



Construction Management Plan (CMP)

for the

Development to the rear of 17 Frognal, London NW3 6AR

Planning reference – 2019/2263/P

Prepared by CMP Construct Limited

Rev No.	Revision Description	Updated by	Date
1	Updated further to Client review	DFK	3 Aug 23
2	Updated further to Neighbours feedback	DFK	1 Dec 23
3	Log of Neighbours feedback included	DFK	26 Jan 24
4			

Introduction to CMP Construct Limited

CMP Construct Limited was established to provide professional Construction Management Plans and associated documents, to Clients, Developers, Contractors, Project Managers and Architects to support their Planning Applications and Discharge of Conditions. We have completed CMPs for the majority of London and South-East England Councils over the past 6 years.

CMP Construct Limited is led by Damien Kenny – Technical Director (BSc in Construction, MCIQB), who has over 30 years' experience in the delivery of Construction Projects across the UK. He has worked for a number of major contractors including Sir Robert McAlpine, Bovis Lendlease and has been a main board director at Mace Limited and at Morgan Sindall plc subsidiary – Overbury plc.

Contents

This document has been structured to follow the numbering and questions included in the London Borough of Camden – Construction Management Template dated 10 November 2022.

This CMP follows the best practice guidelines as described in the [Construction Logistics and Community Safety \(CLOCS\) Standard](#) and the [Guide for Contractors Working in Camden](#).

Section 1. Introduction & Contact

Section 2. Site.

Section 3. Community Liaison

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Section 1. Introduction and Contact

The following Construction Management Plan (CMP) has been produced for, the site owner Nick Sofroniou, to explain the proposed programming and construction logistics methodology for the development to the Rear of 17 Frognal, London NW3 6AR for the construction of a single storey three-bedroom home and associated amenity space.

The CMP has been produced in accordance with the requirements of the Section 106 Agreement dated 1 December 2021.

1. Please provide the full postal address of the site and the planning reference relating to the construction works.

Address: Rear of 17 Frognal, London NW3 6AR

Planning reference number to which the CMP applies: 2019/2263/P

2. Please provide contact details for the person responsible for submitting the CMP.

Name: Damien Kenny – Technical Director – CMP Construct Limited

Address: 112 South Block, 1b Belvedere Road, London, SE1 7GD

Email: damien@cmpconstruct.com

Phone: 07973 205 531

3. Please provide full contact details of the site project manager responsible for day-to-day management of the works and dealing with any complaints from local residents and businesses.

Name: The client is currently tendering the works and we will update once appointed

Address:

Email:

Phone:

4. Please provide full contact details of the person responsible for community liaison and dealing with any complaints from local residents and businesses if different from question 3. In the case of the Community Investment Programme (CIP), please provide the contact details of the Camden officer responsible.

Name: This will be the contractor as per Question 3

Address:

Email:

Phone:

5. Please provide full contact details including the address where the main contractor accepts receipt of legal documents for the person responsible for the implementation of the CMP.

Name: This will be the contractor as Q3, details to be added upon appointment

Address:

Email:

Phone:

Section 2. Site

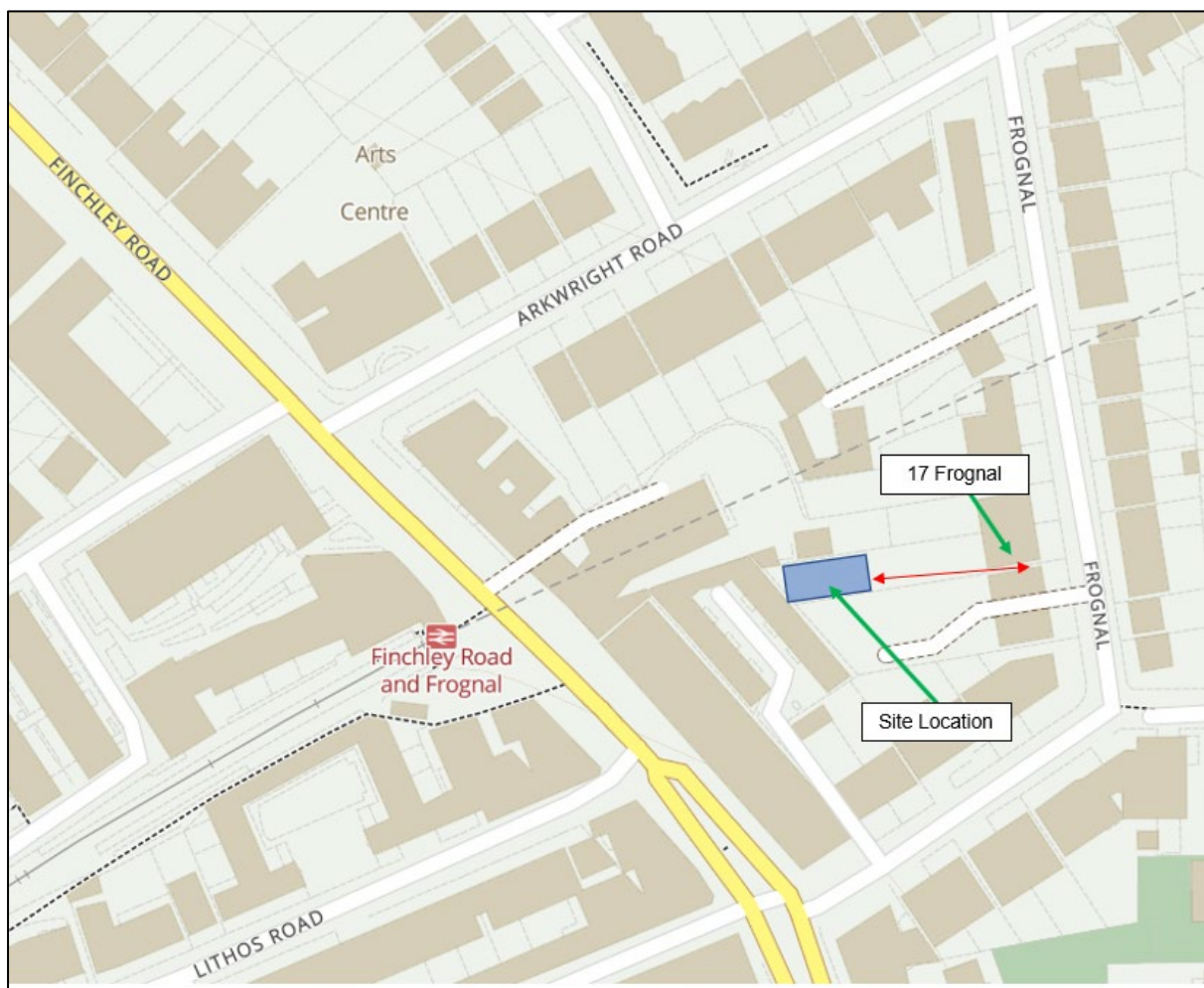
6. Please provide a site location plan and a brief description of the site, surrounding area and development proposals for which the CMP applies. Please fill up [Cumulative Impact Area \(CIA\) checklist form](#) if site fall within the CIA zone (Central London)

The site does not fall within the Cumulative Impact Area (CIA), as it is located in the Western side of the borough adjacent to Finchley Road.

The scope of works includes the clearance of the site which has been used as a garden, and the construction of a single storey, three-bedroom home on piled foundations, beam and block flooring and using traditional blockwork walls with timber and metal cladding to the external elevations together with a cycle store, amenity space and landscaping of the external areas.

The primary access to the site is proposed to be via the side entrance to 17 Frognal.

The intention is that the site operations will commence with the erection of a plywood walkway from Frognal to the site to give an even access for the movement of materials.



Location Plan

7. Please provide a very brief description of the construction works including the size and nature of the development and details of the main issues and challenges (e.g., narrow streets, close proximity to residential dwellings etc).

To minimise the impact on the adjacent properties, the construction of the piling, foundations, drainage and beam and block ground floor slab to the new house will be undertaken by a competent groundworks subcontractor. The works are planned to commence after the site clearance works are completed and the erection of the site set up and hoardings.

Foundation Works will follow the sequence and methodology prepared by a competent Structural Engineer commencing with the installation of piling, pile caps strip footings together with any propping required to provide a stable environment which will allow the completion of the foundation works, this will minimise the risk of movement and subsidence on adjacent buildings.

The excavation of the foundations/groundbeams will be completed by small excavator and excavated materials will be loaded into skips located on a suspended parking bay on Frognal. The skips will be regularly collected with the wheels of the skip vehicles remaining on the highway road at all times.

All vehicles leaving the site, via Frognal will be monitored to ensure that should any dirt or dust be dropped onto the highway it is immediately cleaned up. Further to this, the area around the site, will be regularly and adequately swept to prevent any accumulation of dust and dirt.

All waste away vehicles shall be properly covered when leaving the site and disposed of at a licensed tip.

To ensure the minimum disruption to users of Frognal and to prevent damage to the existing footpath, it is proposed that the contractor will provide protection to the footpath surface using specialist protective mats, during the deliveries to site. To ensure delivery periods are kept to a minimum, it is intended to utilise the use of palletted materials which can be offloaded quickly using a lorry mounted crane and delivered into the site compound using a trolley suitable for the width of the path, excavated materials will also be moved using suitable size wheelbarrows as shown overleaf .

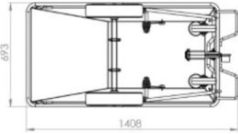
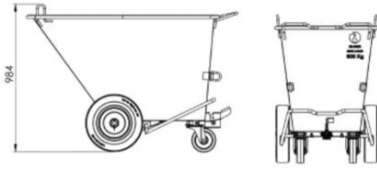
EUROTRAK HEAVY DUTY ACCESS MAT



SKU GM215601

These heavy-duty access mats are designed as a temporary short term and long-term access solution whilst providing ground protection for heavy vehicles and pedestrians. Quick and easy to install, this mat has a highly engineered non-slip traction surface which helps reduce slippage. With 6 specifically positioned hand/connection holes the mat is a lightweight mat that packs a punch. Withstands 60 tonnes.

SAFE RUBBLE SKIP



SKU MP2152

The Safe Rubble Skip has been designed to transport up to 355 litres of waste materials such as construction rubble safely away from site.

Overall Dimensions

↕	OVERALL HEIGHT	910 mm
↕	OVERALL DEPTH	1190 mm
↔	OVERALL WIDTH	690 mm
↔	PLATFORM THICKNESS	40 mm
⊕	WHEEL DIAMETER	195 mm
⊕	HOLE DIAMETER	34 mm
⊕	BOTTOM GAP	80 mm
⊕	POLE GAP	215 mm
⚖	WEIGHT	35.3 kg

8. Please provide the proposed start and end dates for each phase of construction as well as an overall programme timescale.

The principal strategy in programming the works, is to minimise the disruption to the residents of Frognal, Hampstead Gate and adjacent roads, road users, and pedestrians in and around Frognal and adjacent streets during the construction programme. It is proposed to achieve this both by timing the works to minimise disruption to adjacent properties during the working day, but to also avoid traffic movements to and from the site during peak periods, including the school drop off and pick up times.

Subject to planning conditions being discharged in Late 2023/Early 2024, it would be envisaged that the contractor will commence the site works in Spring 2024.

On commencement of the construction works, the first activity will be the site clearance and construction of the foundations and drainage to the new dwelling this will be completed in the initial 1-2-month period, with the construction works being completed over a 3-4-month period and fit out works being completed over a 2-3-month period for projected completion in Autumn 2024.

The phasing will be as follows:

Phase 1. Site set up / Foundations / Drainage	1-2 months.
Phase 2. Building Envelope inc. Roofs	3-4 months
Phase 3. Internal Fit Out / External works	2-3 months.

The phasing outlined above is indicative and there will be some overlapping between the phases to suit site conditions and sequencing.

It is envisaged that during the project the loading and unloading of vehicles will be completed from Frognal using 2 and 3 axle HGV vehicles, which will access the site via the designated access route.

During the delivery of materials, to protect pedestrians and cyclists, a traffic marshal will be in place to stop traffic and pedestrians for a short period, as vehicles unload onto the pavement of Frognal, before the materials are moved immediately to the site via the footpath, minimising the disruption to the general public from the works.

9. Please confirm the standard working hours for the site, noting that the standard working hours for construction sites in Camden are as follows:

- 8.00am to 6pm on Monday to Friday
- 8.00am to 1.00pm on Saturdays
- No working on Sundays or Public Holidays

This is Camden's standard times. However, the times operated should be specific to the site and related to the type of work being carried out, and the proposed working hours will be considered on a case-by-case basis.

If the site is within the Cumulative Impact Area (CIA), then Saturday working is not permitted, unless agreed with Camden.

The programme is based on the working hours for the site being in accordance with London Borough of Camden Council permitted hours.

08:00 and 18:00 Monday to Friday (normal working)

08:00 and 13:00 on Saturday (no noisy works e.g., excavation works)

No work on Sunday, Bank and Public Holidays

All site deliveries and rubbish removal will be arranged between these hours and will be co-ordinated and managed on a 'just-in-time' delivery basis. Deliveries will be programmed to avoid the peak travel periods and arrival and departure of parents and children at local schools of 8.00am to 9.30am and 3.00pm to 5.00pm Monday to Friday, during term time. All subcontractors and suppliers will be required to agree dates and times prior to delivery in addition confirmation of size of vehicle and unloading point.

Any noisy work outside these hours will only be undertaken by prior agreement, and / or reasonable notice to London Borough of Camden Environmental Health Teams. However, it is envisaged that all of the works will be completed without the need for out-of-hours works.

NB – The site is not located within the Cumulative Impact Area (CIA)

Section 3. Community Liaison

10. Sensitive/affected receptors

Please identify the nearest potential receptors (dwellings, business, etc.) likely to be affected by the activities on site (i.e., noise, vibration, dust, fumes, lighting etc.).

The diagram below shows the neighbouring properties which will potential direct impacted during the siteworks and who have all been contacted by the Developer as part of the Communications strategy for the redevelopment of the site during the CMP process.

The areas shown on the plan in red highlight the residential properties, yellow the retail and commercial facilities in the proximity of the site, which could potentially be impacted by the works, for some period of the project programme.



11. Consultation

Please provide details of consultation of the draft CMP with local residents, businesses, local groups (e.g., residents/tenants and business associations) and Ward Councillors.

The client, Nick Sofroniou, has been in contact with the residents of the adjacent properties in Early August 2023 on both Frognal and at Court Mansions to discuss the use of access routes to the site using the passageway adjacent to 17 Frognal, which will become the permanent access to the new development and to discuss the site activities to be completed. Adjustments to the movement of materials has been made in light of the comments from the residents of 17 Frognal, in this latest revision of the CMP, see summary below.

17 Frognal - Residents Consultation - Log of responses

Date	Consultee	Subject	Action
08-Aug-23	Astrid C - 17 Frognal	Confirmation of Construction Period	Programme confirmed as per CMP
11-Aug-23	Kate C - 17 Frognal	Access route to rear of 17 Frognal	Methodology updated in CMP Rev 2 - 1 Dec 23, to reflect comments
10-Aug-23	Brett D - 17 Frognal	Access to rear of 17 Frognal - movement of materials	Methodology updated in CMP Rev 2 - 1 Dec 23, to reflect comments
15-Aug-23	Rebecca G - 17 Frognal	Access to rear of 17 Frognal - movement of materials	Methodology updated in CMP Rev 2 - 1 Dec 23, to reflect comments
16-Aug-23	Valentina A - 17 Frognal	Access to rear of 17 Frognal - movement of materials	Methodology updated in CMP Rev 2 - 1 Dec 23, to reflect comments
18-Aug-23	Artur S - 17 Frognal	Access to rear of 17 Frognal - movement of materials	Methodology updated in CMP Rev 2 - 1 Dec 23, to reflect comments
29-Aug-23	Linda L - 17 Frognal	Access to rear of 17 Frognal - movement of materials	Methodology updated in CMP Rev 2 - 1 Dec 23, to reflect comments

Contact was also made in Early August 23 as part of consultation process with local councillor – Gio Spinella who is the Local Ward Councillor for the Frognal Ward.

The new drainage and electrical connection to the site will come from the residential development at Hamstead Gate immediately to the west of the site, these neighbours have also been contacted as part of the consultation process, and the proposed plans have been discussed in detail with the residents as it will require a localised suspension of two parking bays whilst the works are being completed, which has been agreed with the residents.

To the north of the site there is a derelict house, who's owners have been consulted, however the current understanding is that no work to this property will take place during the duration of the construction of the new house.

Allied Irish Bank who have a car park to the North of the site have been consulted in relation to the works and understand that the impact of the works is likely to be minimal as the office is

some distance from the site boundary and is an air-conditioned building with limited openable windows.

12. Construction Working Group

For particularly sensitive/contentious sites, or sites located in areas where there are high levels of construction activity, it may be necessary to set up a construction working group.

As this is a small development with limited development in the proximity of the site, it is not considered necessary to set up a construction working group.

13. Schemes

Please provide details of your Considerate Constructors Scheme (CCS) registration. Please note that Camden requires [CCS site registration](#) for the full duration of your project including additional [CLOCS visits](#) for the full duration of your project. Please provide the CCS site ID number that is specific to the above site. A company registration will not be accepted, the site must be registered with CCS.

Be advised that Camden is a Client Partner with the Considerate Constructors Scheme and has access to all CCS inspection and CLOCS monitoring reports undertaken by CCS.

Contractors will also be required to follow the [Guide for Contractors Working in Camden](#). Please confirm that you have read and understood this, and that you agree to abide by it.

Whilst a contractor has yet to be appointed, part of the tender requirements is that the selected Contractor will register the project with the Considerate Constructors Scheme and will ensure the CCS / CLOCS monitoring is undertaken.

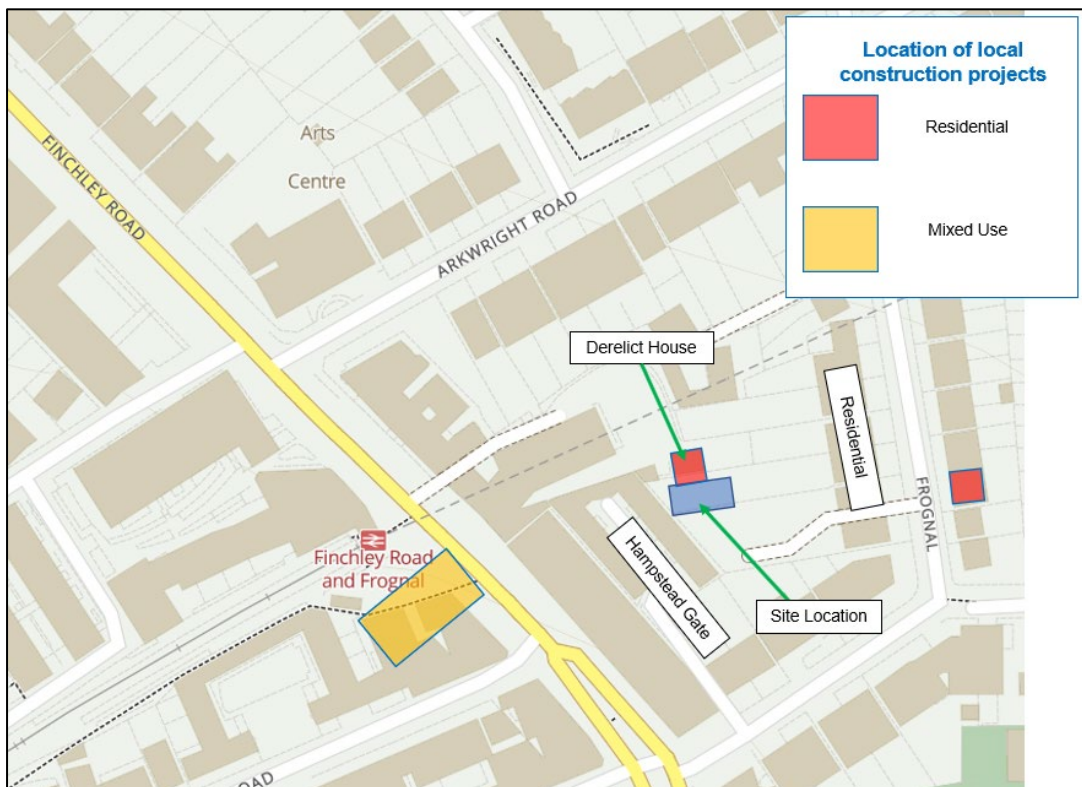
A copy of the Guide for Contractors Working in Camden has been included in the Tender documents and it is a requirement that the Contractor will confirm they have read and understood the guidelines and will abide by them for the duration of the works.

14. Neighbouring sites

Please provide a plan of existing or anticipated construction sites in the local area and please state how your CMP takes into consideration and mitigates the cumulative impacts of construction in the vicinity of the site. The council can advise on this if necessary.

There are limited sites in the area of the development at 17 Frognal as shown on the drawing below, so limited mitigation over and above that described in response to Question 9 is proposed.

NB Derelict House is not expected to be progressed during the project programme.



Section 4. Transport

CLOCS Contractual Considerations

15. Name of Principal contractor:

The principal contractor has yet to be appointed as the project is currently being tendered, the contractor selected will comply with the CDM Regulations 2015 and will register the contract with the Considerate Constructors Scheme (CCS).

16. Please submit the proposed method for checking operational, vehicle and driver compliance with the CLOCS Standard throughout the duration of the contract.

We have made it a condition of the tender that all supplier agreements with the suppliers delivering to site that vehicles used to deliver to site must be less than three years old and must as a minimum comply with the standards laid down in the London Ultra Low Emissions Zone (ULEZ). As of October 2021, the ULEZ was extended and now includes the location of the project.

All operators of delivery vehicles to the site will also be expected to adopt the Fleet Operator Recognition Scheme (FORS) and the Construction Logistics and Community Safety (CLOCS) standards and will be expected to become as a minimum FORS silver accredited, and CLOCS silver standard accredited.

As part of the procurement of the Enabling Works package, the contractor was required to ensure any subcontractors confirm that they will comply with the requirements of CLOCS and FORS.

It will also be a requirement that all HGV vehicles delivering to site, should be a minimum of three stars on the HGV Direct Vision Standard, by virtue of complying with CLOCS and FORS, the vehicles will generally be compliant with the HGV Direct Vision Standard.

17. Please confirm that you as the client/developer and your principal contractor have read and understood the CLOCS Standard and included it in your contracts.

I confirm on behalf of the developer, Nick Sofroniou, that the tender documents for the Contractor have included the requirement to abide by the CLOCS Standard and have also advised, that all subcontracts must also include the requirement to comply.



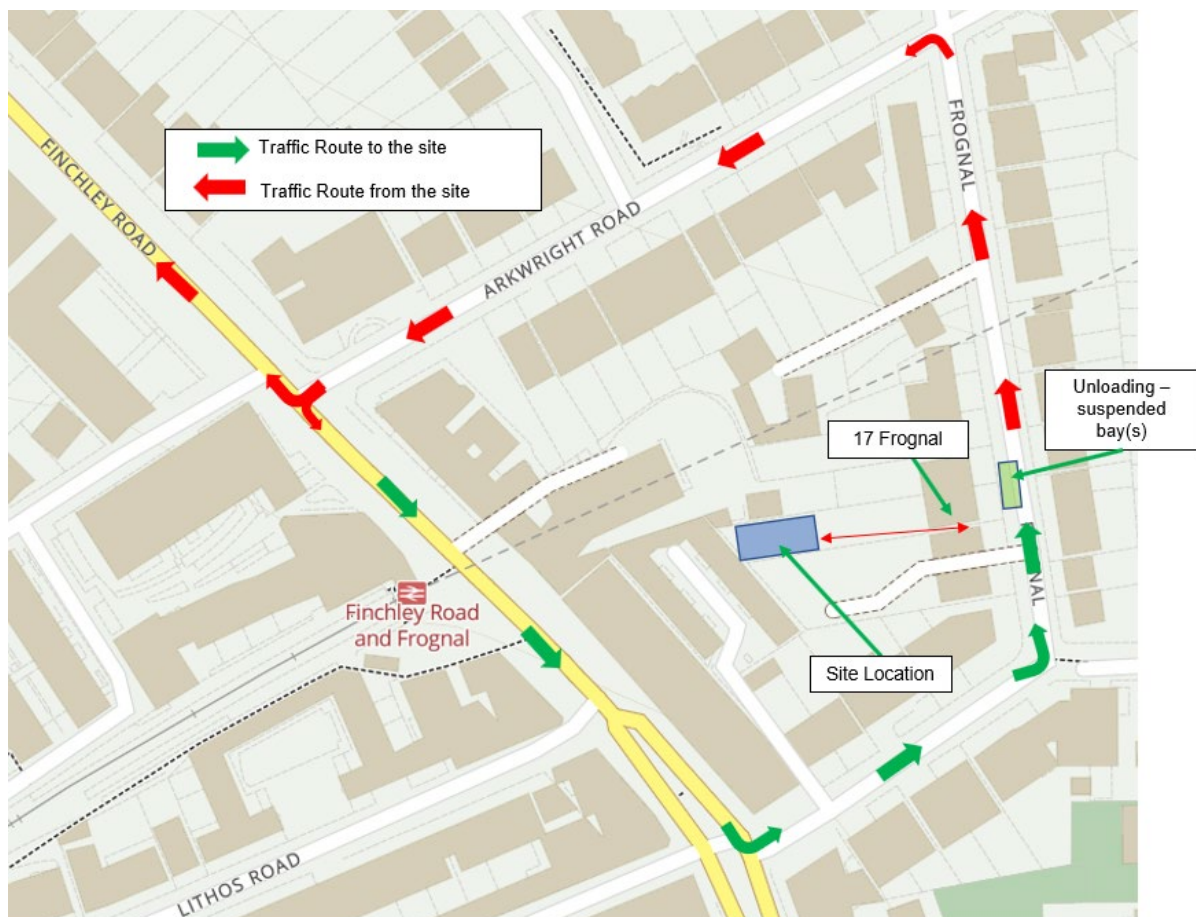
Damien Kenny – CMP Construct Limited.

18. Traffic routing: *“Clients shall ensure that a suitable, risk assessed vehicle route to the site is specified and that the route is communicated to all contractors and drivers. Clients shall make contractors and any other service suppliers aware that they are to use these routes at all times unless unavoidable diversions occur.”* (P19, 3.4.5)

a. Please show vehicle approach and departure routes between the site and the Transport for London Road Network (TLRN). Please note that routes may differ for articulated and rigid HGVs.

Routes should be shown clearly on a map, with approach and departure routes clearly marked. If this is attached, use the following space to reference its location in the appendices.

We have outlined below the access routes to and from the site from the A41 Finchley Road. Vehicles will arrive from the North leaving the A41 turning left into Frognal and bear left on Frognal to reach the site unloading point. Vehicles will leave the site heading North on Frognal before turning left into Arkwright Road where they head west to re-join the A41, where they can either head North or South. The A41 provides access to the North to the A406/A1/M1 and to the south to Central London



b. Please confirm how contractors and delivery companies will be made aware of the route (to and from the site) and of any on-site restrictions, prior to undertaking journeys.

All suppliers will be issued with site access details which will clearly layout the following: -

- Access route be followed to and from site.
- Delivery times
- Location of Unloading area
- No waiting rules.
- Location of off-site waiting area – HGV parking

19. Control of site traffic, particularly at peak hours: *“Clients shall consider other options to plan and control vehicles and reduce peak hour deliveries”* (P20, 3.4.6)

a. Please provide details of the types of vehicles required to service the site and the approximate number of deliveries per day for each vehicle type during the various phases of the project.

We have outlined below the principal types of vehicles that will be used to service the site during the construction period.

	<p>Tipper truck</p> <p>Used for the removal of site clearance / excavated materials from site</p>
	<p>Ready mix delivery truck</p> <p>Used for the delivery of ready-mix concrete for piles and foundations to the building.</p>
	<p>Flat-bed delivery truck</p> <p>Used for the delivery of beam and block floor, bricks/blocks, new cladding / windows, plasterboard and general materials to site, most vehicles will be equipped with lorry mounted cranes</p>
	<p>Skip truck</p> <p>Used for the removal of waste materials from the site set up and from the fit out of the new house.</p>

The following details and schedules provide an overview of the projected plant and vehicles that will be involved in the delivery of materials and construction activities on site. We have broken the analysis into the following phases.

- **Phase 1 & 2** – Site Clearance, Foundations and New build works.
- **Phase 3** –Fit out works.

Plant and Usage	Phase 1/2	Phase 3	Vehicle Movements	Total Duration On Site
Site Enabling Works (flat-bed lorries) required for the erection of site set up and hoardings	✓		2-3 per week	1 week
Site Clearance and Foundation Materials (2-4 axle tipper and skip trucks) Removal of site clearance and excavated materials	✓		2-3 per week	4-5 weeks
Ready Mix Concrete (2-4 axle trucks) Piling and Foundations	✓		5-8 visits	4-5 weeks
Beam and Block flooring, brick, block and timber – Building Frame (3 axle lorries with Hiab crane) Required for new superstructure works and internal walls	✓		1-2 per week	3 months
Hand / Power Tools (van deliveries) required for all works during period of construction	✓	✓	n/a	8 months
Scaffolding/ towers (flat-bed 2/3 axle vehicles) required to protect public, safe methods of working to external envelope for movement of materials	✓		1-2 deliveries for erection and dismantling	3 months
Material Delivery Vehicles (van and small box vehicles deliveries) required for fit out works e.g., plasterboard, sanitaryware, kitchens		✓	3-4 per week	3 months
Skip and Compactor Vehicles (2/3 axle specialist skip and compactor vehicles) required for general waste removal during construction and fit out periods		✓	1 per week	4 months

b. Please specify the permitted delivery times.

All site deliveries and rubbish removal will be arranged during the site hours and will be co-ordinated and managed on a ‘just-in-time’ delivery basis. Deliveries will be programmed to avoid the peak travel periods and arrival and departure of local school and college students of 8.00am to 9.30am and 3.00pm to 5.00pm Monday to Friday.

All subcontractors and suppliers will be required to agree dates and times prior to delivery in addition confirmation of size of vehicle and unloading point.

c. Cumulative effects of construction traffic servicing multiple sites should be minimised where possible. Please provide details of other developments in the local area or on the route that might require deliveries coordination between two or more sites. This is particularly relevant for sites in very constrained locations.

We have highlighted in the response to Question 14, the location of current operating sites in the locality and on the proposed access routes, as there are no large sites close and the delivery requirements for this site are relatively light, we do believe there is a need for a delivery co-ordination strategy with other sites.

d. Please provide swept path analyses for constrained manoeuvres along the proposed route.

As it proposed to load and unload from a suspended parking bay(s) in Frognaal, there are no constrained manoeuvres, that will require a Swept Path Analysis to be completed for.

e. Consideration should be given to the location of any necessary holding areas/waiting points for sites that can only accommodate one vehicle at a time/sites that are expected to receive large numbers of deliveries. Vehicles must not queue or circulate on the public highway. Whilst deliveries should be given set times to arrive, dwell and depart, no undue time pressures should be placed upon the driver at any time.

Please identify the locations of any off-site holding areas or waiting points. This can be a section of single yellow line that will allow the vehicle to wait to phone the site to check that the delivery can be accommodated.

Please refer to question 24 if any parking bay suspensions will be required to provide a holding area.

All subcontractors and suppliers will be required to agree dates and times prior to delivery in addition confirmation of size of vehicle fits the unloading point, all drivers will have the mobile number for the site manager and will be asked to call 10-15 minutes ahead of their planned delivery, to confirm arrangements.

As this is a small site with a maximum of 3-4 deliveries at peak per day, the issue of large numbers of vehicles is not an issue as confirmed in response to Q19 a)

Parking bay suspensions will be required for the offloading of materials from Frognaal.,

f. Delivery numbers should be minimised where possible. Please investigate the use of construction material consolidation centres, and/or delivery by water/rail if appropriate.

The small number of deliveries to the site, and its location do not make it suitable for considering consolidation or rail / water deliveries.

g. Emissions from engine idling should be minimised where possible. Please provide details of measures that will be taken to reduce delivery vehicle engine idling, both on and off site (this does not apply to concrete mixers).

All drivers will be asked to turn off their engine during deliveries, unless required to power Hiab cranes or concrete mixer drums.

20. Site entry/exit: “Clients shall ensure that access to and egress from the site is appropriately managed, clearly marked, understood and clear of obstacles.” (P18, 3.4.3)

This section is not used, as vehicles will be unloaded on the highway.

21. Vehicle loading and unloading: “Clients shall ensure that vehicles are loaded and unloaded on-site as far as is practicable.” (P19, 3.4.4)

a. Please provide the location where vehicles will stop to unload. If this is attached, use the following space to reference its location in the appendices. Please outline in question 24 if any parking bay suspensions will be required.

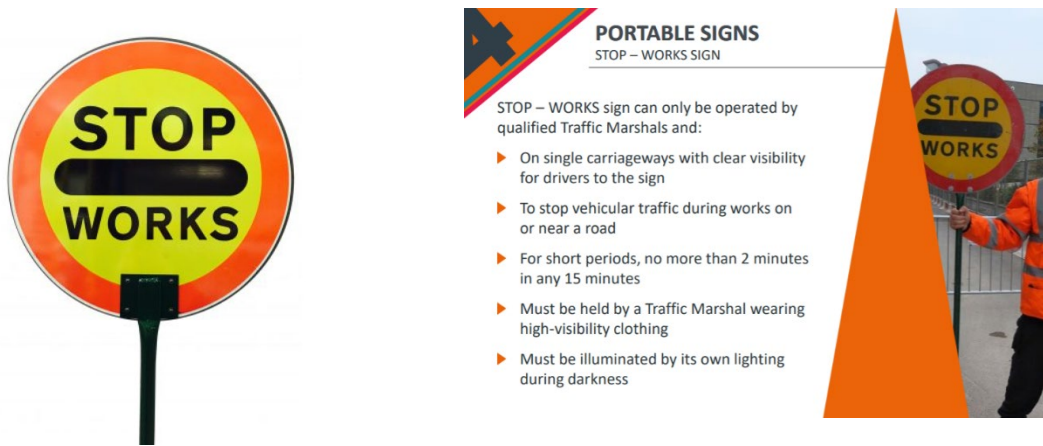
Please see diagram below showing proposed location of suspended parking bays for delivery of materials during the working day



b. Where necessary, Traffic Marshals must ensure the safe passage of pedestrians, cyclists and motor traffic in the street when vehicles are being loaded or unloaded. Please provide detail of the way in which marshals will assist with this process. Please note that deliveries should pause where possible to allow passage to pedestrians.

During the delivery of materials / collection of waste materials, to protect pedestrians, cyclists and other road users, a traffic marshal will be in place to stop traffic and pedestrians for a short period, as vehicles reverse into the suspended parking bay, minimising the disruption to the general public from the works. All vehicles will leave site, leaving in forward gear, negating the need for vehicles to reverse back down Frognal.

The intention will be to arrange for the majority of deliveries to be made using vehicles with lorry mounted Hiab cranes, so materials can be lifted directly from the lorry into site, so there will be no unloading or storage of materials on the public highway.



Control signage to be used during arrival and departure of vehicles.

The traffic will utilise a TSRGD 7031 – Stop Works sign (as per example above), to control the traffic, cyclists and pedestrians whilst vehicles are arriving and departing the site and will limit the stopping of traffic to a maximum of two minutes in any 15-minute period.

All vehicle manoeuvres will be in line with the plan above so vehicles reversing will be under the control of a traffic marshal at all times.

Site Set-up

22. Site set-up and occupation of the public highway

Please provide detail drawings of the site up on the public highway. This should be presented as a scaled plan detailing the local highway network layout in the vicinity of the site. This should include details of on-street parking bay locations, cycle lanes, footway extents, relevant street furniture, and all relevant key dimensions. Please note that lighting column removal/relocation may be subject to UKPN lead times and is outside of our control. Any gantries will require a structural assessment and separate agreement with the structures team.

a. Please provide details of any measures and/or structures that need to be placed on the highway. This includes dedicated pit lanes, temporary vehicle access points/temporary enlargement of existing crossovers, occupied parking bays, hoarding lines, gantries, crane locations, crane oversail, scaffolding, scaffolding oversail, ramps, barriers etc. Please use this space to justify the use of the highway, and to state how the impacts have been minimised. Please provide drawings separately in the appendices and reference their location below. Please provide further details of any changes to parking and loading in section 23.

It is not proposed to put any structures or store materials on the public highway or pavements. Only during the delivery of materials Unloading signs will be put in place warning pedestrians of material deliveries, the movement of materials across the pavement will be controlled by a traffic marshal who will ensure pedestrians are safe at all times.





b. Please provide details and associated drawings/diagrams showing any temporary traffic management measures needed as part of the above site set up. Alternatively, this can be shown as part of the above drawings if preferred. Please note that this must conform to the [Safety at Street Works and Road Works Code of Practice](#).

Please refer to response to Question 22a)

23. Parking Bay suspensions and temporary traffic orders

Parking bay suspensions should only be requested where absolutely necessary and these are allowed for a maximum period of 6 months only. Information regarding parking suspensions can be found [here](#). For periods greater than 6 months, or for any other changes to the parking/loading/restrictions on the highway, a [Temporary Traffic Restriction \(TTR\)](#) will be required for which there is a separate cost. Please note that any temporary changes to parking and loading to be delivered using a TTR need to be consulted upon as part of our legal obligations as a highway's authority. Camden may require separate consultation to take place specifically around such changes if these have not been adequately reflected in any prior consultation as part of the CMP process.

A space cannot be suspended for convenience parking, a [trade permit](#) is available for trade vehicle parking. Building materials and equipment must not cause obstructions on the highway. Building materials may only be stored on the public highway if permitted by the Street Works team.

Please provide details of any proposed such changes on the public highway which are necessary to facilitate the construction works. Where these changes apply to parking bays, please specify the type of bays that are to be impacted and the anticipated timeframes.

It is proposed to apply for a TTR to suspend two residents parking bays for trade use only during the working day – Monday to Friday, it is intended that no materials will be stored on the highway and pavement as confirmed in responses to Questions 21 a) and b)

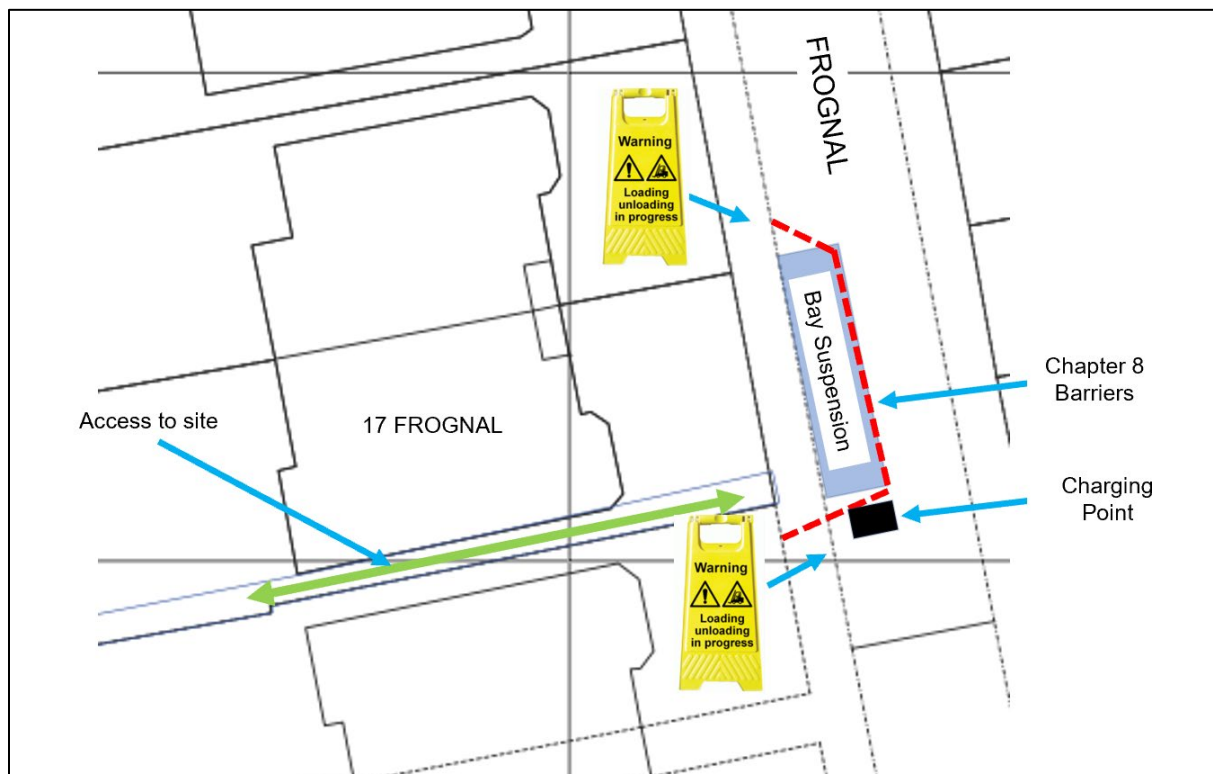
24. Motor vehicle/cyclist diversions/pedestrian diversions

Pedestrians' safety must be maintained if diversions are put in place. Vulnerable footway users must be considered as part of this. These include wheelchair users, the elderly, those with walking difficulties, young children, those with prams, the blind/partially sighted. Appropriate ramps must be used if cables, hoses, etc. are run across the footway.

Please note that footway closures are not permitted unless there is no alternative. Footway access must be maintained using a gantry or temporary walkway in the carriageway unless this is not possible. Where this is not possible, safe crossing points must be provided to ensure that pedestrian access is maintained. Where formal or controlled crossing points are to be suspended, similar temporary facilities must be provided. Camden reserves the right to require temporary controlled crossing points in the event of any footway closures.

Please provide details of any diversion, disruption or other anticipated use of the public highway during the construction period. Please show locations of diversion signs on drawings or diagrams and provide these in the appendices. Please use the following space to outline these changes to and to reference the location of any associated drawings in the appendices. Please show diversions and associated signage separately for pedestrians/cyclists/motor traffic.

We have already highlighted in the responses to Question 21 a) and b) the proposed arrangement during deliveries, please see sketch below for further details.

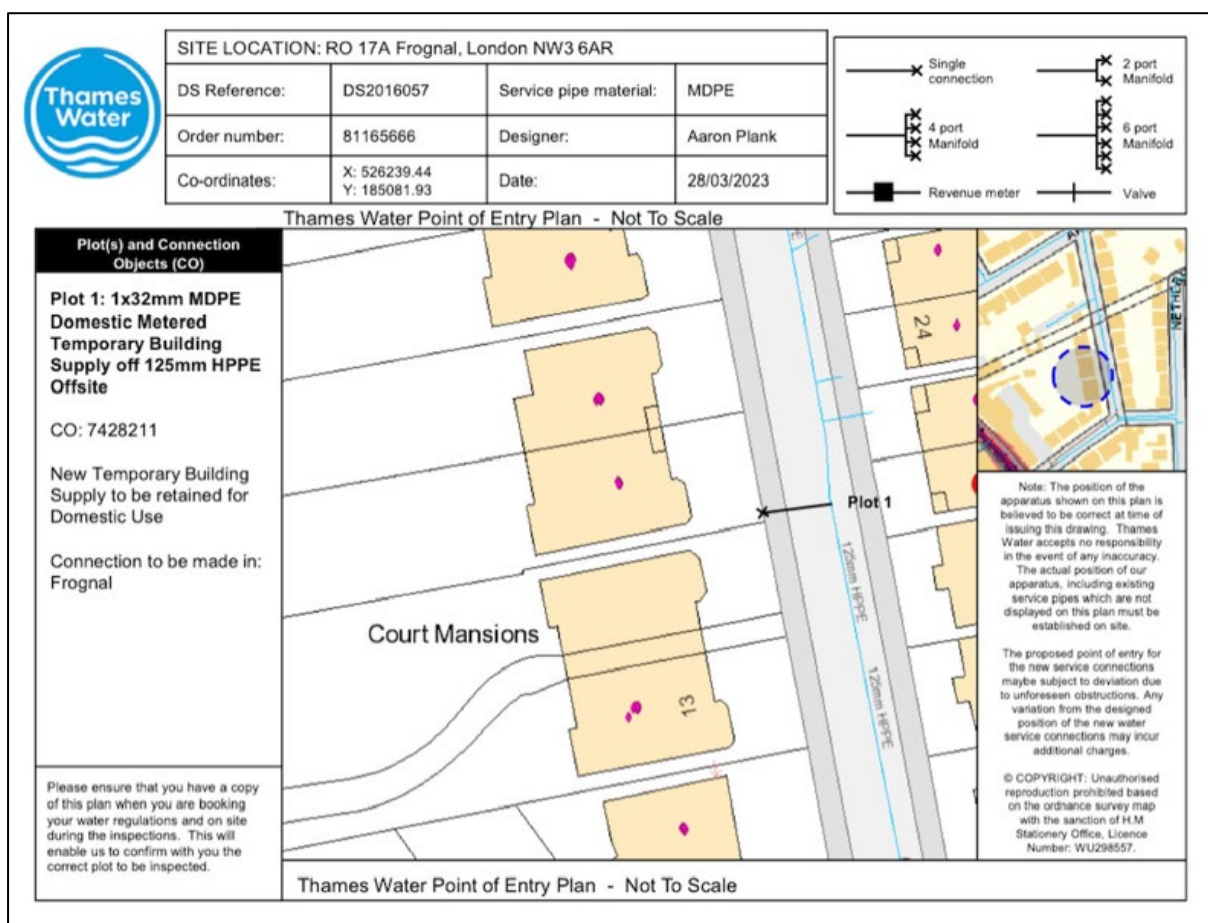


25. Services

Please indicate if any changes to services are proposed to be carried out that would be linked to the site during the works (i.e., connections to public utilities and/or statutory undertakers' plant). Larger developments may require new utility services. If so, a strategy and programme for coordinating the connection of services will be required. If new utility services are required, please confirm which utility companies have been contacted (e.g., Thames Water, National Grid, EDF Energy, BT etc.) You must explore options for the utility companies to share the same excavations and traffic management proposals. Please supply details of your discussions.

Drainage and Electrical connections will come via Hampstead Gate, which is a privately owned road, and agreements are in place with the residents to agree the suspension of parking bays to complete the connections.

The 32mm water supply provided by Thames Water, will be picked up from the pavement on Frognal as per the sketch below. As they are the only service coming in this route, there is no opportunity to share excavations. The Contractor will ensure that they co-ordinate site deliveries, as the pipe route is along the passageway next to 17 Frognal, which is the main access route for materials.



Section 5. Environmental

28. Please list all noisy operation and the construction methods used and provide details of the times that each of these are due to be carried out.

The intention is to limit noisy activities, where practicable noisy plant and equipment will be situated as far as possible from noise sensitive buildings and / or acoustic lined enclosures will be erected. Use of plant will be limited to the site working hours.

Potential Noisy Activities

Activity	Potential Noise source
Piling	Piling Rig / Concrete Pump / Concrete Mixer
Excavation/Foundations	Excavator / Dumper Truck
Brickwork / Blockwork	Cutting of bricks and blocks
Sheet roofing	Cutting of membrane / roof sheets
External Works	Excavator / Dumper Truck / Cutting of paving slabs

29. Please confirm when the most recent pre-construction noise survey was carried out and provide a copy. If a noise survey has not taken place, and it has been requested by the local authority, please indicate the date (before any works are being carried out) that the noise survey will be taking place and agree to provide a copy.

A pre-construction noise survey has not been carried out as there are very limited noisy activities to be completed on the project. The client will complete a noise survey if required by Camden Environmental Team

30. Please provide predictions for noise levels throughout the proposed works.

As indicated in our response to Question 29 there is limited high noise generating activities in completing the build of a small single storey house, we do not therefore expect noise levels to exceed the recommendations in Camden's Minimum Requirements for Building Construction (CMRBC).

31. Please provide details describing mitigation measures to be incorporated during the construction/demolition works to prevent noise and vibration disturbances from the activities on the site, including the actions to be taken in cases where these exceed the predicted levels.

NB – There is no demolition required on this project.

Where practicable noisy plant and equipment will be situated as far as possible from noise sensitive buildings and / or acoustic lined enclosures will be erected. In accordance with the latest version of the Mayor of London's Planning Guidance on 'The Control of Dust and Emissions during Construction and Demolition', from 1 September 2015, any Non-Road Mobile Machinery (NRMM) of net power between 37kW and 560kW used on the project will be required to meet the standards based upon the engine emissions standards in EU Directive 97/68/EC and its subsequent amendments.

Where practicable, plant and equipment powered by mains electricity will be used in preference to equipment powered by petrol or diesel engine.

Where practicable, plant and equipment will be fitted with effective exhaust silencers; compressors will be fitted with properly lined and sealed acoustic covers which will be kept closed whenever in use; and pneumatic percussive tools will be fitted with mufflers or silencers of the type recommended by the manufacturers. All plant and equipment will be maintained in good and efficient working order and operated in such a manner as to minimise noise emissions.

Plant and equipment in intermittent use will be shut down or throttled down to a minimum when not in use.

Excessive noise on site can represent a major impact to site workers, neighbours and adjacent wildlife. To avoid these impacts, the selected Contractor will:

- Carefully select equipment, construction methods and programming to reduce noise and vibration.
- Use of hoardings or screens as noise barriers.
- Locate plant as far as reasonably practicable from receptor.
- Ensure that plant is shut down when they are not in use.
- Monitor the noise levels regularly to confirm the noise level of site activities.
- Include noise minimisation practice in induction.
- Liaise with the community to provide information of the noise work activities and their durations.
- Arrange delivery times to suit the area, as it is primarily a residential area.

32. Please provide evidence that staff have been trained on BS 5228:2009

The project is currently being tendered and it will be a requirement that the site foreman / manager has received training in Environmental Management including BS5228:2009.

33. Please provide specific details on how air pollution and dust nuisance arising from dusty activities on site will be prevented. This should be relevant and proportionate to activities due to take place, with a focus on both preventative and reactive mitigation measures.

The works will be carried out taking consideration of 'Air Quality: Best Practice Guidance - The Control of Dust and Emissions During Construction and Demolition Supplementary Planning Guidance (published by the Greater London Authority, July 2014).

Materials with the potential to produce dust e.g., sand, excavated materials and aggregates, will be kept away from working area boundaries and shall be stored in bunded areas or bins, and will be covered and not be allowed to dry out to minimise the risk of dust transfer.

Methods of working will be selected for all activities that will aim to minimise dust and air pollution.

No burning of materials / refuse will be permitted on the site.

Site Clearance and excavation pollution will be minimised by a combination of screening and watering down. No crushing of materials will be undertaken on site.

34. Please provide details describing how any significant amounts of dirt or dust that may be spread onto the public highway will be prevented and/or cleaned.

All vehicles will be loaded on the public highway using motorised wheelbarrows or small dumper to move the excavated materials so there should be no mud on the tyres of the vehicles. However, all vehicles leaving the site will, be subject to an inspection to ensure that they are 'clean 'enough to leave site without depositing dirt or debris on the public highway.

If there are any instances of mud/spoil being dropped during the loading of lorries onto the highway, the contractor will direct operatives to sweep up and clear the highway of debris.

35. For medium or high impact risk level sites, please provide details describing arrangements for monitoring of noise, vibration and dust levels, including instrumentation, locations of monitors and trigger levels where appropriate.

As this is a low-risk site this is not required.

36. Please confirm that an Air Quality Assessment and/or Dust Risk Assessment has been undertaken at planning application stage in line with the GLA policy [The Control of Dust and Emissions During Demolition and Construction 2014 \(SPG\)](#) (document access at bottom of webpage), and that the summary dust impact risk level (without mitigation) has been identified. The risk assessment must take account of proximity to all human receptors and sensitive receptors (e.g. schools, care homes etc.), as detailed in the [SPG](#). **Please attach the risk assessment and mitigation checklist as an appendix.**

As this is a low-risk site this has not been completed.

37. Please confirm that all of the GLA's 'highly recommended' measures from the SPG document relative to the level of dust impact risk identified in question 36 have been addressed by completing the GLA mitigation measures checklist. (See Appendix 7 of the SPG document.

As this is a low-risk site this has not been completed, however please refer to response to Question 33, for mitigation measures.

38. Please confirm the number of real-time dust monitors to be used on-site.

As this is a low-risk site real-time dust monitoring is not required.

39. Please provide details about how rodents, including rats, will be prevented from spreading out from the site. You are required to provide information about site inspections carried out and present copies of receipts (if work undertaken).

The ecology survey completed as part of the planning process has indicated no presence of Rats on the site, as it is currently used as a garden.

The contractor will ensure that any food left on site is in sealed containers and food waste will be stored in dustbins to prevent vermin being attracted to the site.

41. Complaints often arise from the conduct of builders in an area. Please confirm steps being taken to minimise this e.g., provision of a suitable smoking area, tackling bad language and unnecessary shouting.

As the site is located at the rear of the garden of 17 Frognal, the interface with the public will be minimal, and primarily only during the loading and unloading of vehicles.

At their site induction, all site staff and operatives will be advised that they should use the welfare area on site and be courteous to the neighbours and users of Frognal.

42. If you will be using non-road mobile machinery (NRMM) on site with net power between 37kW and 560kW it will be required to meet the standards set out below. The standards are applicable to both variable and constant speed engines and apply for both PM and NOx emissions. See the Mayor of London webpage 'Non-Road Mobile Machinery (NRMM)' for more information, a map of the Central Activity Zone, and for links to the NRMM Register and the NRMM Practical guide (V4):

<https://www.london.gov.uk/what-we-do/environment/pollution-and-air-quality/nrmm>

Direct link to NRMM Practical Guide (V4):

https://www.london.gov.uk/sites/default/files/nrmm_practical_guide_v4_sept20.pdf

From 1st September 2015

(i) Major Development Sites – NRMM used on the site of any major development will be required to meet Stage IIIA of EU Directive 97/68/EC

(ii) Any development site within the Central Activity Zone - NRMM used on any site within the Central Activity Zone will be required to meet Stage IIIB of EU Directive 97/68/EC

From 1st September 2020

(iii) Any development site - NRMM used on any site within Greater London will be required to meet Stage IIIB of EU Directive 97/68/EC

(iv) Any development site within the Central Activity Zone - NRMM used on any site within the Central Activity Zone will be required to meet Stage IV of EU Directive 97/68/EC

Please provide evidence demonstrating the above requirements will be met by answering the following questions:

- a) Construction time period (mm/yy - mm/yy): - March 2024 to November 2024
- b) Is the development within the CAZ? (Y/N): No
- c) Will the NRMM with net power between 37kW and 560kW meet the standards outlined above? (Y/N): Yes
- d) Please confirm that all relevant machinery will be registered on the NRMM Register, including the site name under which it has been registered: Confirmed
- e) Please confirm that an inventory of all NRMM will be kept on site and that all machinery will be regularly serviced and service logs kept on site for inspection: Confirmed
- f) Please confirm that records will be kept on site which details proof of emission limits, including legible photographs of individual engine plates for all equipment, and that this documentation will be made available to local authority officers as required: Confirmed

43. Vehicle engine idling (leaving engines running whilst parked or not in traffic) produces avoidable air pollution and can damage the health of drivers and local communities. Camden Council and the City of London Corporation lead the London **Idling Action Project** to educate drivers about the health impacts of air pollution and the importance of switching off engines as a simple action to help protect the health of all Londoners.

Idling Action calls for businesses and fleet operators to take the **Engines Off pledge** to reduce emissions and improve air quality by asking fleet drivers, employees and subcontractors to avoid idling their engines wherever possible. Free driver training materials are available from the website: <https://idlingaction.london/business/>

Please provide details about how you will reduce avoidable air pollution from engine idling, including whether your organisation has committed to the Engines Off pledge and the number of staff or subcontractors who have been provided with free training materials.

As already indicated in the response to Question 19 g), all drivers will be asked to turn off their engines during deliveries, unless required to power equipment, such as, concrete mixer or Hiab crane.

Mental Health Training

44. Poor mental health is inextricably linked to physical health, which in turn impacts performance and quality, and ultimately affects productivity, creativity and morale. Workers in the construction industry are six times more likely to take their own life than be killed in a fall from height.

We strongly recommend signing up to the "[Building Mental Health](#)" charter, an industry-wide framework and charter to tackle the poor mental health in the construction industry, or joining [Mates In Mind](#), which providing the skills, clarity and confidence to construction industry employers on how to raise awareness, improve understanding and address the stigma that surrounds mental health.

The Council can support by providing free Mental Health First Aid training, publicity resources and signposting to local support services.

Please state whether you are or will be signed up to the Building Mental Health charter (or similar scheme), and that and appropriate number of trained Mental Health First Aiders will be available on site.

As this is a very small site with a likely peak labour force of 4-6 operatives, it will be difficult to get a Mental Health Charter in place, however the contractor selected, will be asked to ensure that their First Aider has received Mental Health training.

Section 6. Agreement

The agreed contents of this Construction Management Plan must be complied with unless otherwise agreed in writing by the Council. This may require the CMP to be revised by the Developer and reapproved by the Council. The project manager shall work with the Council to review this Construction Management Plan if problems arise in relation to the construction of the development. Any future revised plan must be approved by the Council in writing and complied with thereafter.

It should be noted that any agreed Construction Management Plan does not prejudice further agreements that may be required such as road closures or hoarding licences.

Signed:



Date: 26 January 2024

Print Name: Damien Kenny

Position: Technical Director – CMP Construct Limited

Please submit to: planningobligations@camden.gov.uk

End of form.