overlooking potential or result in loss of privacy. The relationship between 11A and the neighboring dwellings will remain unchanged in this respect.

It is also proposed to replace the existing timber pedestrian access gate and install a vehicular gate in the drive opening of the property. There was previously a vehicular gate in the drive opening of the property, but this had been removed at some point in the past. The proposed gates will both be timber to match the existing gate that is being replaced and the vehicular gate will be of a bespoke bi-fold design to ensure that it can be opened without obstructing the pavement.

2.2 Justification

The proposed external alterations will enable for the reconfiguration of the internal layout, creating a separate kitchen on the ground floor and a larger lounge/dining room space. They will also create additional external amenity space (balcony) and a secured parking area (vehicular gates).

The existing pedestrian access arrangement is redundant as the driveway is open, this will allow for the amenity space to be closed off and made secure, it will also create a private area for the occupiers as currently they have to keep their living room curtains drawn to ensure that no one can look in from the pavement.

The new windows on Elevation A-A will also provide sufficient natural light and ventilation in the new bathroom and kitchen areas.

2.3 Design Inspiration

There is a diverse range of architectural language in the Conservation Area with most construction in masonry with wide variation in detail and articulation including fenestration. The history in the area is progressive and experimental with housing at different scales exploring materiality, scale, layout and ways of living. Many of these are now listed and have been successful in architecture and design awards.

The range is summarised in the Fitzjohn and Netherhall Conservation Area (FNCA) character appraisal as:

1_Queen Anne: This is the predominant style, combining brick and tile construction with small painted windows, towers, multiple gables, oriel windows.

2_Arts and Crafts and Freestyle: Brick and tile construction with complex combinations of dormer and gables.

3_Classical: Elements of classical architecture are evident through the use of pilasters and pediments.

4_Gothic Revivalism: Gothic elements are present in decorative patterning, finials, lancet windows and hood moulds.

5_International Influences: including Dutch gables and mansard roofs with dormers.

6_Inter-War and Post-War Architecture: Buildings in modern styles including the International Style and Post Modernism.

7_Recent Architecture: *'A few recent houses demonstrate quite individualistic and wilfully expressionistic characteristics. These could be seen to fit in with the character of the wider area, which includes scattered avant-garde houses. They are too recent to be of historic interest. However, the best examples are of architectural interest.'* (FNCA pg 17.)

Key materials found in the area are:

- Walls as red brick, London yellow brick or brown brick
- Roofs as plain clay tiles in dark grey or red
- There are some examples of terracotta detailing, stone dressings and decorated plaster work. Windows are mainly timber but with some examples of metal frames in the inter-war and post-war 20th Century buildings.

Key architectural features in the area include:

- Modulation through projection and recession, for example bay windows
- Bay windows and Oriel windows
- Porches with recessed doors
- Prominent gables

In addition to the above more general character appraisal for Lyndhurst Street as set out in the FNCA Management Plan, we have taken design inspiration and design queues from the architectural features of the surrounding properties along Lyndhurst Road which have a direct visual relationship with the application site, to ensure that the proposed scheme is in keeping and sympathetic to the street scene of the Conservation Area.

There are several examples of balconies above parapet windows and wrought iron balustrades on Lyndhurst Road which demonstrates that the proposed external design changes are in keeping with the character and appearance of the street scene and with contribute to local distinctiveness.



Figure 3: Rosslyn Lodge, Lyndhurst Road



Figure 4: 18 Lyndhurst Road

These are local examples of black painted wrought iron balustrade on properties which are visible from the application site. In addition, 18 Lyndhurst Road along with 13 & 14 Lyndhurst Road demonstrate the key architectural features from the character appraisal that we are also included in this scheme.

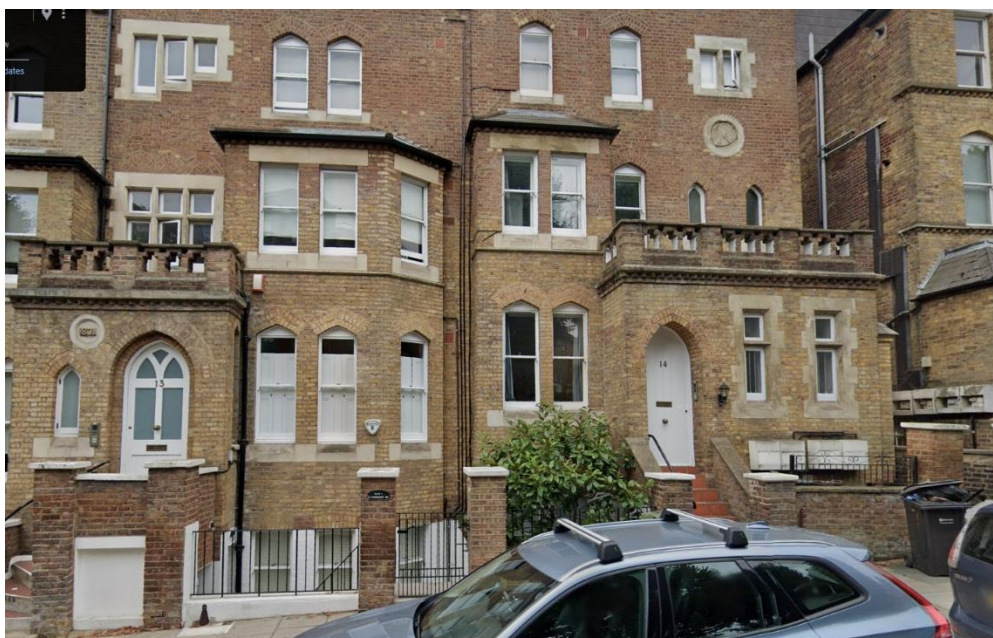


Figure 5: 13 & 14 Lyndhurst Road

There are few examples of private driveways off of Lyndhurst Road, however the few examples available have gates to create a clear distinction between the private and public realms. The gates are generally either wrought iron or timber. We have chosen to replicate the timber gates of the existing pedestrian gate at 11A and the pedestrian and vehicular gates of the directly adjacent 11B Lyndhurst Road, see figure 6 below.



Figure 6: 11B Lyndhurst Road

3.0 Access and Parking

3.1 Access

The access to the property remains mostly unchanged, with the exception of the new vehicular gate to create a secure off-road parking area for the property.

3.2 Parking

The existing parking arrangements for the property are unchanged.

4.0 Conclusions

In conclusion the proposed scheme is acceptable on design and access grounds.

The proposals have taken design queues from the architectural features of the existing property and surrounding buildings. They are in keeping with the architectural and historic context of Lyndhurst Road and will ensure that the dwelling continues to positively contribute to local distinctiveness and the street scene.

We respectfully request that the Planning Officer looks upon this application in a favourable manner.