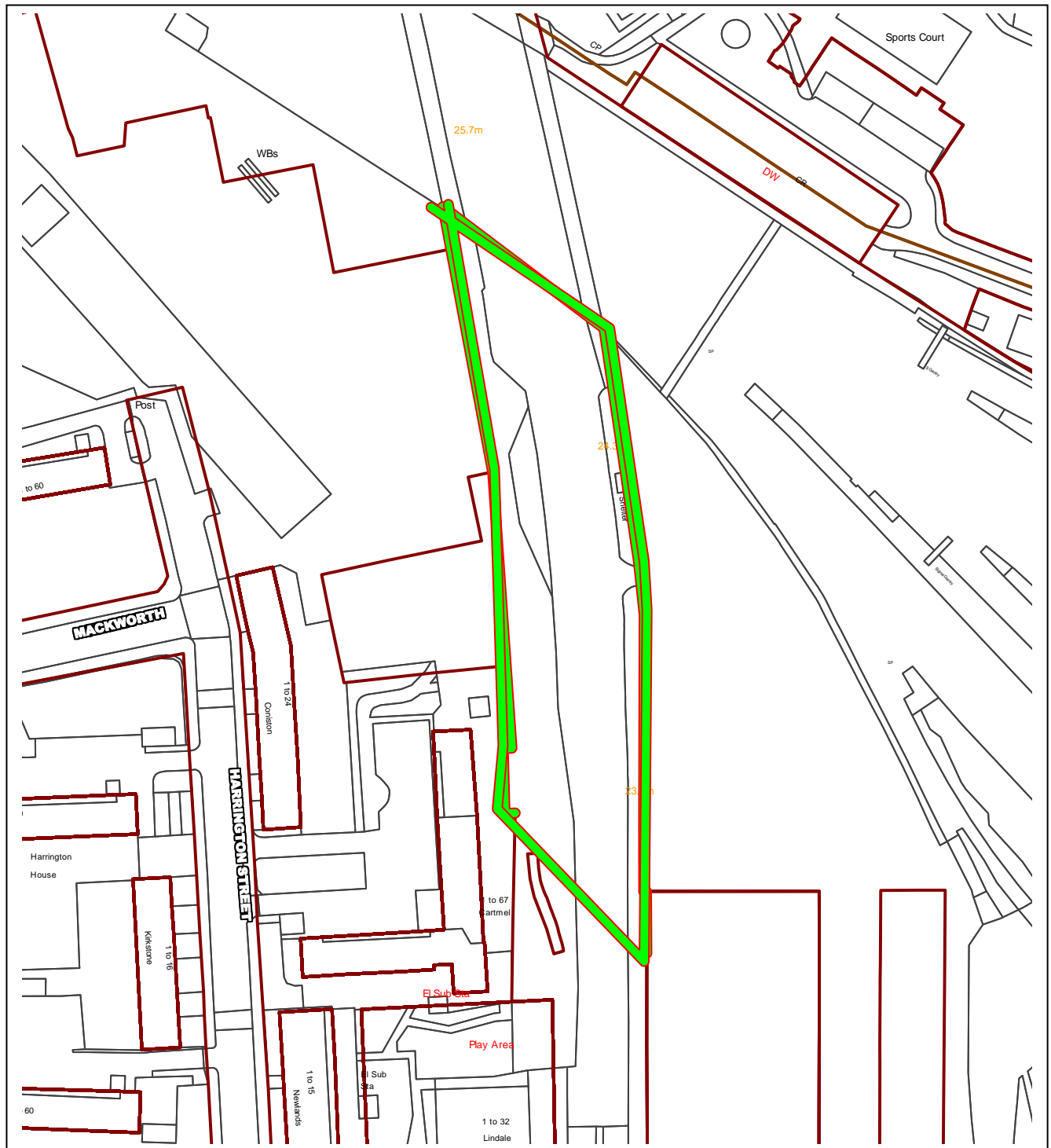
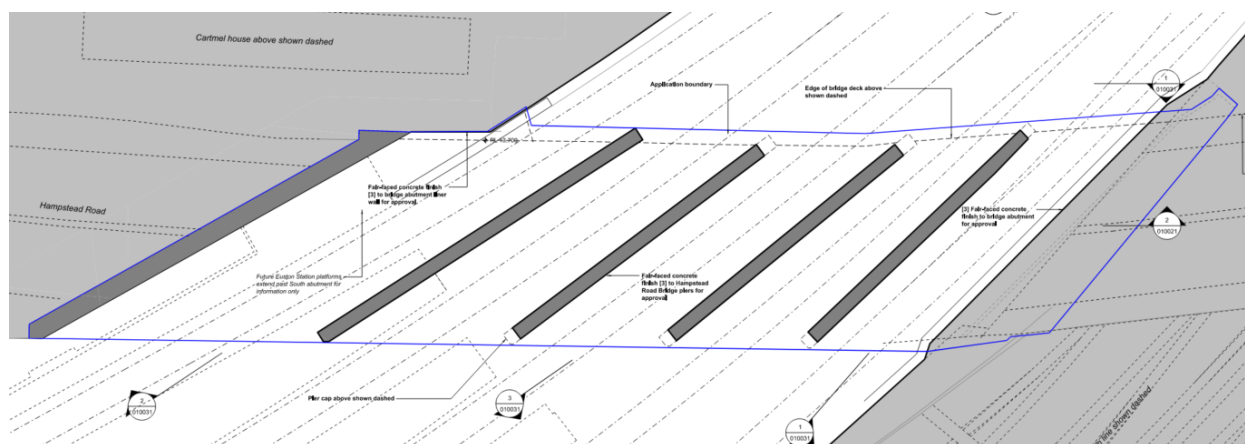
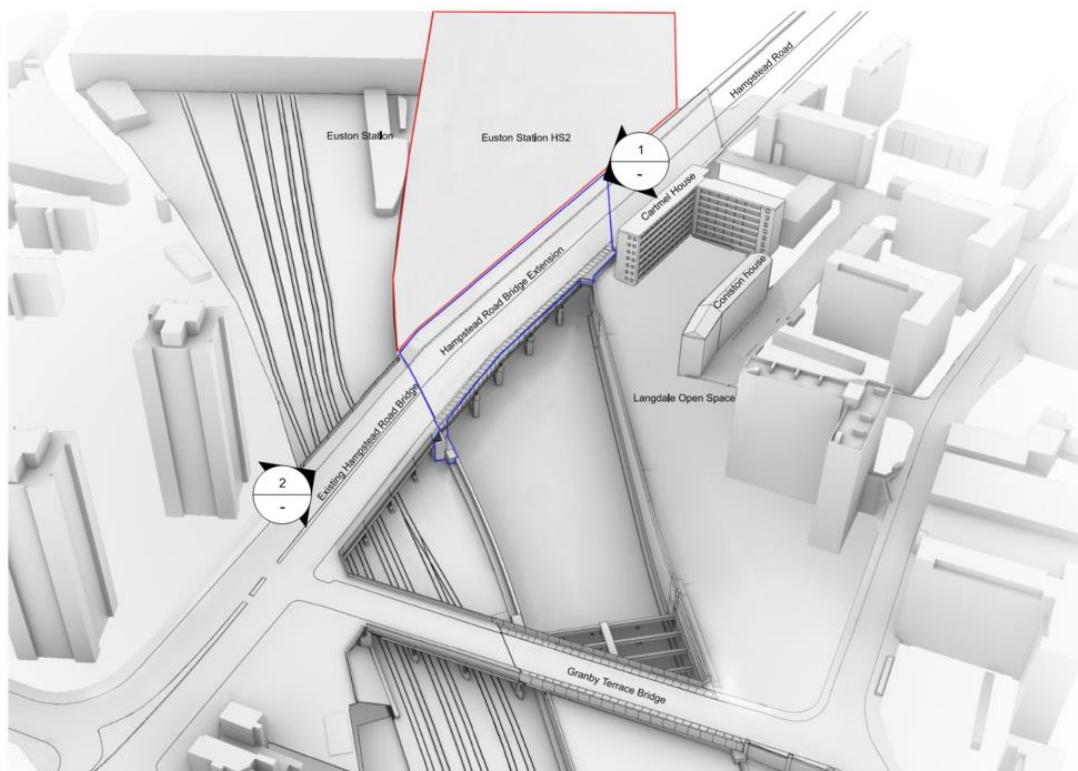


2024/0309/HS2 - Hampstead Road Bridge



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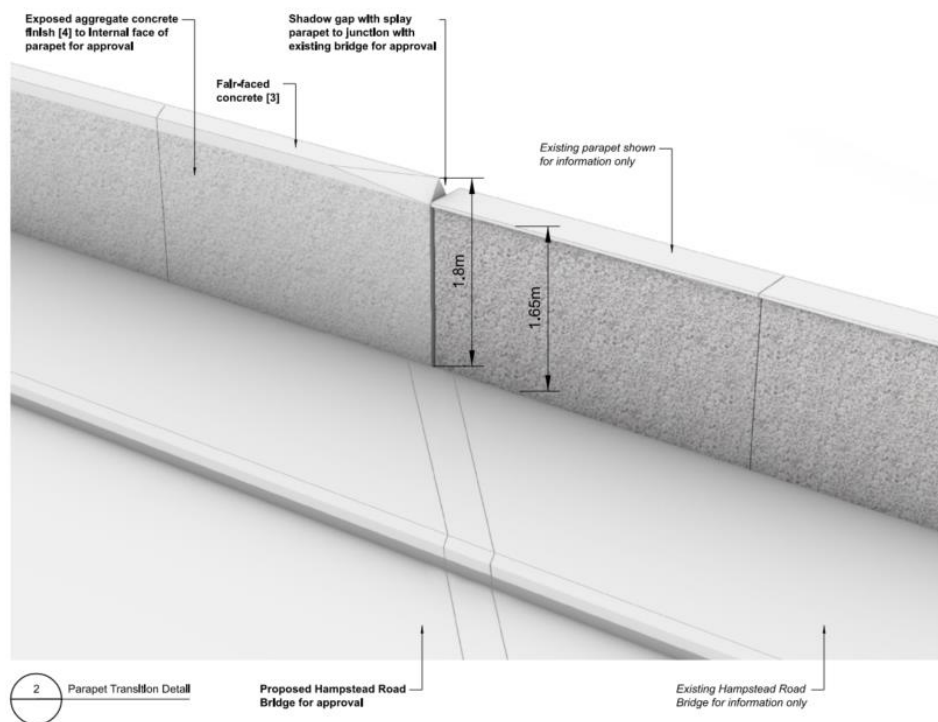


Figure Three: Parapet transition details

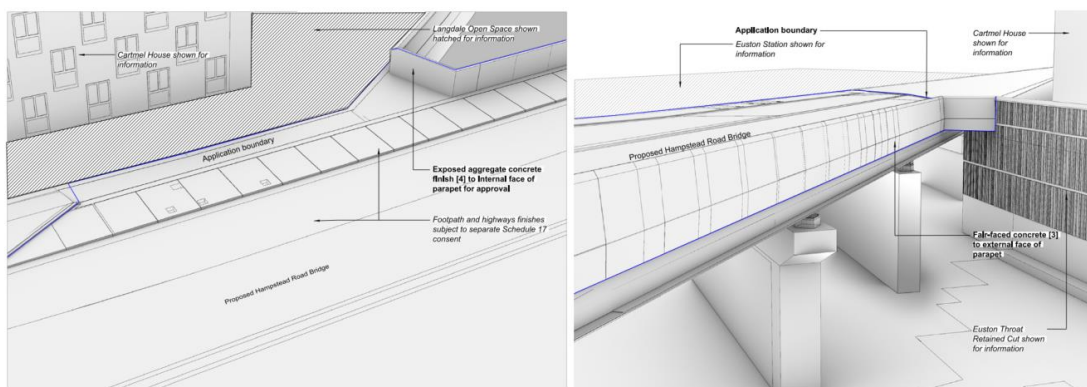
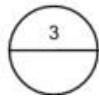
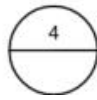


Figure Four: Perspective images showing Langdale open space parapet chamfer



[3] Fair faced concrete finish



[4] Exposed aggregate finish

Figure Three: Parapet materiality

Delegated Report (Members Briefing)		Analysis sheet N/A / attached		Expiry Date: Consultation Expiry Date:	22/03/2024 26/02/2024
Officer			Application Number(s)		
Brendan Versluys			2024/0309/HS2		
Application Address			Drawing Numbers		
Hampstead Road Bridge			Please refer to draft decision notice		
PO 3/4	Area Team Signature	C&UD	Authorised Officer Signature		
Proposal(s)					
Construction of an extension to the existing Hampstead Road Bridge comprising a bridge structure with parapets; associated earthworks.					
Recommendation(s):		Grant consent			
Application Type:		Schedule 17 Application			

Conditions or Reasons for Refusal:	Refer to Draft Decision Notice					
Informatives:						
Consultations						
Adjoining Occupiers:	No. notified	00	No. of responses	4	No. of objections	4
Summary of consultation responses:	Site notices were displayed on 2/02/2024 (consultation expiry date 26/02/2022).					
	Letters of objection have been received from 4 local residents, summarised as follows:					
	<ul style="list-style-type: none">There is insufficient information in the application for Camden to determine the application following the major change of rail engineering function announced by the Government in October for Euston Station.There needs to be confirmation that sections of the west parapet will be removable so that Camden's proposed "mixed use development (mostly housing)" behind the parapet shown on Euston Area Plan can be connected to the street (to provide a living street) otherwise the bridge will not be a pedestrian friendly environment.The proposal would be greatly improved if at least one viewing window each side could look out with a view of the railways (east and west) through both the parapet and any site hoarding along pedestrian pavements prior to the installation of the parapet.Table 6 of the application Written Statement does not comply with Camden's requirements for the "report summarising the key consultation etc" contained in Camden's 'Statement of Community Involvement 2016' in para 3.11.as the plan for the station is still not locked down, indeed is subject to private funding being agreed - so no irreversible engineering works such as bridges should be going ahead in the immediate vicinity.But with only six platforms there is no longer any necessity to extend platforms under the Hampstead Road Bridge.					
	<u>Officer's response:</u>					
	In response to the consultation responses from residents, HS2 have contacted Euston Delivery Sponsor, (High Speed Rail Group) at the Department for Transport, who have advised: <i>Following Government's 'Network North' announcement in October 2023, HS2 Ltd continue to work to determine the specific layout of Euston Station, its platforms, and the associated approach tracks which, as noted, pass underneath Hampstead Road Bridge to the immediate north of the station.</i> <i>The Schedule 17 application for Hampstead Road Bridge is a significant and critical milestone for the delivery of a revised bridge which provides the infrastructure for the trains entering Euston Station. It is also critical in enabling the construction of Euston Station itself, including various enabling works at the north of the station site.</i> <i>It is therefore proposed that the submitted design is taken forward to</i>					

	<p><i>delivery to provide full flexibility in light of this ongoing design activity for the station and to avoid the significant delay, associated extra cost and potentially nugatory work (on already constructed assets) associated with revising designs for the bridge.</i></p> <p>HS2 assert that it is for these reasons cited by Euston Delivery Sponsor, (High Speed Rail Group) and the significant amount of work that that has already been undertaken both in the preparation, construction, and design of ancillary engineering operations, together with the extensive pre-application meetings with LB Camden, that the significant amendments sought by consultees cannot be incorporated into the design of the bridge extension. LB Camden have to consider each Sch 17 application on its own merits once it has been submitted to ensure it is in line with the Act.</p>
Other Consultee comments	<p><u>Transport for London (Infrastructure)</u></p> <p>TfL (Infrastructure Protection) have no comment to make on the application except that HS2 must continue their consultation with TfL and act in accordance with the Protective Provisions Agreement between TfL and the Secretary of State, dated 15th May 2014 as well as the Further Protective Undertakings Agreement.</p> <p><u>Network Rail</u></p> <p>Network Rail confirm they have been consulted in advance of the application and is aware of the proposed works, which are also subject to the terms of a protective provisions agreement; therefore Network Rail have no additional comments to make.</p>

Site Description

The existing Hampstead Road Bridge (HRB) is situated to the northwest of Euston Station.

The existing bridge carries the A400 Hampstead Road (part of the Transport for London Road Network – TLRN) over the West Coast Mainline and forms a key north – south highway route in the area.

The nearest public transport links include Euston Station, Mornington Street Underground Station, and key bus routes with a stop to the south of the existing bridge.

The bridge consists of three traffic lanes (including a bus lane) in each direction either side of a kerbed central island. Guard railings are located along the outside kerb on both sides, and the bridge is lined by parapet walls 1.65m in height on both sides for the entire length. The existing bridge has a slight downward slope to the south.

No part of the site is located within a conservation area, nor does it include any listed buildings or Scheduled Ancient Monuments. However, it is in proximity to the south-eastern most tip of the Camden Town Conservation Area to the north of Hampstead Road Bridge. The surrounding townscape is varied in scale and character with the differing characters of the adjacent conservation areas and post war housing estates to the west and east.

Relevant History

2022/5079/HS2: Lorry routes to and from the HS2 North Main Worksite associated with enabling works and early works packages for the HS2 station at Euston. Enabling works comprise: logistics, ground clearance, installation of pile mat and guide wall, surveying works, exhumations, and utility works. Early work packages relate to the HS2 station, London Underground interchange, and Interim Taxi Rank. Incorporating lorry routes via: Transport for London Road Network (TLRN), Great Portland Street, Albany Street, Osnaburgh Terrace, Osnaburgh Street, Melton Street, Cardington Street, Harrington Square, Churchway, Grafton Place, Eversholt Street, Freight Lane, York Way, Camden Park Road, Wharfdale Road, Caledonian Road, Pancras Road, and Crowndale Road. **Granted 14/03/2023**

2021/0126/HS2: Application for approval under Schedule 17 of High Speed Rail (London - West Midlands) Act 2017 of installation of Wall Berm and Upright Support Structure to structurally support the existing Park Village East retaining wall. The berm will be located in the railway cutting, adjacent to the existing Park Village East retaining wall and extending from Euston Scissor Box (open section) to Parkway Tunnel; and the installation of Euston Scissor Box (open section) - structurally supporting the portal between the Euston Tunnels and Scissor Cut, located within the railway cutting bound by Mornington Street and Granby terrace Bridge and forming part of the portal for the new HS2 tunnels. **Approved 17/03/2021**

2021/0356/HS2: Application for approval under Schedule 17 of High Speed Rail (London - West Midlands) Act 2017 of a two-span expansion to the existing Granby Terrace Bridge (in the Euston Approaches) to provide a connection over the new HS2 rail corridor comprising of a bridge structure and parapets; and the installation of supporting concrete props for the provision of bridge support and ground stability. **Approved 24/03/2021**

2019/6302/HS2: Submission under Schedule 17 of High Speed Rail (London - West Midlands) Act 2017 for plans and specifications for the excavation of a railway cutting involving erection of retaining walls with concrete parapets on top between Hampstead Road Bridge and Granby Terrace Bridge adjacent to the existing West Coast Main Line (to the east).

Refused 26/03/2020. APP/HS2/6 - Appeal Allowed 27th July 2020

2019/4700/HS2 - Lorry routes to and from the Euston Approaches and Adelaide Road worksites associated with works for HS2. Main works activities include: Construction of the Park Village East retaining wall, portal and high-speed dive unders including the installation of ground anchors; Removal of excavated material from the station approach, tunnel portal and headhouse works; Construction of the decks over the high-speed dive under and railway south of Mornington Street Bridge; Construction of the west and east side retaining wall around Hampstead Road Bridge; Extension of Hampstead Road Bridge as well as associated utilities and highway works; Support the movement of plant and material down into the Euston approach railway cutting; Support the removal of excavated material generated in the railway cutting; Construction of Adelaide Road vent shaft and single storey headhouse building; and all other activities for the purposes and in connection with the scheduled and ancillary works. Incorporating lorry routes detailed in 'List of Roads for Approval' document. Recommended for refusal. Decision appealed and allowed 25/08/2020.

Relevant policies

The HS2 Act

- High Speed Rail (London - West Midlands) Act 2017, in particular Schedule 17 paragraph 6

Statutory Guidance

- High Speed Rail (London - West Midlands) Act 2017 - Schedule 17 Statutory Guidance

Environmental Minimum Requirements and related documents

- High Speed Rail (London - West Midlands) Environmental Minimum Requirements (the EMRs) General Principles February 2017
- High Speed Rail (London-West Midlands) Environmental Minimum Requirements Annex 1: Code of Construction Practice High Speed Rail (London-West Midlands) Environmental Minimum Requirements Annex 1: Code of Construction Practice
- High Speed Rail (London-West Midlands) Environmental Minimum Requirements Annex 2: Planning Memorandum
- High Speed Rail (London-West Midlands) Environmental Minimum Requirements Annex 3: Heritage Memorandum
- High Speed Rail (London-West Midlands) Environmental Minimum Requirements Annex 4: Environmental Memorandum
- HS2 Context Report October 2017
- London - West Midlands Environmental Statement 2013
- Supplementary Environmental Statement 4 and Additional Provision 5 (Supplementary Environmental Information) 2015
- HS2 Phase One information papers: environment (series E)
- Local Environmental Management Plan London Borough of Camden (LEMP) December 2017
- Camden Local Traffic Management Plan
- Local Traffic Management Plan (S3 Main Works - Early Works Packages)
- The Dales Local Traffic Management Plan
- High Speed Two Phase One: Route-wide Traffic Management Plan

Assessment

1. Background

Legislation and policy context

- 1.1 Phase One of High Speed 2 (HS2) is the first phase of a new high speed railway network proposed by the Government to connect major cities in Britain.
- 1.2 On 23rd February 2017, Royal Assent was granted, namely the High Speed (London-West Midlands) Act 2017 ("the HS2 Act"), for Phase One of HS2. The HS2 Act provides powers for the construction and operation of Phase One of HS2.
- 1.3 High Speed Two (HS2) Limited is the company responsible for developing and promoting the UK's new high speed rail network. It is funded by grant-in-aid from the government.
- 1.4 In order to carry out HS2 works in Camden, HS2 Ltd (a nominated undertaker) is submitting a series of lorry route applications under Schedule 17 of the HS2 Act, to govern the transportation of materials to and from construction sites.
- 1.5 Paragraph 4.4 of the DfT's Schedule 17 Guidance notes states; '*These approvals have been carefully defined to provide an appropriate level of local planning control over the works while not unduly delaying or adding cost to the project*'.
- 1.6 Schedule 17 of the HS2 Act sets out the approvals required to be obtained by the nominated undertaker. These approvals include lorry route applications under Paragraph 6 of Schedule 17.
- 1.7 The Council can only consider the application within the constraints of the HS2 Act, rather than planning policies set out in the Development Plan. The grounds for determination under the HS2 Act which the Council can base its decision to approve the application and attach reasonable conditions, or to refuse the application, are where the arrangements ought to be modified:
- to preserve the local environment or amenity;
 - to prevent or reduce prejudicial effects on road safety or on the free flow of traffic in the local area; or
 - to preserve a site of archaeological or historic interest or nature conservation value;
- and are reasonably capable of being so modified.
- 1.8 Any representations received from the public or third parties will be considered by the Council but within context of the HS2 Act.
- 1.9 It is important to note that the HS2 Act states that all applications must be determined within eight weeks of submission (unless the Council and the Nominated Undertaker agree an extension of time for determination), or the application is deemed to have been refused.

Framework of assessment

- 1.10 If Schedule 17 lorry route applications are assessed to be acceptable, then the Council, as determining planning authority, would only have discretion to attach conditions for any approval on the grounds raised in paragraph 1.7 above. Importantly, conditions can only be attached with prior agreement from HS2 Ltd.
- 1.11 The DfT's Schedule 17 Guidance notes states: '*The purpose of this is to allow the nominated undertaker and the planning authority the opportunity to agree whether the condition is necessary and appropriate, and would not unreasonably impede the building and operation of the railway, prior to the planning authority issuing its decision. It also avoids the potential for delay that would*

result from decisions being issued with inappropriate conditions

- 1.12 In addition to the planning regime described above, the HS2 Act contains provisions setting out the protections to be provided for various bodies with statutory responsibilities, likely to be affected by the works that impact accesses to highways affecting traffic, stopping up, diversion and interference with the highway, highways, water and listed buildings.

Additional environmental and community protection measures

- 1.13 The High Speed Two (HS2) Phase One Environmental Statement (ES) was produced to accompany the HS2 Act. The ES includes the likely significant environmental impacts along the route along with the measures to manage and reduce these impacts. In order to ensure that the environmental effects of the project do not significantly exceed those assessed in the ES, Environmental Minimum Requirements (EMRs) sit alongside the statutory environmental controls included in the HS2 Act. Throughout the construction and operation of the project (Phase One), HS2 Ltd and its contractors will be required to comply with both the EMRs and those statutory environmental controls.
- 1.14 The EMRs are a group of documents that have been developed in consultation with local authorities and other relevant stakeholders. Paragraph 1.1.5 of the EMR General Principles document states that the nominated undertaker is to use reasonable endeavours to adopt mitigation measures to further reduce the adverse environmental effects reported in the ES, provided that such measures are reasonably practicable and do not add unreasonable cost or delay to the construction or operation of the project.
- 1.15 The Code of Construction Practice (CoCP) is Annex 1 of the EMRs. It sets out specific details and working practices in relation to site preparation (including site investigation and remediation, where appropriate), demolition, material delivery, excavated material disposal, waste removal and all related engineering and construction activities. The CoCP sets out the measures that the nominated undertaker and contractors are required to implement in order to limit disturbance from construction activities, as far as reasonably practicable, including traffic and transport.
- 1.16 Local Environmental Management Plans (LEMPs) have been prepared for each local authority area which set out site specific control measures to be adopted by HS2 Ltd.'s Contractors.
- 1.17 HS2 Ltd. is required to prepare Local Traffic Management Plans (LTMP) for areas such as Camden that are impacted by HS2. The LTMPs build on the general environmental requirements contained in the CoCP and a route wide traffic management plan and set out how the project will adapt and deliver the required traffic management measures. HS2 Ltd. has produced an LTMP which sets out how Mace Dragados Joint Venture (MDJV) will deliver the Logistics and Traffic Management requirements for the HS2 Sector 3 (S3) Main Works Station Contract Early Works Packages within the London Borough of Camden.
- 1.18 The LTMP has been developed by MDJV with input from HS2, London Borough of Camden, the emergency services and TfL. The LTMP has been consulted on through the Camden Traffic Liaison Group which have been established in accordance with the CoCP and the RTMP.
- 1.19 The LTMP is a 'live' document that will be added to and revised as further details of the proposals, works packages and associated traffic management arrangements are worked up and implemented. The current LTMP structure details matters including highway/traffic management and the cumulative traffic impacts of the combined HS2 programme of enabling works, plus appended details of works packages, as they come forward. It should be noted however, that the LTMP is a supporting document only, and is not for approval as part of this lorry route application; however, attention is drawn to HS2 Ltd.'s commitment to Parliament to comply with the

Environmental Minimum Requirements (EMR's). The Code of Construction Practice for HS2 arises from the EMR's and requires HS2 Ltd. to prepare Local Traffic Management Plans for areas such as Camden that are significantly impacted by HS2 construction works. HS2 Ltd. and its contractors are therefore effectively committed to complying with the contents of the S3 Early Main Works Mini LTMP. In considering applications for lorry routes, Camden as a qualifying authority, should have due regard to the system of controls available under the HS2 Act and should not seek to duplicate controls that it already contains.

1.20 The LTMP document provides some context under which lorry route applications would be considered. As such, it provides information on the movement of HS2 construction traffic to construction sites, building on that set out in the Environmental Statement, and provides further information on its traffic management and refers to undertakings and assurances provided during the Parliamentary process of consideration of Phase 1 of HS2.

1.21 It is worth noting that HS2 Ltd. has indicated that it does not currently intend to undertake further assessments (such as modelling) on impacts on the local road network, including at junctions, impacts on highway safety, other highway users, pedestrian severance for example to those provided in the ES, unless the anticipated impacts significantly exceed those in the ES.

2. Proposal

2.1 The request for approval of plans and specifications has been made under paragraphs 2 and 3 of Schedule 17 of the HS2 Act.

2.2 The works submitted for approval and their ground for approval under the HS2 Act include:

- A five-span continuous overbridge, with a maximum 138 metres in length. The bridge extension would form an extension to the existing Hampstead Road Bridge to support the Hampstead Road highway over the new HS2 railway.
- 1.8m high precast concrete parapets at 3 locations:
 - Western bridge parapet;
 - the Langdale Open Space interface parapet; and
 - the eastern interface parapet.
- Lining walls on the southern abutment, northern abutment, and bridge piers 1 to 4. The lining walls will have a smooth faced concrete finish.
- Northern abutment retaining wall with a smooth-faced concrete finish.

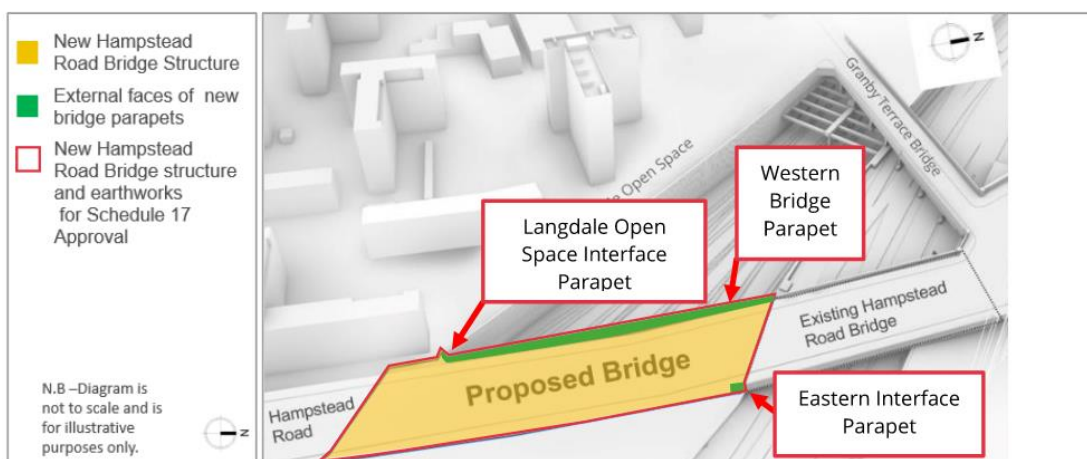


Figure 1: Elements for approval within this Schedule 17 application at highway level. The location of the parapets for approval are shown in green.

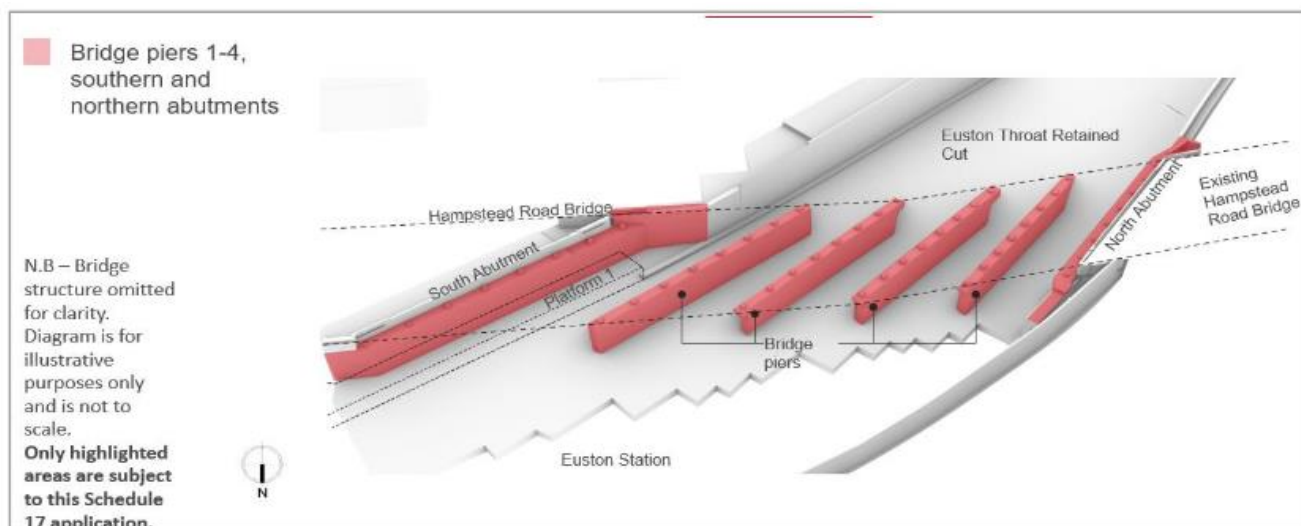


Figure 2: Elements for approval within this Schedule 17 application at track level (shown in red).

2.3 The proposed section of new bridge will be constructed to link directly with the existing bridge, spanning over the new HS2 tracks beneath. It will comprise a five-span continuous overbridge, 138 metres in length, constructed with steel girders made composite with a reinforced concrete deck slab on top. The existing Network Rail bridge is approximately 70 metres in length; therefore, the new section of bridge will extend this in total proportionally by approximately three times.

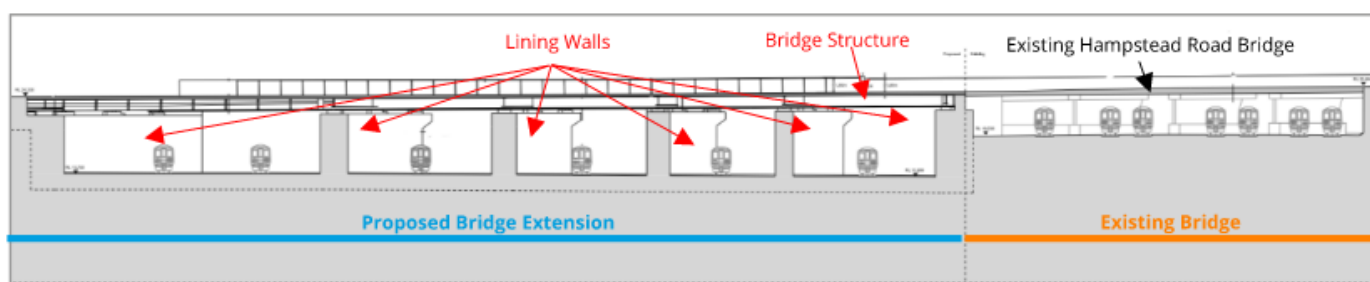


Figure 3: Long section looking north (extract from drawing 1MC03-SCJ_SDH-AR-DSE-SS01_SL12-010021). Elements for Schedule 17 approval under this submission are labelled in Red.

Note: The urban realm and highways elements, and highway layout, would require separate Schedule 17 and Schedule 4 highways consents respectively. These elements are still under development and does not form part of this application.

Revisions:

No revisions were received during the course of this application.

3. Assessment

3.1 The main considerations in relation to this proposal are:

- Local environment or local amenity;
- Impact on archaeological, historic and nature conservation value;
- Amenity;
- Effects on road safety or on the free flow of traffic in the local area

3.2 The Council notes that the application is made under paragraphs 2 and 3 of Schedule 17 of the HS2 Act (as noted at paragraph 3.2.3 of the Written Statement submitted in support of the application). The applicant has stated the nature of the works submitted for approval and their grounds for approval under the HS2 Act. Works of this type have been clarified by the Planning Appeal reference APP/HS2/6 where the Inspector considered whether the proposed parapet walls (for the erection of retaining and parapet walls between Hampstead Road Bridge and Granby Terrace Bridge) can lawfully be considered as part of the earthworks, in terms of Schedule 17 of the Act.

Local environment or amenity

3.3 When determining an application for planning permission regard must be had to the development plan and to other material considerations and the determination must be made in accordance with the development plan unless material considerations indicate otherwise (see section 70(2) of the Town and Country Planning Act 1990 and section 38(6) of the Planning and Compulsory Purchase Act 2004). The current application is not made under the TCPA, but under Schedule 17 of the HS2 Act and therefore the statutory duties imposed by the 1990 and 2004 Acts do not apply. However, the policies of the development plan set out the Council's general approach to dealing with matters of design, conservation, archaeology, amenity and transport are a helpful reference point in terms of making an assessment of an application of this nature under Paragraph 3 of Schedule 17 of the HS2 Act.

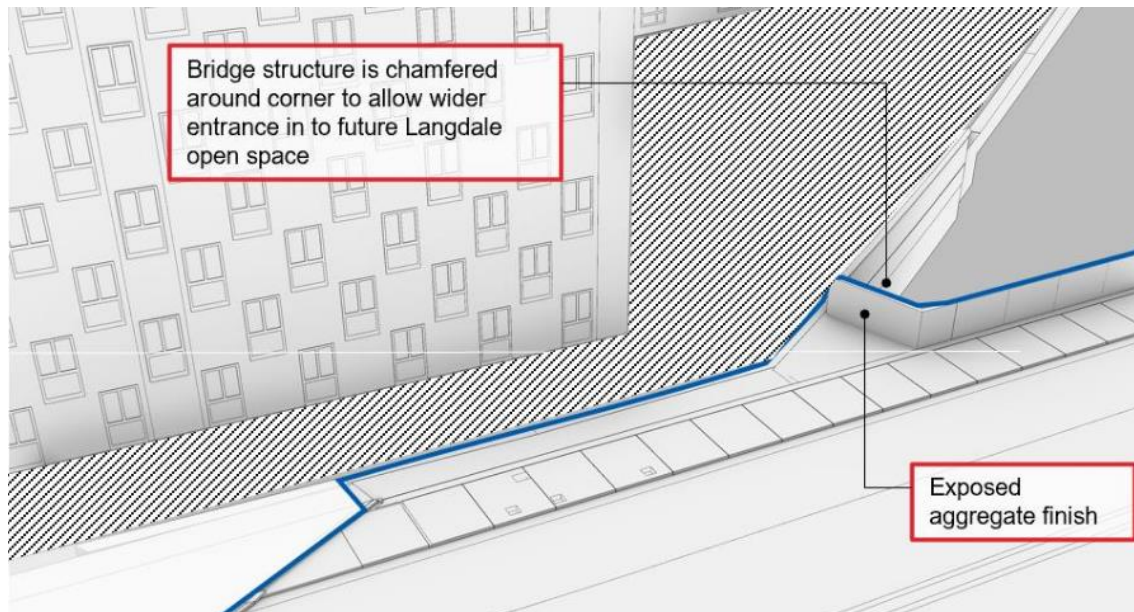
3.4 The proposals seek to extend the existing Hampstead Road Bridge across the widened railway cutting to accommodate the additional rail tracks into/out of Euston Station as part of the HS2 proposals. The existing bridge is owned by Network Rail and would be retained in situ and merged.

3.5 The aim of the bridge design has been to integrate the appearance of the bridge extension with the existing bridge, with particular attention to the experience of its users. With regards to the proposed bridge structure and parapet walls, a number of design solutions and iterations have been reviewed with the applicant, Planning Officers and Urban Designers at pre-application stage to reach the existing proposals. A primary constraint has been HS2 bridge design requirements and codes of practice relating to structural strength and efficiency, user safety and the discouragement of graffiti and climbing. A further consideration has been the treatment of the transition between the existing bridge parapet walls and the proposed parapet walls in terms of the differing heights and material continuity. Additionally, while not proposed as part of this application, there is the potential to integrate artwork into the bridge and more specifically the bridge's parapet walls. HS2 may explore the potential for integration of art work within the bridge in the future, when greater clarity is provided on the more significant elements of the HS2 proposals in the Euston area, as and when they are brought forward.

3.6 The existing bridge parapet walls are 1.65m high, but the proposed bridge extension requires parapet walls with a height of 1.8m to meet current safety standards. The profile to the top of the proposed parapet walls would also be different to the existing parapets, for the safety of the wall as a barrier and to discourage climbing. The proposed 'shadow gap' is considered to provide the most effective and 'honest' solution rather than, for example, splicing the two structures together in an attempt to disguise the transition. This approach is similar to that which was taken on the neighbouring Granby Terrace Bridge. The addition of a splay to the triangular 'steeple' top of the proposed parapet walls adjacent to the existing flat-topped parapets aids the transition between the differing geometries of the parapet walls. A condition will be added to secure the submission of further detailed drawings of the transition between the existing and proposed parapet walls, including the shadow gap and splay to the proposed coping, to both sides of the bridge, to ensure a successful transition.

3.7 Regarding the proposed short section of eastern parapet wall, that will join the existing Hampstead Road Bridge parapet to the HS2 Euston Station, this incorporates a chamfer to accommodate any future development within the station boundary.

3.8 Similarly, the Langdale Open Space parapet, is chamfered around the corner to match the parapet along the western wall of Euston Throat Retained Cut (ETRC). The chamfer would allow visibility from Langdale Open Space towards the station and vice-versa, creating additional space and amenity to public areas at this key junction of the Hampstead Road bridge, HS2 Euston Station, the pedestrian crossing across Hampstead Road, and Langdale Open Space. Whilst the details of such proposal is unknown and in the future, Officers considered that this element presents a number of opportunities to create a welcoming entrance to any proposals for Open Space in the future.



3.9 With regard to materiality for the parapet walls, to provide a degree of continuity between the existing flint finish to the pavement facing parapet wall, an exposed aggregate finish is proposed as the preferred option. This option is considered to provide a continuity of character of the wall surface whilst differentiating itself as a new addition. It also provides a deterrent to graffiti. The precise finish of the exposed aggregate will be the subject of an approval of details application to secure the appropriate particle size and colour and therefore a condition is recommended to be added to any permission.

3.10 As with the existing bridge, the visibility the exterior façade of the proposed parapet walls and the bridge structure, is considered to be limited due to the heights of surrounding walls and sight lines. To the extent that it will be visible, it is considered to successfully integrate with the materiality of the ETRC and the slab structure of the existing Hampstead Road Bridge through its construction in smooth-faced concrete. As such, it is considered that the proposed structures will be viewed in association with the railway infrastructure and it is not considered that they will cause harm to the setting of the adjacent conservations areas or any surrounding listed buildings.

3.11 The proposed retaining and liner walls would be cast against the ground and would have limited visibility from the public realm. The walls will however be visible from the HS2 trains coming into/out of Euston Station. The liner walls will have a smooth-faced concrete finish, which would differ from the ribbed concrete finish of the adjacent ETRC retaining walls. This is required due to maintenance, safety and buildability issues which prevent this section of the wall from also having a ribbed concrete finish. In the context of the above, this finish is considered acceptable.

Archaeological, historic or nature conservation value

- 3.12 The site is not located within an area of archaeological interest, and as such, the proposed development is not considered to result in harm to the archaeological interest of the site. However, with regard to cultural heritage (including archaeological or historic interest), it is noted that control measures are outlined within the Code of Construction Practice (CoCP) (Section 8), E8: Archaeology. This is in addition to the HS2 Phase One Heritage Memorandum within the EMRs.
- 3.13 In a similar vein to paragraphs 189 - 192 (Proposals affecting heritage assets) of the National Planning Policy Framework, a route-wide Generic Written Scheme of Investigation: Historic Environment Research and Delivery Strategy (GWSI:HERDS) has been prepared which sets out the general principles for design, evaluation, mitigation, analysis, reporting and archive deposition which is to be adopted for the design development and construction of the HS2 scheme.
- 3.14 While the site itself is not located within a Conservation Area, it is located within relative proximity of the Camden Town Conservation Area. As discussed within the local environment section above, it is not considered that the proposal would harm the setting of the adjacent conservation area. This is due to the limited visual impact afforded from both public and private vantage points. The structures would be read as part of the railway structure and infrastructure rather than a separate structure within the immediate setting of the conservation area.

Amenity

- 3.15 In considering the amenity impacts of the proposal, it is not considered that the proposals result in loss of outlook nor is it considered to result in a poorer quality visual amenity to the properties along Hampstead Road that face onto the proposed structures, and as a result local amenity will be preserved. Given this scale and siting of the proposal, the development is not considered to have a detrimental impact on neighbouring residential amenity in terms of loss of light, privacy, overlooking or a sense of enclosure.
- 3.16 It is noted that there are a number of regulatory controls to mitigate impacts to residential amenity. These include those under the EMRs, CoCPs, LTMPs, LEMPs, and the assurances specific to Camden alongside the other statutory environmental controls included in the HS2 Act and the assurance that HS2 Ltd. Therefore, there are no outstanding additional issues with regards to the local environment or amenity, which would warrant grounds for refusal on this matter.

Effects on road safety or on the free flow of traffic

- 3.17 The Council's Highways department raised no objection if the development is constructed in line with the HS2 Act.
- 3.18 TfL were consulted and raised no objection. HS2 have advised the A400 Hampstead Road is required to stay open to vehicular, pedestrian and cycle traffic for the duration of the construction works (with exceptions of agreed road closures approximately once per year during the construction period). To facilitate this, a temporary road will be provided to divert traffic past areas of construction. A phased programme of works will also be adopted to minimise highways disruption. Separate applications for the temporary road, as well as the construction and realignment of Hampstead Road bridge highway and road lighting will be submitted under Schedule 4 of the HS2 Act in due course. Lorry routes will also be subject to a separate Schedule 17 application, which will carefully consider the impact on pedestrian and cyclist safety.

4. Recommendation

4.1 Approval of plans and specifications pursuant to paragraphs 2 and 3 of Schedule 17 of the HS2 Act.

The decision to refer an application to Planning Committee lies with the Director of Regeneration and Planning. Following the Members Briefing panel on Monday 3rd June 2024, nominated members will advise whether they consider this application should be reported to the Planning Committee. For further information, please go to www.camden.gov.uk and search for 'Members Briefing'.

Application ref: 2024/0309/HS2
Contact: Brendan Versluys
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Email: Brendan.Versluys@camden.gov.uk
Date: 29 May 2024

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Harrington Street
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NW1 3SJ
United Kingdom

DRAFT

Dear Sir/Madam

DECISION

High Speed Rail (London - West Midlands) Act 2017

Schedule 17 - Conditions of Deemed Planning Permission Approval

Address:

Hampstead Road Bridge

Proposal:

Construction of an extension to the existing Hampstead Road Bridge comprising a bridge structure with parapets; associated earthworks

DECISION

Drawing Nos: 1MC03-SCJ_SDH-AR-DGA-SS01_SL12-010012, rev C01; 1MC03-SCJ_SDH-AR-DGA-SS01_SL12-010011, rev C01; 1MC03-SCJ_SDH-AR-DEL-SS01_SL12-010031, rev C01; 1MC03-SCJ_SDH-AR-DGA-SS01_SL12-010041, rev C01; 1MC03-SCJ_SDH-AR-DDE-SS01_SL12-010042, rev C01; 1MC03-SCJ_SDH-AR-DSE-SS01_SL12-010021, rev C01; 1MC03-SCJ_SDH-AR-DGA-SS01_SL12-010042, rev C01; 1MC03-SCJ_SDH-AR-DGA-SS01_SL12-010001, rev C01; Proforma 1MC03-SCJ-IN-APP-SS01_SL12-000002, rev C02; Written Statement prepared by 1MC03-SCJ-IN-STA-SS01_SL12-000003, rev C02; Submission letter 1MC03-SCJ-IN-TEM-SS01_SL12-000001, rev C02

The Council as the Local Planning Authority & Qualifying Authority within the meaning of the above Act has **granted** permission subject to the following condition(s) and informative(s) listed below:

Condition(s) and Reason(s):

- 1 The proposed works hereby permitted shall be carried out in accordance with the following approved documents:

1MC03-SCJ_SDH-AR-DGA-SS01_SL12-010012, rev C01; 1MC03-SCJ_SDH-AR-DGA-SS01_SL12-010011, rev C01; 1MC03-SCJ_SDH-AR-DEL-SS01_SL12-010031, rev C01; 1MC03-SCJ_SDH-AR-DGA-SS01_SL12-010041, rev C01; 1MC03-SCJ_SDH-AR-DDE-SS01_SL12-010042, rev C01; 1MC03-SCJ_SDH-AR-DSE-SS01_SL12-010021, rev C01; 1MC03-SCJ_SDH-AR-DGA-SS01_SL12-010042, rev C01; 1MC03-SCJ_SDH-AR-DGA-SS01_SL12-010001, rev C01.

Reason: For the avoidance of doubt and in order to define the permission in accordance with Schedule 17 to the High Speed Rail (London - West Midlands) Act 2017.

- 2 Before the relevant part of the work is begun, detailed drawings, or samples of materials as appropriate, in respect of the following, shall be submitted to and approved in writing by the local planning authority:

a) Details of the particle size and colour of the aggregate for the parapet walls (to be submitted to the Local Planning Authority), and a sample panel of those materials of not less than 1m x 1m demonstrating colour and texture (to be provided on site).

b) Plans, elevation drawings, details and section drawings at a scale of 1:20 demonstrating the transition between the existing and proposed bridge parapet walls, including the shadow gap and splay to the proposed coping, to both the north and south sides of the bridge.

The relevant part of the works shall be carried out in accordance with the details thus approved and all approved samples shall be retained on site during the course of the works.

Reason: To preserve the local environment, amenity, and the historic interest of the immediate area in accordance with Schedule 17 to the High Speed Rail (London - West Midlands) Act 2017.

Informative(s):

- 1 In accordance with assurances given by the Secretary of State, the applicant is reminded that HS2 Phase 1 Environmental Minimum Requirements must at all times be fully complied within undertaking the works.
- 2 In accordance with the Phase 1 Code of Construction Practice, the applicant must adhere to the control measures set out in the HS2 Phase 1 Route-wide Traffic Management Plan and the Ealing Local Traffic Management Plan. The measures contained in the Local Traffic Management Plan must be kept under review during the execution of the works, in consultation with TfL, London Borough of Camden and other relevant stakeholders.

Yours faithfully

Chief Planning Officer

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DECISION