

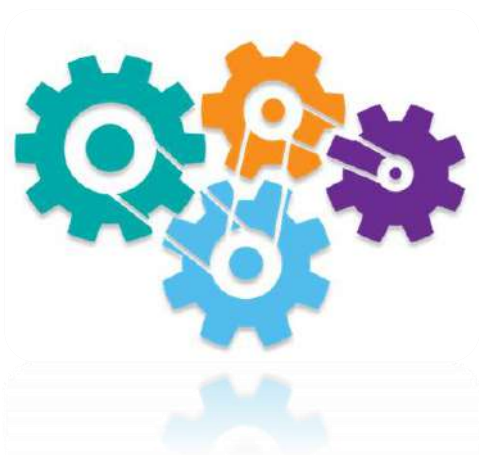


39A Fitzjohn's
Avenue & 46
Maresfield
Gardens

Transport Statement (inc.
Delivery and Servicing Plan)

February 2024

Ref 22-10104



Quality Standards Control

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<i>Revision</i>	<i>Initial</i>	<i>Rev A</i>
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Prepared by	JF	JF
Checked by	LA	LA
Authorised by	AK	AK

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1 Introduction

1.1 Overview

- 1.1.1 Syntegra Consulting Ltd (“SC”) have been commissioned to produce this Transport Statement (TS) in connection with the submission of a planning application for the redevelopment of 39A Fitzjohn’s Avenue and land adjacent to 46 Maresfield Gardens, Camden.
- 1.1.2 The proposals comprise the redevelopment of 39A Fitzjohn’s Avenue to provide two townhouses and two maisonettes, and the development of 29 apartments on land adjacent to 46 Maresfield Gardens. The proposed site layout is included at **Appendix A**.
- 1.1.3 The site is located approximately 600m south of Hampstead and is bound to the north by existing residential development, to the east by Fitzjohn’s Avenue, to the south by Nutley Terrace and to the west by Maresfield Gardens. The site location is indicated at **Figure 1**. The site is within a PTAL 5/6a, indicating high levels of public transport accessibility.
- 1.1.4 The Local Planning Authority (LPA) and Local Highway Authority (LHA) is the London Borough of Camden (LBC).

1.2 Scope

- 1.2.1 This TS has been prepared to consider the impact of the proposals on the local highway network and will demonstrate that the proposals constitute sustainable development, compliant with transport planning policy. The report sets out the following elements:
- Section 2 – describes in detail the site location, existing use, and the local highway network, including a review of existing road safety;
 - Section 3 – describes the existing sustainable transport infrastructure;
 - Section 4 – sets out the development proposals;
 - Section 5 – sets out the anticipated impact of the development on the local highway network;
 - Section 6 – provides a framework Delivery and Servicing Management Plan; and
 - Section 7 – provides a summary and conclusions.

2 Transport Planning Policy

2.1 National Policy

- 2.1.1 The National Planning Policy Framework (NPPF) came into effect in 2012. The document was designed to supersede and simplify previous national planning documents and their policies. The latest revision of the NPPF was adopted in 2023.
- 2.1.2 The preparation of this TS is consistent with the guidance set out in the NPPF, which advocates the submission of such documents to support applications for new development which generate traffic movements.
- 2.1.3 The NPPF states under the subheading 'Considering Development Proposals; that:

'In assessing sites that may be allocated for development in plans, or specific applications for development, it should be ensured that:

- a) appropriate opportunities to promote sustainable transport modes can be – or have been – taken up, given the type of development and its location;*
- b) safe and suitable access to the site can be achieved for all users;*
- c) the design of streets, parking areas, other transport elements and the content of associated standards reflects current national guidance, including the National Design Guide and the National Model Design Code; and*
- c) any significant impacts from the development on the transport network (in terms of capacity and congestion), or on highway safety, can be cost effectively mitigated to an acceptable degree (Paragraph 110).*

Development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.

Within this context, applications for development should:

- a) give priority first to pedestrian and cycle movements, both within the scheme and with neighbouring areas; and second – so far as possible – to facilitating access to high quality public transport, with layouts that maximise the catchment area for bus or other public transport services, and appropriate facilities that encourage public transport use;*
- b) address the needs of people with disabilities and reduced mobility in relation to all modes of transport;*

c) create places that are safe, secure and attractive – which minimise the scope for conflicts between pedestrians, cyclists and vehicles, avoid unnecessary street clutter, and respond to local character and design standards;

d) allow for the efficient delivery of goods, and access by service and emergency vehicles; and

e) be designed to enable charging of plug-in and other ultra-low emission vehicles in safe, accessible and convenient locations.

All developments that will generate significant amounts of movement should be required to provide a travel plan, and the application should be supported by a transport statement or transport assessment so that the likely impacts of the proposal can be assessed.'

- 2.1.4 The Government's objectives set out in the revised NPPF are to ensure that new developments are provided in sustainable locations, where the need to travel is minimised and the use of sustainable modes can be maximised.
- 2.1.5 Paragraph 115 of the NPPF states that '*development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe*'.

2.2 Regional Policy

The London Plan (March 2021)

- 2.2.1 The London Plan is a spatial development strategy which sets out the framework for the development of London over the next 25 years. The policies set out in the London Plan which are pertinent to the proposed site are detailed below.
- 2.2.2 Policy T1 sets out a number of strategic aims, which include delivering the Mayor's strategic targets of 80% of all trips in London to be made by foot, cycle or public transport by 2041.
- 2.2.3 Policy T4 provides guidance on assessing and mitigating the transport impacts of new development:

'When required in accordance with national or local guidance, transport assessments/statements should be submitted with development proposals to ensure that impacts on the capacity of the transport network (including impacts on pedestrians and the cycle network), at the local, network-wide and strategic level, are fully assessed. Transport assessments should focus on embedding the Healthy Streets Approach within, and in the vicinity of, new development. Travel Plans, Parking Design and Management Plans, Construction Logistics Plans and Delivery and Servicing Plans will be required having regard to Transport for London guidance.'

2.2.4 Policy T5 considers cycling and notes that new development should help remove barriers to cycling. Developments should provide cycle parking at least in accordance with the minimum standards set out in Table 10.2 and Figure 10.3 of the London Plan. The standards set out for residential development are as follows:

- Long-stay (e.g. for residents):
 - 1 space per studio or 1 person, 1 bedroom dwelling;
 - 1.5 spaces per 2 person, 1 bedroom dwelling; and
 - 2 spaces per all other dwellings.
- Short-stay (e.g. for visitors):
 - 5-40 dwellings – 2 spaces; and
 - Thereafter – 1 space per 40 dwellings.

2.2.5 Policy T6 considers car parking and states that this should be restricted in line with levels of existing and future public transport accessibility and connectivity. Car-free development should be the starting point for all development proposals in places that are well-connected by public transport.

Mayor's Transport Strategy (2018)

2.2.6 The Mayor's Transport Strategy was published in March 2018 and sets out a range of policies and proposals aimed at creating Healthy Streets and healthy people. The document sets a target of 80% of all trips in London to be made on foot, by cycle or using public transport by 2041.

2.3 Local Policy

Camden Local Plan (2017)

2.3.1 The Camden Local Plan was adopted in July 2017 and acts as planning guidance for all sites in the borough.

2.3.2 Policy T1 'Prioritising walking, cycling and public transport' describes the measures put in place by LBC, and the requirements of developers, to promote sustainable transport in the Borough:

'Walking

In order to promote walking in the borough and improve the pedestrian environment, we will seek to ensure that developments:

a. improve the pedestrian environment by supporting high quality public realm improvement works;

b. make improvements to the pedestrian environment including the provision of high quality safe road crossings where needed, seating, signage and landscaping;

c. are easy and safe to walk through ('permeable');

d. are adequately lit;

e. provide high quality footpaths and pavements that are wide enough for the number of people expected to use them. Features should also be included to assist vulnerable road users where appropriate; and

f. contribute towards bridges and water crossings where appropriate.

Cycling

In order to promote cycling in the borough and ensure a safe and accessible environment for cyclists, the Council will seek to ensure that development:

g. provides for and makes contributions towards connected, high quality, convenient and safe cycle routes, in line or exceeding London Cycle Design Standards, including the implementation of the Central London Grid, Quietways Network, Cycle Super Highways and;

h. provides for accessible, secure cycle parking facilities exceeding minimum standards outlined within the London Plan (Table 6.3) and design requirements outlined within our supplementary planning document Camden Planning Guidance on transport. Higher levels of provision may also be required in areas well served by cycle route infrastructure, taking into account the size and location of the development;

i. makes provision for high quality facilities that promote cycle usage including changing rooms, showers, dryers and lockers;

j. is easy and safe to cycle through ('permeable'); and

k. contribute towards bridges and water crossings suitable for cycle use where appropriate.

Public Transport

In order to safeguard and promote the provision of public transport in the borough we will seek to ensure that development contributes towards improvements to bus network infrastructure including access to bus stops, shelters, passenger seating, waiting areas, signage and timetable information. Contributions will be sought where the demand for bus services generated by the development is likely to exceed existing capacity. Contributions may also be sought towards the improvement of other forms of public transport in major developments where appropriate.

Where appropriate, development will also be required to provide for interchanging between different modes of transport including facilities to make interchange easy and convenient for all users and maintain passenger comfort.'

2.3.3 Policy T2 considers parking and where LBC will permit parking at new developments. The Council will not issue on-street or on-site parking permits in connection with new developments and will limit on-site parking to spaces designated for disabled people where necessary, and/or essential operational or servicing needs.

Camden Planning Guidance: Transport (2021)

- 2.3.4 Camden's Planning Guidance dictates the Borough's requirements for transport related planning concepts such as Travel Plans, parking and servicing.
- 2.3.5 The Guidance notes that the Council will expect all new residential development to be car-free, including redevelopments, regardless of PTAL ratings.
- 2.3.6 LBC expect new development to provide, as a minimum, the number of cycle parking spaces as set out in the London Plan. The Council will also seek an additional 20% of spaces over and above the London Plan standard to support the expected future growth of cycling in the Borough.
- 2.3.7 The Council state that cycle parking should be provided off-street and close to the site entrance. Cycle parking needs to be accessible and secure. The route to cycle parking from street level must be step free.

3 Existing Conditions

3.1 Site Location

- 3.1.1 The site is located approximately 600m south of Hampstead. The site is bound to the north by existing residential development, to the east by Fitzjohn's Avenue, to the south by Nutley Terrace and to the west by Maresfield Gardens. The site location is indicated at **Figure 1**.

3.2 Planning History

- 3.2.1 Planning permission has previously been granted for the extension and refurbishment of 39A Fitzjohn's Avenue, subject to a S106 agreement, under planning references 2020/2172/P; 2018/2415/P; and 2017/4654/P. Planning permission has also been granted for the development of 35 apartments at 39 Fitzjohn's Avenue under reference 2020/2169/P.

3.3 Details of the Local Highway Network

- 3.3.1 The site is currently accessed from Fitzjohn's Avenue and Maresfield Gardens. Maresfield Gardens is a single carriageway, two-way residential road which is subject to a 20mph speed limit. Footways and street lighting are provided on both sides of the carriageway.
- 3.3.2 The road routes in a general north / south alignment and connects with Fitzjohn's Avenue to the south and Netherhall Gardens to the north. Immediately south of the site, Maresfield Gardens connects with Nutley Terrace at a four-arm priority crossroads, where a raised table is present.
- 3.3.3 Fitzjohn's Avenue is a single-carriageway two-way road which is subject to a 20mph speed limit, is street lit and has footways present along both sides of the carriageway. The road routes in a general north / south alignment, providing access to the A502 Hampstead High Street and Holly Hill at a four-arm signalised crossroads to the north. To the south it connects with College Crescent, which in turn connects with the A41 Finchley Road at a signalised gyratory.
- 3.3.4 The surrounding road network sits within a controlled parking zone (CPZ), which restricts parking between Monday to Friday 09:00-18:30, and Saturday 09:30-13:30 to permit holders only. There are two car club bays immediately adjacent to the site on Nutley Terrace.
- 3.3.5 The A41 Finchley Road is a dual carriageway with an approximate north / south alignment, to the north connecting to the Brent Cross Interchange where the A406 North Circular Road can be accessed. The A406 routes around the northern extents of London, connecting Chiswick in the west with Woolwich in the east.
- 3.3.6 The site is therefore considered to be in a suitable location for access to the local highway network.

3.4 Road Safety

3.4.1 A review of the most recent five-year period of personal injury collision (PIC) data on the local road network has been undertaken using the CrashMap database. The study area includes Maresfield Gardens, Nutley Terrace and Fitzjohn's Avenue between its junctions with Maresfield Gardens and Netherhall Gardens.

3.4.2 Across the study area, 18 collisions were recorded, of which one was classified as 'serious' in severity and the rest as 'slight'. Each of the incidents is summarised below.

Maresfield Gardens

3.4.3 A single collision was recorded on Maresfield Gardens. The collision was classified as 'slight' in severity and involved two cars.

Fitzjohn's Avenue / Netherhall Gardens

3.4.4 Two 'slight' incidents were recorded adjacent to this junction, both of which involved a collision between a car and a motorcycle.

Fitzjohn's Avenue / Lyndhurst Road

3.4.5 Four incidents were recorded adjacent to this junction, all of which were classified as 'slight' in severity. Each of these collisions involved a cyclist and a motorised vehicle.

Fitzjohn's Avenue between St Mary's School and 39A

3.4.6 Three incidents were recorded along this stretch of road, all classified as 'slight' in severity. One incident involved a collision between a pedestrian and a cyclist; one involved a collision between a goods vehicle and a cyclist; and the final incident was a collision between two cars.

Fitzjohn's Avenue / Nutley Terrace

3.4.7 Four collisions were recorded adjacent to this junction, of which one was classified as 'severe', and three as 'slight' in severity.

3.4.8 The 'severe' incident involved a collision between a cyclist and a car. A single incident involved a car and a motorcycle, and the remaining incidents each involved two cars.

Nutley Terrace

3.4.9 A single collision was recorded on Nutley Terrace, which was classified as 'slight' in severity and involved a car and a pedestrian.

Fitzjohn's Avenue between Nutley Terrace and Maresfield Gardens

- 3.4.10 Three incidents were recorded along this length of road. The first was classified as 'serious' and involved four cars and a goods vehicle. The second was classified as 'slight' and involved three cars and the third involved a car and a motorcycle and resulted in 'slight' injuries.
- 3.4.11 It is not considered that there is a pattern to the existing road safety record that would be exacerbated by the proposed development, particularly given the minimal change in motorised vehicle movements anticipated. As such, it is not considered that the proposals will negatively impact the existing highway safety record.

Summary

- 3.4.12 It is not considered that there is a pattern to the above collision record that would be exacerbated by the development proposals.

4 Access by Sustainable Modes

4.1 Walking

- 4.1.1 Lit footways are present along both sides Fitzjohn's Avenue, Maresfield Gardens and Nutley Terrace. Dropped kerbs are provided over the existing site access on Fitzjohn's Avenue and dropped kerbs with tactile paving are provided over most local junctions.
- 4.1.2 Immediately adjacent to the site access on Fitzjohn's Avenue a zebra crossing facility is provided. A further two zebra crossings are located approximately 230m north and 380m south of the site on Fitzjohn's Avenue, providing safe routes to local schools.
- 4.1.3 Whilst superseded by the NPPF, the transport policies in the former PPG13 set out specific guidance related to walking: *"Walking is the most important mode of travel at the local level and offers the greatest potential to replace short car trips, particularly under 2 kilometres"* (Para 74).
- 4.1.4 A plan showing key pedestrian infrastructure is included in **Figure 2** and a plan showing a 2km walking catchment from the centre of the site is included at **Figure 3**. This demonstrates that a significant area can be accessed within an appropriate walk distance. This area includes 10 Underground stations, and a range of facilities including a number of schools, medical facilities, supermarkets, retail stores and leisure facilities.
- 4.1.5 In summary, there is a good level of pedestrian infrastructure to facilitate trips from the site on foot. The site is therefore well located to encourage pedestrian journeys in place of journeys undertaken by private car.

4.2 Cycling

- 4.2.1 Whilst superseded by the NPPF, the transport policies in the former PPG13 set out specific guidance related to cycling: *"Cycling also has the potential to substitute for short car trips, particularly those under 5 kilometres, and to form part of a longer journey by public transport"* (Para 77).
- 4.2.2 The plan attached at **Figure 4** shows a 5km cycling catchment from the site. The plan identifies that much of northwest London can be accessed within an appropriate cycle distance, including areas such as Hampstead Village, Camden, Archway, Hampstead, Cricklewood and Marylebone.
- 4.2.3 The local cycle network is shown in **Figure 5**. The southern sections of Maresfield Gardens and Fitzjohn's Avenue form part of the London Cycle Network, connecting the site with the wider network of cycle routes throughout Greater London.

4.3 Public Transport

- 4.3.1 Within a 400m catchment of the site are six bus stops, all of which are situated along Fitzjohn's Avenue. These stops are generally facilitated with a sheltered seating area, as well as a bus stop flag and timetabling information.
- 4.3.2 Additional services can be accessed from Finchley Road, which is an approximate 550m walk distance from the site.
- 4.3.3 A summary of the routes available from these stops, and the service frequencies, is provided below in **Table 1**.

Table 1 Local Bus Routes and Frequencies

No.	Route	Weekday	Saturday	Sunday
46	Paddington Station – St. Bartholomews Hospital	Every 9-12 Minutes	Every 10-11 Minutes	Every 15 Minutes
603	Princes Avenue – Swiss Cottage	School Service	-	-
13	North Finchley – Victoria Station	Every 6-10 Minutes	Every 8-11 Minutes	Every 7-11 Minutes
113	Edgware – Marble Arch	Every 7-10 Minutes	Every 8-12 Minutes	Every 10-13 Minutes
187	Central Middlesex Hospital – O2 Centre	Every 15 Minutes	Every 15 Minutes	Every 20 Minutes
268	Golders Green – O2 Centre	Every 14 Minutes	Every 15 Minutes	Every 20 Minutes
N113*	Edgware – Trafalgar Square	Every 30 Minutes	Every 30 Minutes	Every 30 Minutes

*Night Service

- 4.3.4 The site is also located in close proximity to a range of rail services.
- 4.3.5 Finchley Road Underground Station is located an approximate 650m walk distance (8 minutes) to the west of the site. From here, the Jubilee and Metropolitan lines can be accessed, providing regular services to destinations including Stratford, Stanmore, Amersham, Watford, Chesham, Uxbridge, Wembley Park, Aldgate and Baker Street.
- 4.3.6 Finchley Road and Frognal Railway Station can be accessed via a 750m / 10-minute walk. This station provides access to the London Overground which provides services to a range of destinations including Gospel Oak, Barking Riverside, Highbury, New Cross, Clapham Junction, Crystal Palace, West Croydon, Liverpool Street, Enfield Town, Cheshunt, Chingford, Richmond, Stratford, Romford, Upminster, Watford Junction and Euston.
- 4.3.7 West Hampstead Thameslink Railway Station can be accessed within an 18-minute walk or 7-minute cycle. The station provides access to Thameslink services to destinations including St Albans, Brighton, Sutton and Rainham.

4.3.8 Belsize Park Underground Station can be accessed via a 15-minute walk or 5-minute cycle. This station provides access to the Northern Line and regular services to destinations including Edgware, Morden, Kennington, Mill Hill, High Barnet and Battersea Power Station.

4.3.9 The site is therefore well located for travel by public transport.

4.4 Public Transport Accessibility

4.4.1 Public Transport Accessibility Levels (PTALs) are theoretical measures of the accessibility of a given point to the public transport network, taking into account walk time and service availability. PTALs are categorised in 9 levels, from 1a (low accessibility) to 6b (high accessibility).

4.4.2 The proposed development site incorporates two PTALs – 5 and 6a, which demonstrates that the site is well located for access to public transport nodes.

4.5 Summary

4.5.1 It is considered that the site is located within a highly accessible location, with good active travel and public transport infrastructure. This is reflected in the site's PTAL rating of 5 / 6a. It is therefore demonstrated that future residents will not be reliant on the private car for everyday journeys.

5 Proposed Development

5.1 Development Proposals

- 5.1.1 The proposals are for the redevelopment of the site to provide two town houses and two maisonettes fronting onto Fitzjohn's Avenue, and 29 x 1-3 bed flats on land adjacent to Maresfield Gardens.
- 5.1.2 The proposed site layout is included at **Appendix A**.

5.2 Access and Servicing Arrangements

- 5.2.1 No changes are proposed to the existing access arrangements onto Fitzjohn's Avenue. This access will be retained to allow access to a small parking and servicing area.
- 5.2.2 A new access is proposed onto Maresfield Gardens, in the form of a dropped kerb crossover. This will facilitate access for delivery vehicles only.
- 5.2.3 For the townhouses and maisonettes, refuse will be collected from Fitzjohn's Avenue, as per the existing situation. A refuse store is located to the north of the site. Residents will be required to wheel their bins to the front of the property on the appropriate days, as per the existing arrangement.
- 5.2.4 For the apartments located on land adjacent to Maresfield Gardens, a dedicated waste and recycling store is included to the west of the site. Bins will be collected from Maresfield Gardens and a facilities manager will ensure bins are located in an appropriate location for collection.
- 5.2.5 In terms of deliveries, a small (4.6T) delivery van can access the townhouses from Fitzjohn's Avenue. The access onto Maresfield Gardens will provide access to the apartments for delivery vehicles.
- 5.2.6 Vehicle swept path analysis of the site has been undertaken to demonstrate that the necessary vehicles can adequately service the site. These drawings are included at **Appendix B**.

5.3 Cycle Parking

- 5.3.1 For the units fronting Fitzjohn's Avenue, long-stay cycle parking is proposed at the garden level in a secure store with capacity for eight cycles. The number of units proposed here does not meet the threshold for providing short-stay visitor parking.
- 5.3.2 For the apartments, long-stay cycle parking is proposed a lower ground floor level, in the form of two-tier cycle stores. A total of 60 spaces are proposed, which meets the requirements of the London Plan and Camden's Planning Guidance.

- 5.3.3 Short-stay visitor cycle parking is proposed in the form of 2 Sheffield stands close to the building entrance, with capacity to accommodate 4 cycles. This level of short stay parking is greater than that required by the London Plan.

5.4 Car Parking

- 5.4.1 The site at Fitzjohn's Avenue currently accommodates an area of hardstanding to the front of the property, providing space to accommodate approximately 12 cars. It is proposed to partially retain the existing area of hardstanding, but to provide a reduced level of parking.
- 5.4.2 The proposals include the retention of three car parking spaces to serve the units fronting Fitzjohn's Avenue, and no parking to serve the apartments on Maresfield Gardens.
- 5.4.3 Whilst LBC's policy expects all new residential development to be car free, it is considered appropriate to retain three spaces in this instance given the overall reduction in off-street parking at the site. Electric vehicle charging points are proposed to serve the three on-site parking spaces. Therefore, the vehicles using these spaces can be expected to be sustainable and low-emission.
- 5.4.4 Future residents will not be able to apply for an on-street permit, as per LBC's policy requirements.

5.5 Car Park Management and Reduction Plan

- 5.5.1 The proposed layout of the parking area can be seen in the proposed layout included at **Appendix A**. Vehicle swept path analysis has been undertaken to ensure these spaces are fully accessible, with drawings included at **Appendix B**.
- 5.5.2 Access to the parking area will be taken from the existing access to the site from Fitzjohn's Avenue. This access is shared with pedestrians and cyclists, although is not the only access for pedestrians and cyclists onto Fitzjohn's Avenue.
- 5.5.3 The limited level of on-site parking is expected to minimise car use by residents, however, a range of measures to further encourage travel by sustainable modes are set out in the Travel Plan.
- 5.5.4 As noted, all three spaces will be provided with electric vehicle charging points.
- 5.5.5 Three of the apartments will be allocated a single parking space each, including the two townhouses.

6 Trip Generation and Development Impact

6.1 Introduction

- 6.1.1 This section sets out the impact of the development proposals on the local highway network. The assessment focuses on the weekday morning and evening network peak hours, which constitutes the key trip generation periods for residential development.
- 6.1.2 Whilst the existing site is occupied by a single dwelling, for robustness, no consideration will be given to this within the development impact assessment.

6.2 Trip Generation

- 6.2.1 The trip generation of the proposed development has been considered based on data from the TRICS trip rate database. Sites under the categories '03 Residential; C Flats Privately Owned' and '03 Residential; A Houses Privately Owned' in Greater London have been considered. Only those sites in Edge of Town Centre, Suburban and Edge of Town locations have been included within the assessment. For the flats, only those sites comprising 100 units or less have been included, and any surveys undertaken during the Covid 19 pandemic have been excluded.
- 6.2.2 The full TRICS outputs are included at **Appendix C**, and the weekday peak hour person trips are summarised in **Table 2**.

Table 2 – Proposed Weekday Peak Hour Person Trip Generation

Unit Type	Mode of Travel	AM Peak		PM Peak		Daily Movements	
		Arr	Dep	Arr	Dep	Arr	Dep
Private Houses	Person Trip Rates	0.32	0.98	0.8	0.41	6.15	5.95
	Person Trips	1	2	2	1	12	12
Private Flats	Person Trip Rates	0.139	0.708	0.485	0.295	3.771	3.759
	Person Trips	4	22	15	9	117	117
Total	Person Trips	5	24	17	10	129	128

- 6.2.3 The proposed development is expected to generate 29 person trips in the morning peak hour and 27 person trips in the evening peak hour. Across a typical weekday, the site would be expected to generate 258 person movements.
- 6.2.4 To provide an indication of the trip generation of the proposed development by mode of travel, typical travel modes of the existing residential population have been established using Census Method of Travel to Work data for the middle super output area (MSOA) Camden 008, in which the site is located.
- 6.2.5 2021 Census data was obtained during the Covid 19 pandemic and therefore there is the potential that the data is not representative of current travel patterns. However, 2011 Census

data is likely to paint an outdated picture. As such, an average of the two datasets has been used to calculate expected trips by mode for the proposed development.

- 6.2.6 The average Census modal split of travel is summarised in **Table 3** below, with trips by mode calculated based on the person trips identified in **Table 2**.

Table 3 Proposed Trips by Mode

Mode	%	AM Peak		PM Peak		Daily	
		Arr	Dep	Arr	Dep	Arr	Dep
Total Person Trips	100%	5	24	17	10	129	128
Work mainly at or from home	36%	-	-	-	-	-	-
Underground, metro, light rail, tram	22%	1	5	4	2	29	29
Train	2%	0	0	0	0	3	3
Bus, minibus or coach	4%	0	1	1	0	5	5
Taxi	1%	0	0	0	0	1	1
Motorcycle, scooter or moped	1%	0	0	0	0	1	1
Driving a car or van	6%	0	1	1	1	8	8
Passenger in a car or van	0%	0	0	0	0	1	1
Bicycle	3%	0	1	0	0	3	3
On foot	11%	1	3	2	1	14	14
Other method of travel to work	1%	0	0	0	0	1	1

- 6.2.7 The above data demonstrates that very few trips are expected to occur by car, with up to 2 vehicle movements in each peak hour, and 16 two-way trips across the day. As would be expected, given the sustainable location of the site, the majority of trips to and from the site will take place via the Underground or on foot.
- 6.2.8 It is not considered that this level of vehicle movements would have a notable impact on the operation of the local highway network. Furthermore, the level of vehicle use at the site will be minimised through the implementation of a Travel Plan.
- 6.2.9 In terms of the impact on other modes of travel, this is expected to be minimal. Across the day, the proposals are expected to generate 58 additional trips via the Underground (just over four additional trips per hour on average), 10 trips by bus, 6 trips by cycle and 28 trips on foot. It is considered that the existing public transport and active travel infrastructure is of a sufficient standard to accommodate this modest uplift in use.

7 Framework Delivery and Servicing Management Plan

7.1 Introduction

- 7.1.1 The implementation of measures set out within this Framework Delivery and Servicing Management Plan (FDSMP) will aim to proactively manage deliveries to minimise the number of associated trips at the site. The Plan will identify and promote areas where safe and legal loading can take place. The overall aim of the FDSMP is to reduce the impact of servicing on the local highway network and the amenity and safety of the general public.

7.2 Proposed Delivery and Servicing Arrangements

- 7.2.1 The development proposals comprise the development of 29 apartments to be accessed from Maresfield Gardens, and two townhouses and two maisonettes on Fitzjohn's Avenue. The scheme proposes to work in harmony with the adjacent development at 39 Fitzjohn's Avenue, which also proposes residential development.
- 7.2.2 Future servicing and delivery requirements at the site will include refuse / recycling collection and deliveries associated with residents.

7.3 Refuse Collection

- 7.3.1 Refuse collection for all elements of the scheme will take place from the highway, as per the existing situation for all neighbouring residential development.
- 7.3.2 For the units fronting Fitzjohn's Avenue, refuse will be stored to the north of the site in standard refuse bins. Residents will be required to drag these to the edge of the carriageway on collection day.
- 7.3.3 The apartments fronting Maresfield Gardens will be provided with a dedicated waste and recycling store and internal refuse collection will be managed by a facility manager. An integral bin store will be located adjacent to the concierge for residents to deposit their waste and recycling. The concierge / building management will then exchange full bins with empty ones from the refuse store.
- 7.3.4 Bins will be collected from Maresfield Gardens. The refuse store is located 17m from the highway. Whilst this is marginally further than required by LBC (10m) it is not considered a significant departure from standards. If required, the management company will drag the bins closer to the site entrance on refuse collection days to accord with the maximum drag distance.
- 7.3.5 Refuse vehicles service the local area at the current time and therefore it is considered that the local road network is suitable for these types of vehicles, and the modest uplift in collection requirements proposed.

7.4 Deliveries

- 7.4.1 The parking area serving the townhouses fronting Fitzjohn's Avenue is suitable to accommodate a small (4.6T) delivery vehicle. Larger deliveries can be accommodated on Nutley Terrace, as occurs in the existing situation, without preventing the safe operation of highways infrastructure.
- 7.4.2 It is not considered that the number of deliveries associated with the four dwellings fronting Fitzjohn's Avenue will be notably higher than in the current situation and therefore no negative cumulative impacts are expected.
- 7.4.3 For the apartments served from Maresfield Gardens, a service access is proposed, which can accommodate service vehicles up to 7.5T.
- 7.4.4 Residents will be required to let delivery vehicles know of any access restrictions when ordering their goods.
- 7.4.5 Vehicle swept path analysis demonstrating that the necessary vehicles can adequately manoeuvre within the site are included at **Appendix B**.
- 7.4.6 With regards to delivery timing, it is expected that this will mirror the existing situation on local roads. Deliveries to the site will only be associated with residents and as such will be undertaken by commercial operators. It is therefore highly unlikely that deliveries will be undertaken during antisocial hours (i.e. pre-07:00 and post-20:30).

8 Summary and Conclusions

- 8.1.1 Syntegra has been appointed to produce this Transport Statement (TS) in support of proposals to redevelop 39A Fitzjohn's Avenue to provide two townhouses and two maisonettes and develop 29 apartments on land adjacent to Maresfield Gardens.
- 8.1.2 Planning permission has previously been granted for the extension and refurbishment of 39A Fitzjohn's Avenue, subject to a S106 agreement, under planning references 2020/2172/P; 2018/2415/P; and 2017/4654/P. Planning permission has also been granted for the development of 35 apartments at 39 Fitzjohn's Avenue under reference 2020/2169/P.
- 8.1.3 The site is currently accessed from Fitzjohn's Avenue and Maresfield Gardens. Maresfield Gardens is a single carriageway, two-way residential road which is subject to a 20mph speed limit.
- 8.1.4 The site is located in a highly sustainable location with ready access to a high level of local services and amenities as well as public transport nodes. Lit footways are provided on the local highway network, and a number of pedestrian crossings are provided within proximity to the site. A number of London Cycle Networks operate within the vicinity of the site, connecting the local area with the wider network of cycle routes through Greater London.
- 8.1.5 Within a short walk of the site, high frequency bus services can be accessed providing connectivity to key destinations. In addition Finchley Road Underground Station, Finchley Road and Frognal Railway Station, West Hampstead Thameslink Railway Station and Belsize Park Underground Station can all be accessed within a reasonable walk or cycle from the site. The site is within a PTAL zone of 5 / 6a and is therefore considered to be well located for access to public transport nodes.
- 8.1.6 It is therefore considered that residents will not be reliant on the private car to undertake everyday journeys.
- 8.1.7 Cycle parking is provided in line with the local and London Plan standards, with 68 long-stay and 4 short-stay spaces provided across the site. All long-stay cycle parking is provided in secure, covered facilities.
- 8.1.8 The apartments will not be provided with any on-site parking, which is policy compliant and considered appropriate given the sustainable nature of the site. Three parking spaces are proposed to serve the units fronting Fitzjohn Avenue, which will all be provided with electric charging facilities. **Allocation.**
- 8.1.9 A Framework Delivery and Servicing Management Plan has been developed to support the proposals, which demonstrates that refuse collection and deliveries can be undertaken appropriately and safely.

- 8.1.10 Very few trips are expected to occur by car, with up to 2 vehicle movements in each peak hour, and 16 two-way trips across the day. As would be expected, given the sustainable location of the site, the majority of trips to and from the site will take place via the Underground or on foot.
- 8.1.11 In terms of the impact on other modes of travel, this is expected to be minimal. Across the day, the proposals are expected to generate 58 additional trips via the Underground (just over four additional trips per hour on average), 10 trips by bus, 6 trips by cycle and 28 trips on foot. It is considered that the existing public transport and active travel infrastructure is of a sufficient standard to accommodate this modest uplift in use.
- 8.1.12 It is therefore considered that there are no highways reasons why the proposals should not be granted permission.

FIGURES

mail@syntegragroup.com
Tel: 0330 053 6774

LONDON | READING | TONBRIDGE | BRISTOL

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VAT Registration No. 980016044
BSI Certificate Number F5 710041



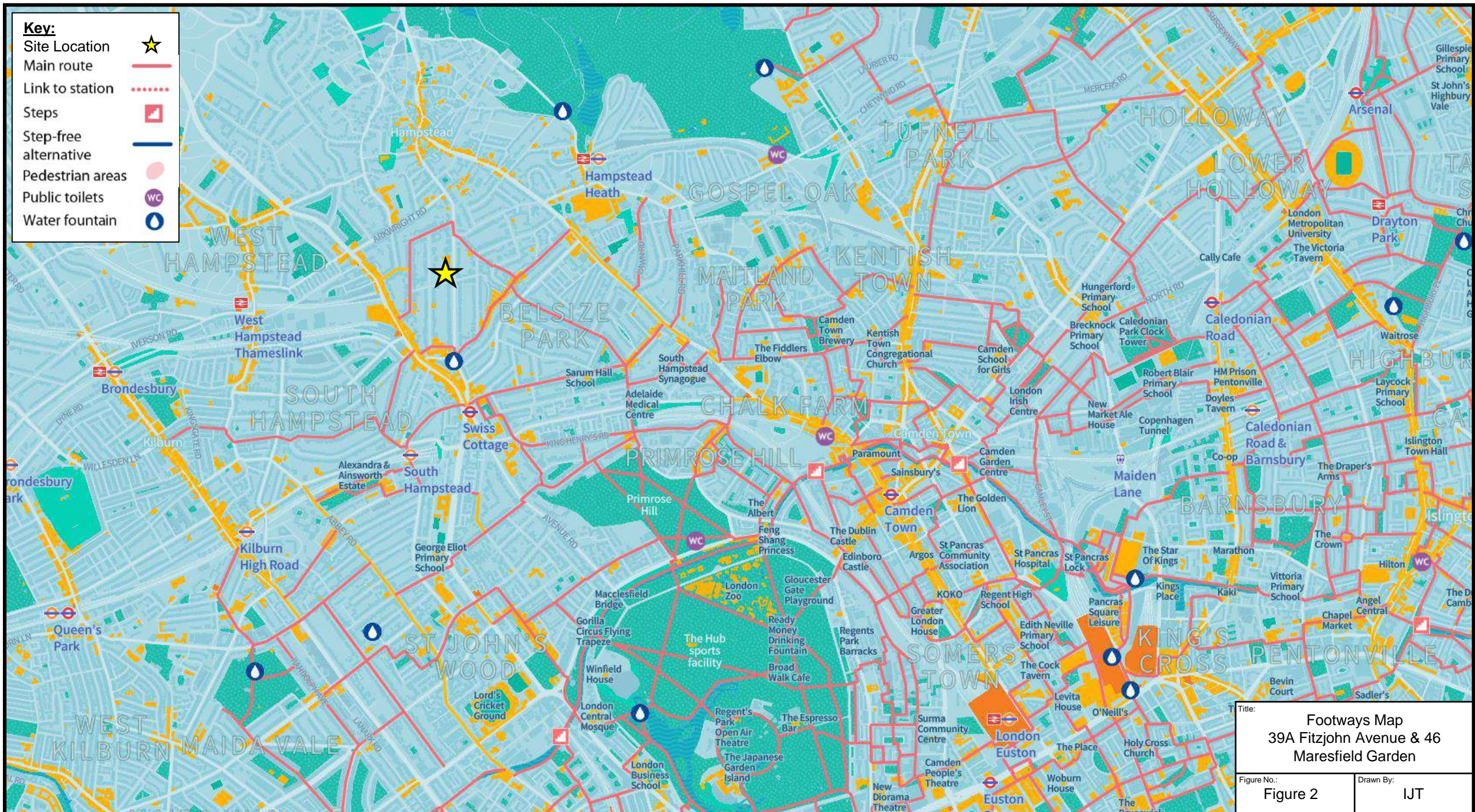
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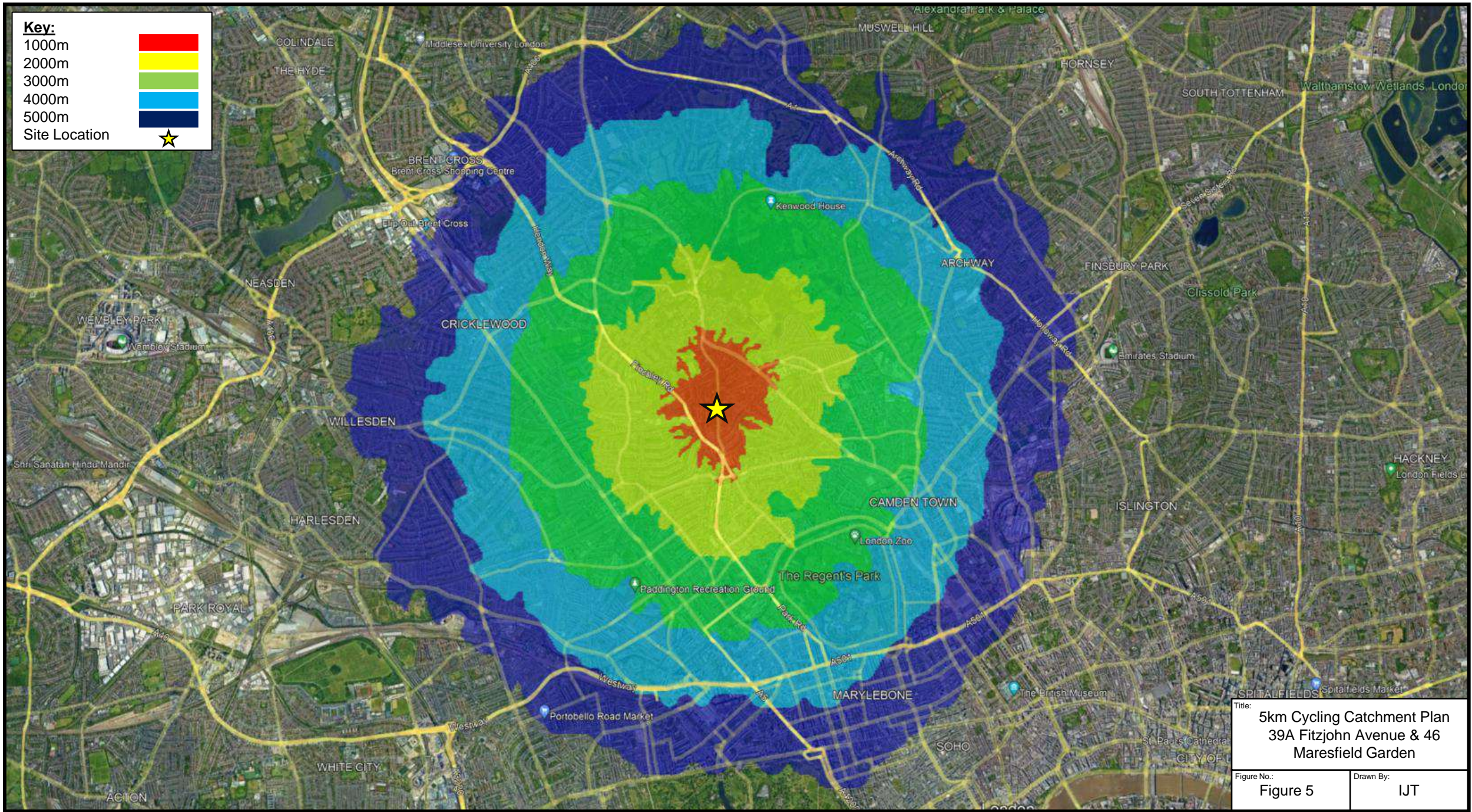


Title:
Indicative Site Location Plan
39A Fitzjohn Avenue & 46
Maresfield Garden

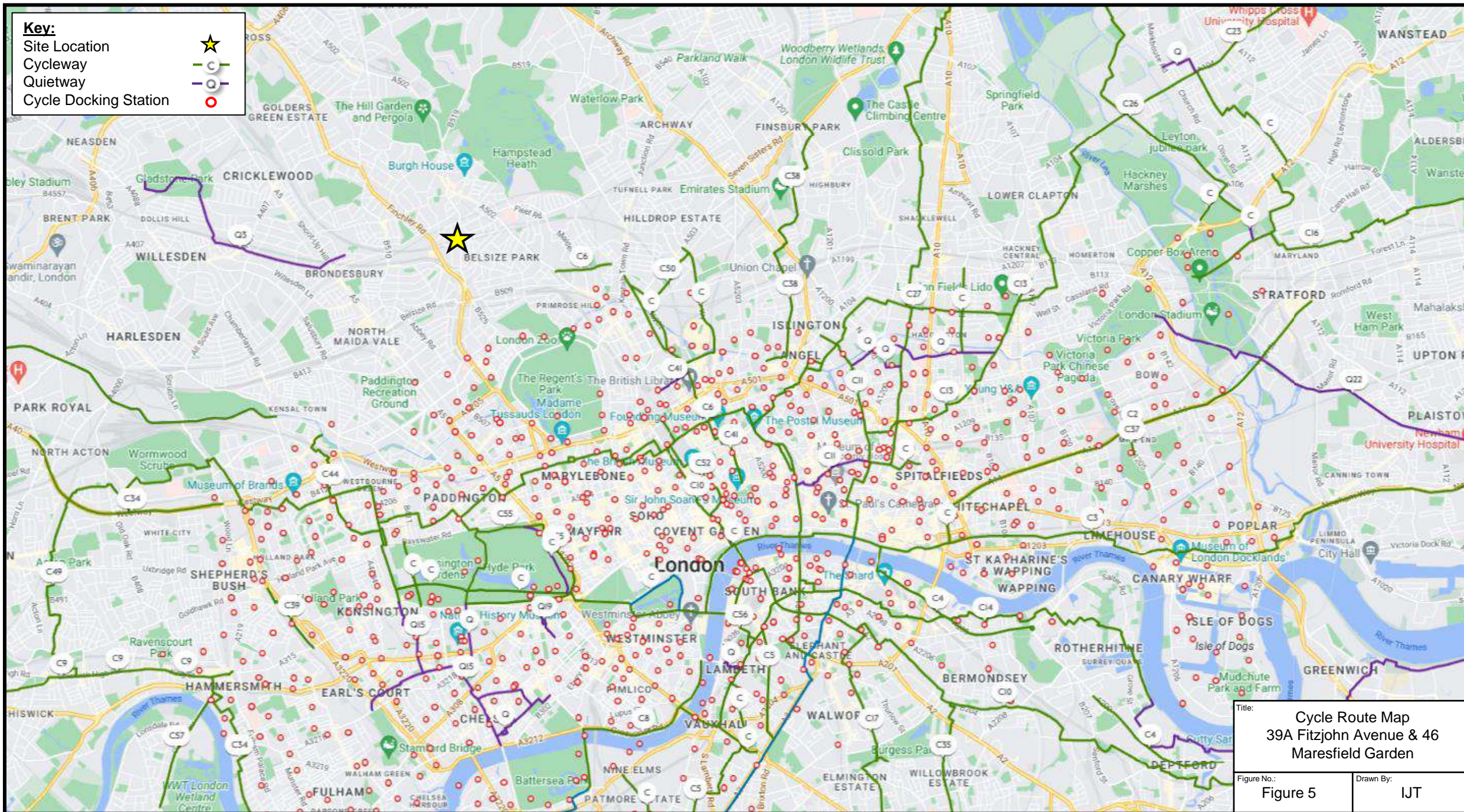
Figure No.:
Figure 1

Drawn By:
IJT



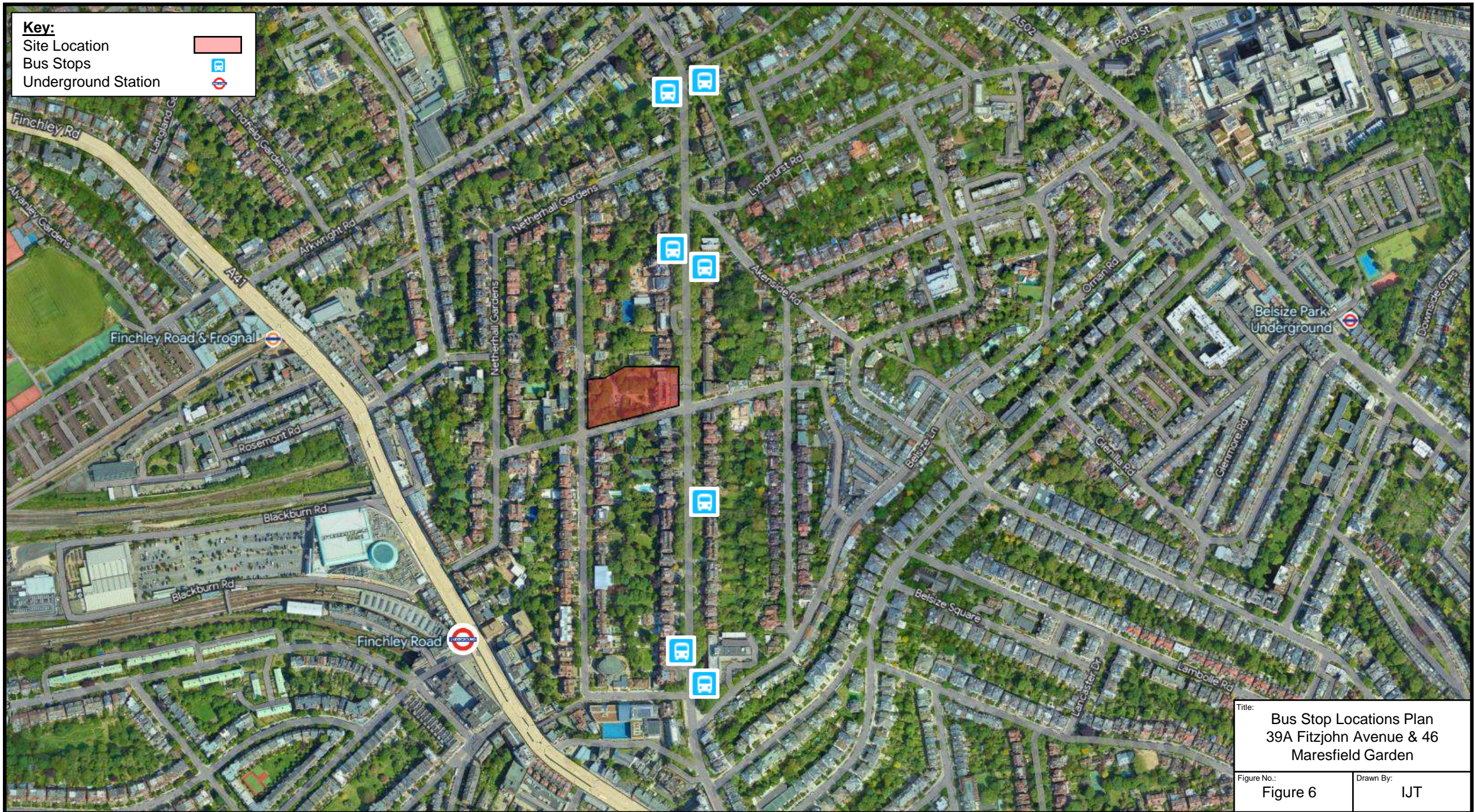


Key:
Site Location
Cycleway
Quietway
Cycle Docking Station



Title: Cycle Route Map
39A Fitzjohn Avenue & 46
Maresfield Garden

Key:
Site Location
Bus Stops
Underground Station



Title:
Bus Stop Locations Plan
39A Fitzjohn Avenue & 46
Maresfield Garden

Figure No.:
Figure 6

Drawn By:
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Appendices

mail@syntegragroup.com
Tel: 0330 053 6774

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Appendix A

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notes
1. Read in conjunction with all relevant Architects, Engineers & design team drawings & specifications
2. Any discrepancies must be drawn to the attention of the lead consultant in writing
3. All dimensions are in millimeters unless otherwise noted & are subject to checking on site prior to fabrication or ordering of materials
4. This drawing is the copyright of Bowles & Wyer (and other consultants where referenced)

KEY

 **Stone paving**
colour: buff, pattern: stretcher bond,
size: 600x900mmx40mm thick, finish:sawn

 **Porcelain paving**
colour:buff, pattern: stretcher bond,
size: 600x900mmx20mm thick

 **Clay pavers**
colour:buff, pattern: stretcher bond,
size: 217mmx52mmx70mm (size subject to
product specification)


 **Footpath gravel**
self-binding aggregate, colour:buff

 **Loose gravel**
colour:buff, size:10-14mm


 **Concrete paving & steps**
colour:buff, pattern: stretcher bond,
size: 350 X 150 X 1000 (size subject to
product specification)

 **Existing trees (retained)**


 **Proposed specimen trees**

 **Guardrails**
1.1m high, galvanised powder coated barrier,
RAL tbc

 **Retaining Wall**
Brick retaining wall


 **Amenity planting**
mix of shrubs, herbaceous and grasses for all
year round interest

 **Wildflower planting**
grass and wildflower species mix that provide
a natural meadow look

 **Proposed hedges**
native hedges along the boundary and amenity
hedges for private gardens

 **Species-rich lawn**

 **Raised planters**
steel planters, rolled top edge, sizes vary
depending on location

 **Pond**
pond with shallow edges 100-200mm with
depth increasing to max 400mm towards the
centre. Allowance for pump and filter for water
circulation and aeration.

+73.815 Proposed levels

(+73.815) Existing levels

PL04	22.01.24	WT	AP	Pond included. Frontage to 39A reviewed
PL03	12.12.23	WT	AP	Play design reviewed
PL02	07.12.23	WT	AP	New fire escape added. Access road updated
PL01	17.11.23	WT	AP	For planning application
B	02.10.23	WT	JW	Masterplan updated for Pre app
A	21.09.23	AP	--	Masterplan updated for landscape workshop

Orig	23.06.23	MC	AP	Stage 2 concept design
rev	date	by	chk	notes

BOWLES & WYER

5 Williams Court, Tunnel Way, Pitstone, Nr. Leighton Buzzard, LU7 9GJ
Tel: 01296 662439 Fax: 01296 663959 Email: admin@bowleswyer.co.uk

project title

**Land adjacent to 46 Maresfield Gardens
and 39a Fitzjohn's Avenue**

drawing title

MASTERPLAN

job number

3031

drawing number

3031-XX-DR-L-101-PL

status

FOR PLANNING

scale 1:200@A1



Appendix B

mail@syntegragroup.com
Tel: 0330 053 6774

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DO NOT SCALE DRAWING -
IF IN DOUBT, ASK

- GENERAL**
1. Do not scale from drawing.
 2. All dimensions are in metres, unless stated otherwise.
 3. This drawing to be read & printed in colour.
 4. This drawing to be read in conjunction with other contract drawings.
- CONSTRUCTION**
1. Works shall comply with the current Department of Transport Specification for Highway Works.
 2. Filling of voids formed by site clearance operations shall be measured under Series 600 of the Specification.
 3. Contractor is to ensure that all voids are to be filled with granular sub base material Type 1.
 4. All hard material broken out under the Contract is to be disposed of to contractor's tip.
- CDM (RISKS & HAZARDS)**
1. Prior to commencement of construction the contractor is to liaise with all relevant statutory undertakers and protect / divert apparatus and to protect the workforce during the works. Any damage caused to the apparatus to be the responsibility of the contractor.
 2. Contractor to undertake their own statutory plant checks on site prior to the commencement of excavation exercise.
 3. The contractor is to make sure that any excavation should be adequately covered at night to protect both public and wildlife from becoming trapped.
 4. Appropriate health and safety measures should be adhered to while working in close proximity to the existing overhead power lines.
- DISCLAIMERS**
1. The information contained in this drawing is based on a combination of OS and survey data provided by others and we shall not be liable for any inaccuracies or deficiencies.

Dennis Sabre Fire Tender (LWB)

Overall Length 7.700m
Overall Width 2.430m
Overall Body Height 3.512m
Min Body Ground Clearance 0.397m
Track Width 2.380m
Lock to lock time 5.00s
Kerb to Kerb Turning Radius 7.400m

Rev	Drawn	App'd	Date	Revision Description
P2	RID	JF	02.02.24	Red Lines Amended
P1	MSF	JF	01.02.24	First Issue

Issue

PRELIMINARY

Syntegra House, 63 Milford Road, Reading, RG1 8LG
Tel: 0118 4028520
mail@syntegragroup.com www.syntegragroup.com

Client

-

Project

39A Fitzjohn Avenue, Hampstead

Title

Fire Tender
Vehicle Swept Path Analysis

Scale: 1:100 @ A1	Drawn By: MSF
Date: FEB 2024	Checked By: JF
Org. No. 22019-ATR007	Rev. P2