

HS2

High Speed Rail (London – West Midlands) Act 2017

HS2 Ltd

London Borough of Camden

Amendment to Berm Wall Pack 2 (Northern extent of Euston Cavern Headhouse to Parkway Tunnels) - Park Village East Wall S1

Schedule 17 Plans and Specifications Written Statement for Information

LBC.PS.10014

Document Reference: 1MC03-SCJ_SDH-IN-STA-SS01_SL03-000005

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1 Introduction

1.1 Background Information

Table 1: Park Village East Wall Berm Wall Schedule 17 Address Details and Description of Works

| Site | Details |
|-------------------|---|
| Scheme | High Speed Two |
| Applicant | High Speed Two (HS2) Limited |
| Applicant Address | <i>c/o Agent:</i> SCS Railways Joint Venture (SCS) Black Arrow House 2 Chandos Road NW10 6NF |
| Site Address | Park Village East, London Borough of Camden. NW1 2DU The works are located from: X528716 (Easting), Y183511 (Northing) to X528670 (Easting), Y183592 (Northing). |
| Description | Submission under Schedule 17 of the High Speed Rail (London-West Midlands) Act for approval of amendments to the approved Park Village East Berm Wall (LPA application ref: 2021/0126/HS2). The Park Village East Berm (PVE Berm) is a berm retaining structure located in the railway cutting, adjacent to the existing PVE retaining wall and extending northwards from north of Euston Cavern Headhouse to south of Parkway Tunnel. The North West Retaining Wall is the northern extent of the PVE Berm. |

1.2 Terms of Reference

- 1.2.1 This Written Statement is compiled in accordance with the High Speed Two (HS2) Phase 1 Planning Memorandum and Planning Forum Notes (PFNs) as required by the planning regime established under Schedule 17 of the High-Speed Rail (London – West Midlands) Act 2017.
- 1.2.2 The submission documents that have been prepared to support the Plans and Specifications submission under Schedule 17 of the Act have been prepared in accordance with the PFN1, PFN2 and PFN3. The engagement undertaken to inform the preparation of this Plans and Specification submission has been in compliance with PFN4 and PFN5.

1.2.3 This statement provides the London Borough of Camden with information to assist with the determination of the Plans and Specifications submission under Schedule 17, in relation to the above description of works.

1.2.4 The information in this Written Statement is provided for information to assist in determining the request for approval. It is not for approval.

1.3 Introduction to High Speed 2

1.3.1 HS2 is a new high-speed railway network that will connect major cities in Britain. It will bring significant benefits for inter-urban rail travellers through increased capacity and improved connectivity between London, the Midlands, and the North. It will release capacity on the existing rail network and so provide opportunities to improve existing commuter, regional passenger, and freight services.

1.3.2 Phase One of HS2 will provide a dedicated high-speed rail service between London, Birmingham, and the West Midlands. It will extend for approximately 230km (143 miles). Just north of Lichfield, high speed trains will join the West Coast Main Line for journeys to and from Manchester, the North West and Scotland.

1.3.3 For further information on HS2 and the route through the London Borough of Camden please refer to the Planning Context Report for the London Borough of Camden, deposited with the Council by HS2 Ltd.

1.4 High Speed Rail (London – West Midlands) Act 2017

1.4.1 The High-Speed Rail (London – West Midlands) Act 2017 ('the Act') provides powers for the construction and operation of Phase 1 of High Speed Two. HS2 Ltd is the nominated undertaker in relation to the works subject to these Plans and Specifications submission.

1.4.2 Section 20 to the Act grants deemed planning permission for the works authorised by it, subject to the conditions set out in Schedule 17. Schedule 17 includes conditions requiring the following matters to be approved or agreed by the relevant LPA.

- Construction arrangements (including large goods vehicle routes),
- Plans and specifications,
- Bringing into use requests, and
- Site restoration schemes.

- 1.4.3 This is therefore a different planning regime to that which usually applies in England (i.e., the Town and Country Planning Act) and is different in terms of the nature of submissions and the issues that the LPAs can have regard to, in determining requests for approval.
- 1.4.4 Schedule 17 of the Act sets out the grounds on which the LPA may impose conditions on approvals or refuse requests for approval.
- 1.4.5 This Written Statement includes information supporting the Plans and Specifications submission in relation to the matters outlined in **Table 2** below.

Table 2 Schedule 17 Plans and Specifications Submission Details

| Site | Details |
|---|--|
| Plans and Specifications (Permanent works) | <ul style="list-style-type: none"> Building works – PVE Berm Wall (Paragraph 2) Earthworks – PVE Wall Berm Support Structure¹ (Paragraph 3) |

- 1.4.6 With respect to the details identified as earthworks, paragraph 3 (9) defines “earthworks” as, “*terracing, cuttings, embankments or other earthworks.*” The Retaining Wall constitutes earthworks because it is not a wall in the usual sense of forming a boundary or enclosure; instead, it forms part of the earthwork itself, being the external part of it and consequently an integral and necessary part of the cutting.
- 1.4.7 The works to which this application relates, and the cumulative impact of the works in conjunction with other HS2 development, have been assessed and are compliant with paragraph 1.1.3 (bullet point 2) of the HS2 Phase 1 Environmental Minimum Requirements General Principles².

1.5 High Speed Two: Environmental Minimum Requirements

- 1.5.1 The Environmental Statement (ES) (as amended) is an assessment of the likely significant environmental effects of the proposed HS2 railway and the proposals to avoid, reduce or remedy these likely significant environmental effects.
- 1.5.2 HS2 Ltd as the nominated undertaker is contractually bound to comply with the controls set out in the Environmental Minimum Requirements (EMRs). These

¹ The Park Village East Berm Wall is a structural support for the existing retaining wall. Further detail on the purpose and design of the berm is outlined in paragraph 4.1.2.

² https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/618074/General_principles.pdf

controls along with the powers contained in the High-Speed Rail (London – West Midlands) Act and the Undertakings and Assurances will ensure that impacts which have been assessed in the ES will not be exceeded.

1.5.3 The EMRs comprise the following suite of documents:

- Code of Construction Practice (CoCP),
- Planning Memorandum,
- Heritage Memorandum,
- Environmental Memorandum, and
- Undertakings and Assurances.

1.6 High Speed Two: Code of Construction Practice

1.6.1 HS2 Ltd as the nominated undertaker is contractually bound to comply with the controls set out in the Environmental Minimum Requirements. The Environmental Minimum Requirements include the High Speed Two Code of Construction Practice (CoCP).

1.7 Structure of Written Statement

1.7.1 This Written Statement is structured as follows:

- A description of the location and main characteristics of the works area is provided in **Section 2**,
- **Section 3** describes the main works being undertaken in the area, as set out in Schedule 1 of the Act, and those that are the subject of this Schedule 17 Plans and Specifications submission,
- The design criteria and rationale for the works which are the subject of this Schedule 17 Plans and Specifications submission are described in **Section 4**,
- **Section 5** summarises the pre-submission consultations that were undertaken, including a list of the consultees, dates, attendees at meetings and a brief summary of the outcome of these discussions,
- A high-level programme for the works and how they fit into the wider programme for other works in the area, as set out in Schedule 1 of the Act, is provided in **Section 6**, and
- **Section 7** identifies any other main consents, or known forthcoming consents associated with the works.

2 Site Location and Characteristics

2.1 Site Location

- 2.1.1 The application site (known hereafter as the 'site') is in an existing railway cutting, and is situated parallel to Park Village East, as shown in **Figure 1**. It is approximately 90 metres in length (as a straight-line distance) and approximately 0.068 hectares in area.
- 2.1.2 Park Village East adjoins Granby Terrace in the south, and Gloucester Gate in the north, providing a link through the residential area to the west of Regents Park.
- 2.1.3 The site lies to the west side of the West Coast Main Line (WCML) cutting to the north of Euston Station. It directly adjoins the existing Park Village East Retaining Wall. There is no direct view into the railway cutting site from the public realm.



Figure 1 Aerial view of Euston and surrounding area. Approximate site location marked in red (Google Maps 2019)

2.2 Adjacent Land Uses

- 2.2.1 The site lies below street level and parallel to Park Village East.
- 2.2.2 Within the railway cutting, immediately north-east of the site, are railway tunnel railings and piers which were added c.1900-6. These were built as part of a significant expansion of Euston Station and the cutting in the early 20th century, by the London & North-Western Railway.
- 2.2.3 These structures were constructed to match the older Grade II listed Parkway Tunnel and Cutting (further to the north-east) which was associated with the original London and Birmingham Railway, which started operation in 1837. East of the cutting are Grade II listed Georgian terraced dwellings on Mornington Terrace.
- 2.2.4 At street level, immediately to the north, there is a group of listed buildings on Parkway, including early 19th century Grade II listed terraced dwellings and a Grade II listed Public House build in c.1826-7 (namely, the York & Albany Public House). These buildings lie opposite the Grade II* listed Nash Villas, further to the south-west, on Park Village East.
- 2.2.5 Immediately south and adjacent to the listed buildings at street level is 117 Parkway which is a modern three-storey terrace building facing onto the cutting (approved under LPA ref: 2005/3996/P) and Park Village Studios (converted traditional stables) which faces onto Park Village East.
- 2.2.6 Also at street level, to the north, there is an overbridge with a carriageway comprising the junctions of the A4201 (Park Street), Gloucester Avenue, Oval Road, and Delancey Street.
- 2.2.7 Regents Park lies approximately 200m to the west of the site (as a straight-line distance) and is one of London's largest and most significant areas of open space.

2.3 Environmental Characteristics

- 2.3.1 Regent's Park Site of Metropolitan Importance (SMI) lies approximately 175m to the west of the application site. The park contains mature parkland trees, a small, enclosed woodland, an ornamental lake, and a grassland area managed specifically for wildlife.

2.3.2 As the site is separated from Regent's Park SMI by the intervening streets, it is not anticipated that construction works will have any impacts on the SMI.

2.3.3 In addition, there are several designated heritage assets in the area, as summarised in **Table 3** and **Figure 2**.

Table 3 Designated heritage assets in proximity to the application site.

| | Listed Asset | Type | Distance to PVE Berm Support Structure |
|---|-------------------------------------|-----------|---|
| 1 | Nash Villas along Park Village East | Grade II* | Adjacent to and above site at road level, along Park Village East highway |
| 2 | Parkway Tunnel and Cutting | Grade II | ~10m to east of eastern extent of site at railway cutting level |
| 3 | York and Albany Public House | Grade II | ~30m to west of site at street level |
| 4 | Parkway: Nos. 119 - 123 and 125 | Grade II | ~20m to west of site at street level |
| 5 | Regents Park Conservation Area | - | Adjacent to west of site |
| 6 | Camden Town Conservation Area | - | To east of cutting |

2.3.4 Regents Park Conservation Area lies to the west. It covers the eastern part of John Nash's Regent's Park masterplan development, as created in the early 19th century, including listed buildings on Parkway, Park Village East, and Park Village West, alongside Regents Park.

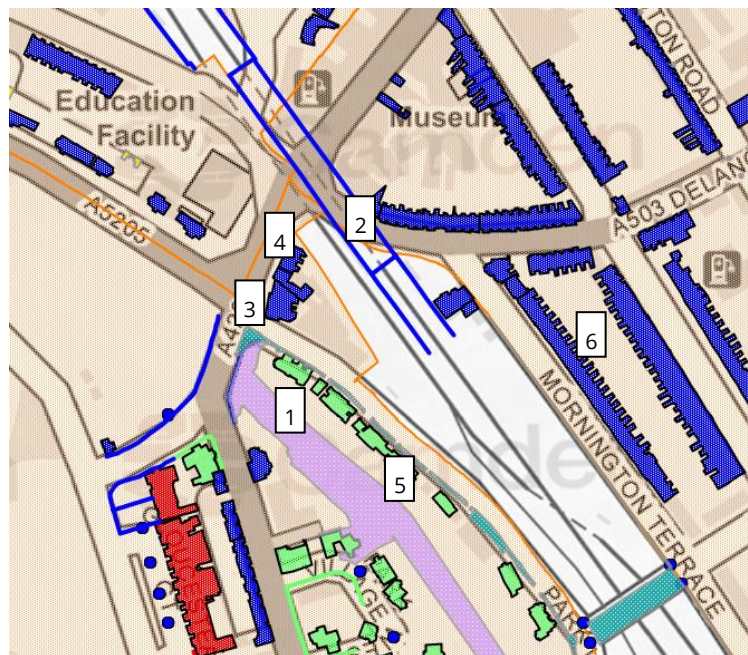


Figure 2 Designated heritage assets in proximity to Park Village East Berm

- 2.3.5 There are also several non-designated heritage assets near the site which contribute to the special character of the area. These comprise the locally listed structures associated with the expansion of the London to Midland Railway at the beginning of the 20th century. These include the parapet wall at street level which runs south from 1 Park Village East to Granby Terrace (adjacent to the west of the rail cutting); and the parapet wall at street level along Mornington Terrace and Clarkson Row (adjacent to the east of the rail cutting).
- 2.3.6 The cutting retains an element of its original character but has been altered constantly as it responds to the technological advancement of the railway, through the provision of gantries, Power Supply Points, Auto Transformer Stations, and the recent provision of the undercut of the West Coast mainline.
- 2.3.7 The retaining walls of the cutting are included as 'street features or other structures' on Camden's Local List (adopted on 21 January 2015), which details non-designated heritage assets within the Borough.

2.4 Surrounding Highway Network

- 2.4.1 The site is adjacent to Park Village East highway, a local road that connects northwards to Parkway (A4201). Further to the south, Park Village East forms onward connections to Hampstead Road (A400).
- 2.4.2 Park Village East also connects to Mornington Street Bridge, which crosses the railway cutting to Mornington Terrace. Mornington Terrace itself connects with Delancey Street (A503) to the north. It also forms onward connections to Hampstead Road (A400).

3 Description of the Works

3.1 Introduction

- 3.1.1 This Written Statement supports the Schedule 17 submission for the approval of plans and specifications for amendments to part of the Park Village East Wall Berm Support Structure.
- 3.1.2 The Plans and Specifications submitted for approval are listed in the proforma accompanying the application. A summary of the proposed works for approval is provided in **Section 3.3**, and a comparison with the originally approved scheme in **Section 3.4**.
- 3.1.3 Information on adjoining HS2 works is supplied in **Section 3.5**. This information is not for approval under Schedule 17.
- 3.1.4 **Sections 3.6 to 3.11** provide information on other aspects of the works to assist in understanding the context of the works being submitted for approval. The text contained within these sections is not for approval under Schedule 17.

3.2 Why has the proposal been amended?

- 3.2.1 The principle of the Berm wall including its function, purpose and location was established and approved through LPA application ref: 2021/0126/HS2.
- 3.2.2 The north section of the approved PVE Berm Wall (LBC Ref: 2021/016/HS2) has been amended for the following reasons:
- There has been a reduction in the bulk of the proposal because of a better understanding of the geo-technical attributes of the existing wall. This has led to a significant reduction in the requirement for composite materials and subsequently LGV movements and CO₂ emissions as part of the manufacturing process.
 - Concerns were raised as to whether there would be a consistent colour in the pigmentation process given the length of the proposed wall, as it is not possible to produce identical colouring given that this process relies on temperature, air humidity and the consistent colour and texture of the aggregates used.
 - The proposal replaces the approved rail-side wall (known as the L shaped wall) with a 2m wall. To ensure the proposal can enable safe pedestrian access of maintenance personnel, a handrail has been added on top of the rail-side wall.

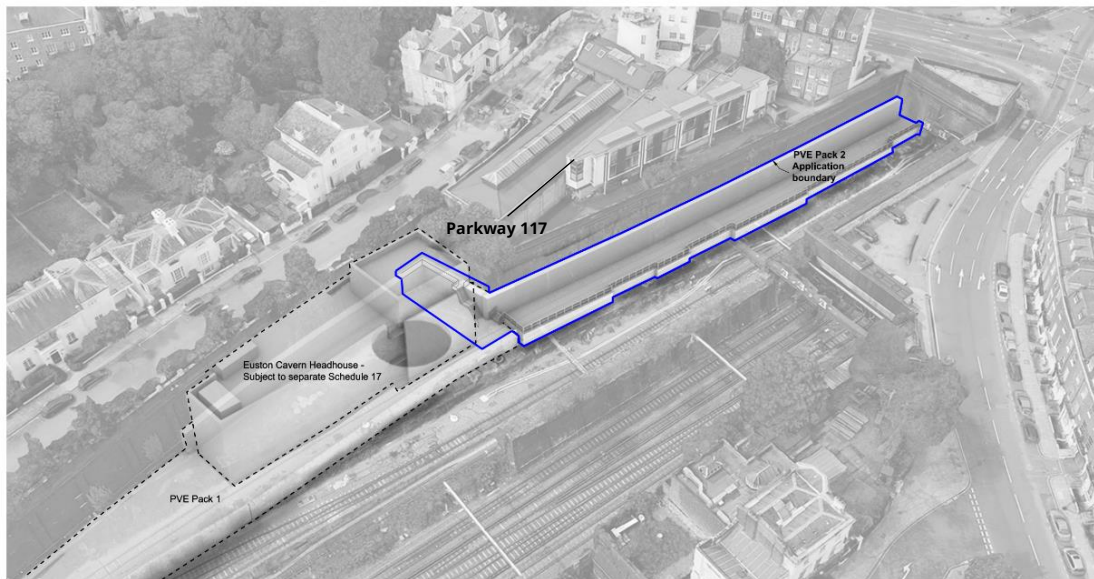


Figure 3 Scope of Pack 2 at the interface with PVE Pack 1 & Pack 3

3.3 Works for Approval

- 3.3.1 This is one of three Schedule 17 applications to amend the approved PVE Berm Wall scheme (LPA application ref: 2021/0126/HS2).
- 3.3.2 The Park Village East Wall Berm Support Structure is not a scheduled work under Schedule 1 of the HS2 Act. It will instead be carried out under the ancillary powers of Section 2 of the Act for the purposes of facilitating scheduled works under Schedule 1 of the Act. The proposed works are within limits.
- 3.3.3 The berm support structure is considered to constitute both a building work, as it performs an integral structural function as a retaining wall, and an earthwork, under Schedule 17 of the HS2 Act.
- 3.3.4 It forms the north section of the previously approved PVE Berm Wall (LPA application ref: 2021/0126/HS2), and so there are no changes to the location of the Wall – it is still located in the railway cutting. There are only minor changes to the design and appearance. It should also be noted that a recent Schedule 17 for the Berm Wall (central section) application ref: 2023/1268/HS2 which was approved by LBC on 8th September 2023 for the following:
- To change the proposed replacement parapet wall with a lower pre-cast L-shaped reinforced concrete wall panels;

- Removal of the proposed upstand element, and
- Change in colour to non-pigmented concrete.

3.3.5 The recently approved Schedule 17 proposals are similar to this current Schedule 17 proposal.

3.3.6 The north section of the Wall is located along the bottom of the existing PVE retaining wall, on the western side of the WCML cutting between the Parkway Tunnel in the north and Euston Cavern Shaft in the south. The existing retaining wall is of masonry construction and was built between 1896 and 1916. It is approximately 11m in height.

3.3.7 The north section of the Berm Wall will be around 3m in height with a typical width of 7.5m. It will therefore be lower in height and wider than the original approved Berm Wall, which was approx. 4m in height and approx. 6m in width.

3.3.8 A rail-side wall and handrail are positioned at the front of the berm structure.

3.3.9 An additional staircase and handrail are positioned adjacent to the berm structure to provide an alternative means of escape.

3.3.10 The General Arrangement, Elevation and Section, which accompany this application, show the dimensions of the berm structure in context.

3.3.11 As in the approved scheme, the purpose of the berm structure will be to improve the stability of the existing PVE wall in relation to potential bearing capacity, sliding and overturning failure mechanisms³.

3.3.12 Additionally, a detailed assessment of the works has been undertaken to confirm that any new works remain comparable to the ES (as amended). It has been considered overall that there are no new or different significant effects when compared to those reported in the ES (as amended).

³ **Sliding failure:** the wall could be pushed forward by the weight of the ground behind whilst remaining upright.

Overturning failure: the base of the wall stays where it is (no sliding) whilst the whole wall rotates. The top of the wall comes forward, pushed by the ground behind.

3.4 Comparison to approved scheme

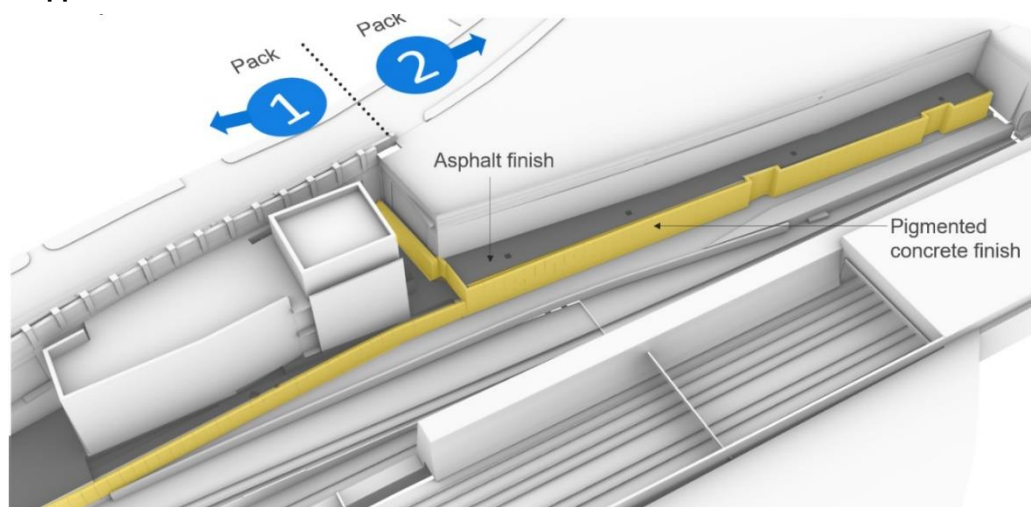
3.4.1 The Park Village East Berm & Upstand was granted approval, under Schedule 17, on 17 March 2021 (LPA ref: 2021/0126/HS2) by the London Borough of Camden.

3.4.2 The current submission seeks to amend the north section of this approved scheme. Amendments for the central and south sections of the approved scheme will be dealt with under separate Schedule 17 amendment applications.

3.4.3 The key amendments in the current submission (**Figures 4 - 6**) include:

- Substantial reduction in mass to a shallower, slimmer berm structure
- Addition of rear wall (known as upstand) against existing cutting wall
- Replacement of rail-side wall (known as L-shaped wall) to a 2m wall with handrail protection
- Addition of maintenance access stairs and associated handrail
- Change of finish to non-pigmented concrete.

Approved



Proposed

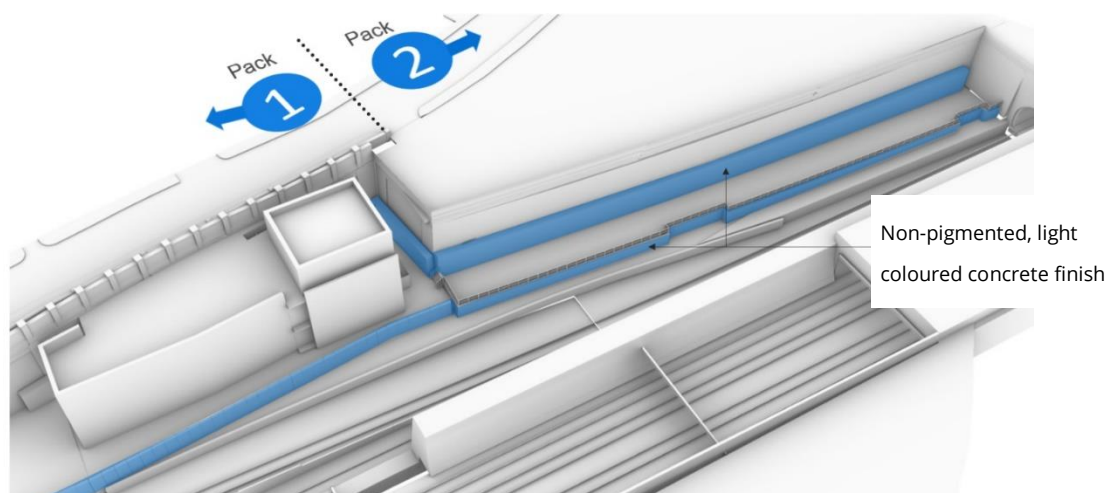


Figure 4 Comparison of finish between approved and amendment schemes.

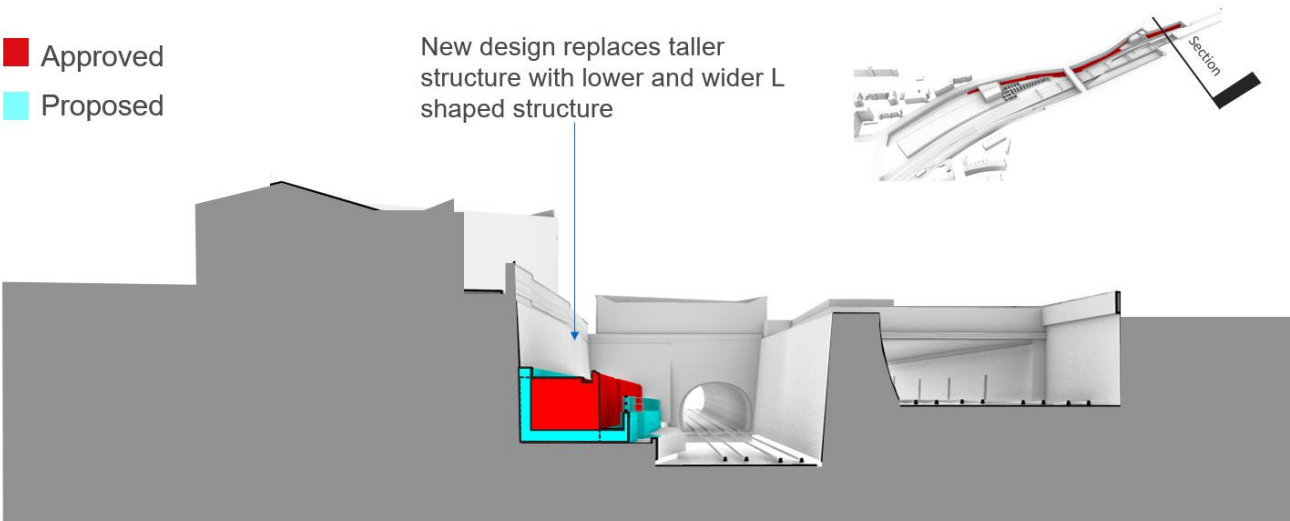


Figure 5 Comparison of approved and proposed schemes within wider context of cutting.

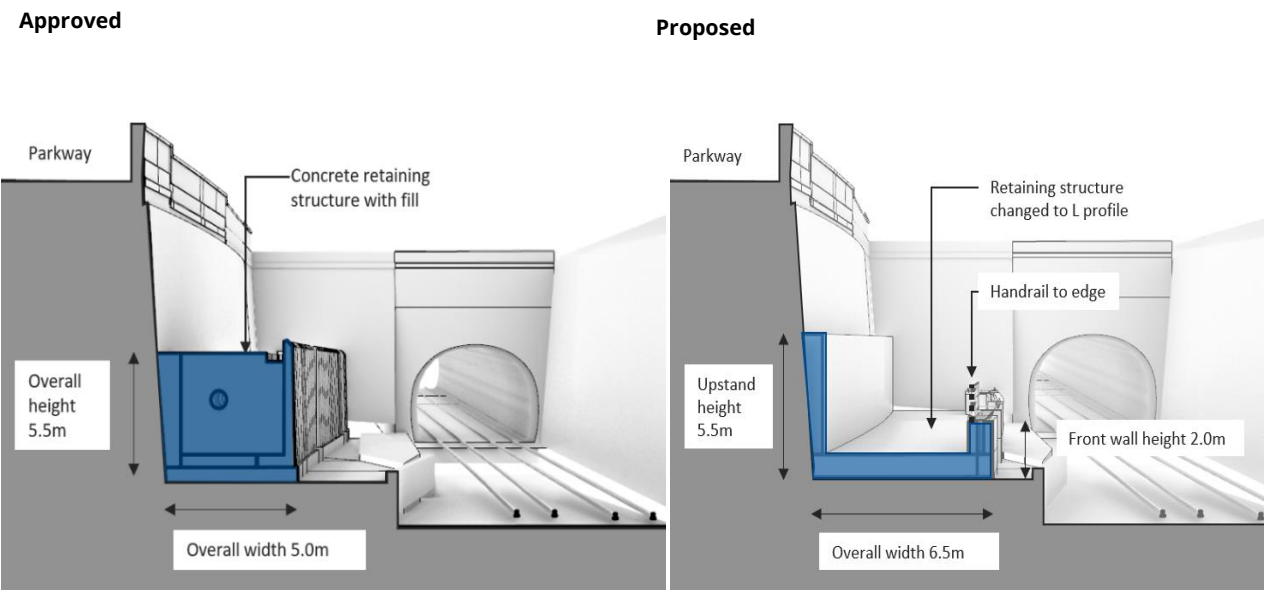


Figure 6 Comparison of sections views of approved and proposed schemes.

3.5 Other works

3.5.1 There are a range of HS2 works in the area surrounding the application boundary for this Schedule 17 application. **Figure 7** provides an overview of neighbouring HS2 works. This section will focus on those works immediately adjacent to the proposed scheme.

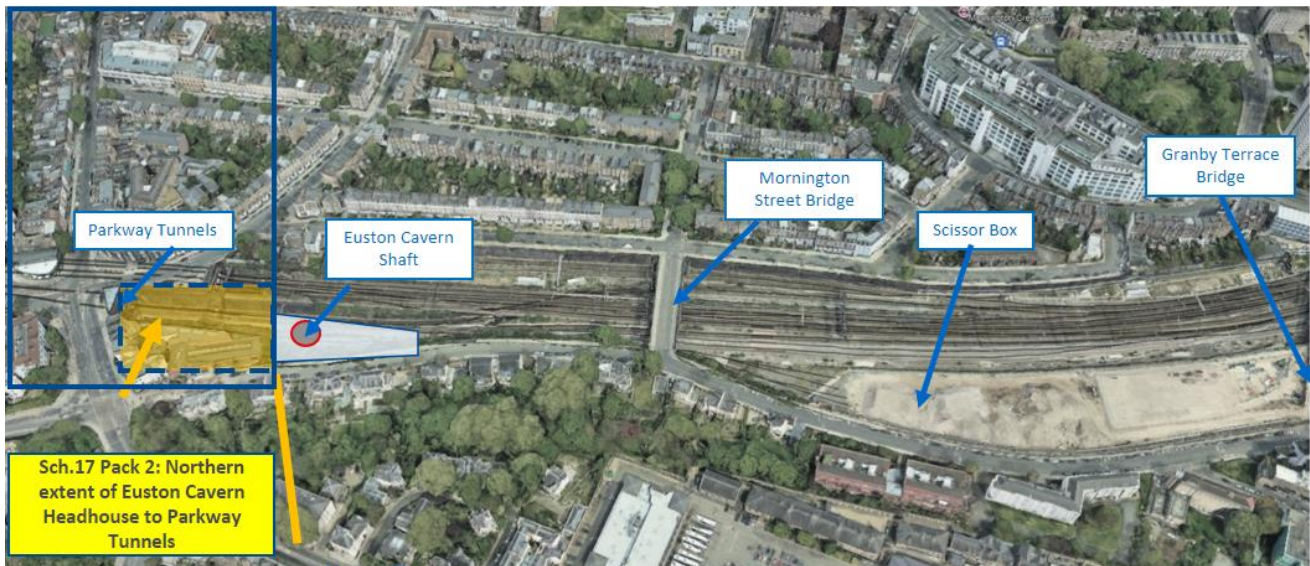


Figure 7 Satellite image locating some of the neighbouring HS2 works to the site

3.5.2 Euston Cavern Headhouse adjoins the south part of the proposed scheme – it comprises an escape and ventilation shaft (ascending from HS2 tunnels to Park Village East Street level) enclosed in a Headhouse building. The Schedule 17 application for this scheme has been submitted for determination by the London Borough of Camden.

3.5.3 The Euston Scissor Box lies further to the south of the proposed scheme and comprises an area of land which will be excavated for the HS2 Euston Tunnels. The section adjacent to the proposed scheme is not enclosed by a roof, so to allow warm air to escape from the HS2 railway below. It is supported by retaining walls and props. The Euston Scissor Box was approved as part of the approved scheme for the Park Village East Berm Support Structure (LPA ref: 2021/0126/HS2).

3.5.4 The Euston Portal & Headhouse also lies further to the south of the proposed scheme. The design for these elements is still in development. The Headhouse is situated on Park Village East and houses mechanical and electrical plant, a separate auto-transformer station that provides power to the railway, and space for maintenance vehicle parking.

3.6 Ecology

- 3.6.1 There is no additional loss of habitats or impacts on species as a result of the works for approval when compared to the impacts as assessed in the Environmental Statement (as amended).

3.7 Operational Noise

- 3.7.1 The design of the PVE Wall Berm Support Structure wall will not result in new or increased airborne noise impacts at receptors from those assessed within the Environmental Statement (as amended).

3.8 Indicative Mitigation

- 3.8.1 No specific indicative ecological or landscape mitigation is provided as part of this application. In line with paragraph 11 of HS2 Planning Forum Note 10, it is not considered reasonably necessary to provide mitigation for the proper consideration of the design proposed, given the proposal is a retaining structure and lies entirely out of sight from public views.

3.9 Construction Method

- 3.9.1 This section summarises the general construction methodology and the main temporary works arrangements. The arrangements described may alter, are for information and background only and do not form part of this request for approval.
- 3.9.2 The works subject to this request for approval of Plans and Specifications will be undertaken in accordance with the Local Environmental Management Plan for London Borough of Camden, HS2 Code of Construction Practice and the Class Approval issued by the Secretary of State (March 2017).

Access to construction site

- 3.9.3 The access to the entire PVE Wall Berm Support Structure will be predominantly via the main site access gate at Point 3 shown in **Figure 8**, through the site haul road towards the northern end of the Mornington Street Bridge. This will vary as the works progress, with access based on the construction methodology, outlined below. The Euston Approaches Worksite will benefit from four access / egress points including one located at the northern end of Park Village East, one on Granby Terrace at the junction of Stanhope Street / Park Village East, and two on Hampstead Road. These access points are set out in **Figure 8**.



Figure 8 Access and egress points from Worksite.

Construction Method

- 3.9.4 The following site set-up methodology (subject to change) is anticipated for the construction of the northern part of the PVE Wall Berm Support Structure:
- Site access and compound handover, with access to work area (in above Figure) using haul road through the live site including ramp down from Granby Terrace adjacent to Granby Terrace Bridge; and
 - Install hoarding at track level.

- 3.9.5 An indicative construction sequence is provided in section 6.

3.10 Historic Environment

Background

- 3.10.1 The HS2 Heritage Memorandum (part of the HS2 Environmental Minimum Requirements) explains that a route-wide generic written scheme of investigation: Historic Environment Research and Delivery Strategy (GWSI: HERDS) has been prepared in consultation with Historic England (HE) and all local planning authorities along the route. It sets out the research framework and general principles for design, evaluation, investigation, recording, analysis, reporting and archive deposition to be adopted for the design development and construction.
- 3.10.2 The arrangements for the management of archaeology during construction are not a matter for approval under Schedule 17. However, the preservation of a site of archaeological or historic interest is a key ground for refusal for the determination of plans and specification for the purposes of paragraph 3 of Schedule 17. Under the Schedule 17 Statutory Guidance (February 2017), grounds for refusal also include the preservation of the setting of designated heritage assets.
- 3.10.3 The HS2 Heritage Memorandum also sets out how the historic environment (including heritage assets and their setting) will be addressed during design. The HS2 Environmental Memorandum sets out the approach to landscape and visual mitigation which takes account of the historic environment.

Heritage assets adjoining and neighbouring the site

- 3.10.4 The proposal adjoins part of the non-designated tunnels and retaining wall, which form part of the second historical phase of railway construction in the early 20th century. The proposed works will have a direct physical impact on the retaining wall element.
- 3.10.5 The Environmental Statement (as amended) does not identify any impacts of the works on the setting for any of the assets detailed in section 2. Whilst the current scheme does differ from the originally approved scheme, the scale, height, and materials of the berm wall are in keeping with the approved design and would not result in any additional adverse impacts to the setting of assets detailed in section 2.
- 3.10.6 The Grade II listed Parkway Tunnel and Cutting further to the north-east, will not be affected by the works. **Figure 9** below pinpoints the extent of the listing and its physical separation from the application site, by virtue of the intervening railway lines.

- 3.10.7 The extent of the listing was confirmed with Historic England during a pre-application meeting on 9th March 2020 and is set out in a Heritage Agreement Method Statement (HAMS) for the Historic Building Recording of Parkway Tunnel and Cutting.

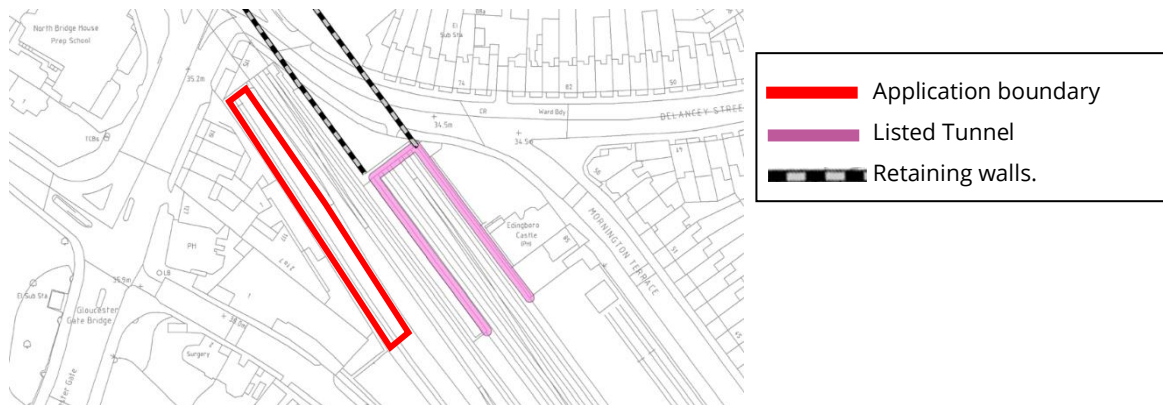


Figure 9 Extract of location plan showing the application site (identified in red) and the listed tunnel and retaining walls (identified in black/ grey stripes and pink respectively).

- 3.10.8 The construction of the amended berm wall, in terms of its attachment to the existing wall, is proposed to be the same as the approved scheme and therefore no new impact will result in the construction of the new wall.
- 3.10.9 Formal recording of the listed asset will be undertaken to mitigate the intervention of the new berm wall to ensure understanding and to provide a record of the structure prior to the works.
- 3.10.10 In addition to above, a level 2 historic building recording of the non-designated Railway Cutting Euston to Parkway was carried out by the Enabling Works Contractor⁴ and the Grade II listed Parkway Tunnel and cutting was recorded in accordance with a HAMS that was approved by Camden in December 2020 (LBC ref: 2020/4629/HS2)⁵.
- 3.10.11 Furthermore, the Environmental Statement (as amended) does not identify any impacts of the works on the setting for any of the nearby and adjacent heritage assets. In particular, the Environmental Statement does not identify the works as having a physical impact on the adjacent Grade II Parkway Tunnel and Cutting.
- 3.10.12 Following the submission of the amended application (LBC Ref: 2023/1268/HS2) for the central section of the proposed wall, concerns were raised to the proposed

⁴ Historic Building Recording of Railway Cutting Euston to Parkway - 1EW02-CSJ-EV-REP-S003-000128

⁵ Historic Building Recording of Parkway Tunnel and Cutting - 1EW02-CSJ-EV-REP-S003-000127

change in colour of berm support structure from 12% pigmented to non-pigmented concrete.

3.10.13 Planning Officers from LB Camden undertook a number of site visits, and did not consider that the views from the public realm to be significant. The wall along Mornington Terrace is considered to screen the views to the track level and therefore, any impact could not be considered as significant.

3.10.14 With regard to any potential adverse effects on the setting of adjacent listed buildings, notably the Grade II* listed Nash Villas on Park Village East and Park Village West, LB Camden officers considered the railway cutting has a nil or negligible contribution to their significance, as this is primarily derived from their picturesque setting and their historic value as a group.

3.10.15 With regard to the Grade II listed Parkway Tunnel and Cutting, the proposed amended PVE berm wall cannot be viewed as overly dominant when compared to the significant Grade II Listed Portal structure, even when considered creatively from a longer view, due to the mix of finishes, colours and infrastructure that occupies this view.

3.11 Environmental Management during Construction

3.11.1 The Environmental Memorandum, which forms part of the High-Speed Rail (London -West Midlands) Environmental Minimum Requirements, sets out the arrangements for the management of environmental issues during construction and the Code of Construction Practice (CoCP) sets out specific details and working practices that apply. The CoCP is supported by Local Environmental Management Plans (LEMPs) which include specific measures by topic, relevant to each relevant local authority area⁶.

3.11.2 Environmental management arrangements during construction do not form part of this request for approval of Plans and Specifications under Schedule 17.

⁶ The LEMP relevant to the works subject to this Schedule 17 submission is P1S Local Environmental Management Plan - London Borough of Camden and can be found here: <https://www.gov.uk/government/publications/local-environmental-management-plans-for-hs2-phase-one>

4 Design Criteria and Rationale

4.1 Design Requirements

- 4.1.1 The design approach for the proposal has been developed through working with key stakeholders including the London Borough of Camden. Details of engagement are provided in **Section 5**.

Functional Design Requirements

- 4.1.2 The proposal forms the north section of the approved PVE Berm Wall (LPA application ref: 2021/0126/HS2). There are therefore no changes to the location of the Berm Wall, only minor changes to its design and external appearance.
- 4.1.3 The rationale for the proposal is the same as the approved scheme. It will structurally support the existing Park Village East (PVE) retaining wall during HS2 construction works and operation of the HS2 railway.

4.2 Proposed Design



Figure 10 Top: Aerial view showing the existing western wall berm (Google Maps).

Figure 11 Bottom: Street view from Park Village East showing the limited views into the railway cutting (Google Maps)



To preserve the local environment or amenity

- 4.2.1 The amended berm comprises a structure of lower height than the original approved scheme. As such, similar to the approved scheme, it is substantially lower in height than the existing Park Village East Retaining Walls.
- 4.2.2 The berm will thus have negligible impact on the appearance of the local environment. In light of this, it is not considered that the change in finish of the berm (to a non-pigmented concrete) will affect the character or appearance of the local streetscape.

- 4.2.3 In terms of preserving amenity, as the berm is located at the base of the railway cutting, it will be sufficiently distanced so as not to affect access to light for neighbouring residential properties. The operational use of the berm will also mean that residential privacy would not be affected.

To prevent or reduce prejudicial effects on road safety or on the free flow of traffic in the local area

- 4.2.4 Once constructed, it only be used by vehicles periodically for maintenance visits and so the proposal will not result in prejudicial effects on road safety or to the free flow of traffic in the local area.

To preserve a site of archaeological or historic interest or nature conservation value

- 4.2.5 The railway cutting is an established historic feature in the area and the proposed PVE Berm Wall would sit amongst the many assets within the railway corridor which are well below street level.
- 4.2.6 Previously the extent and importance of setting of the proposals was expressed by reference to the visual relationship between the asset and the proposed development and associated visual/physical considerations of the listed villas located adjacent to Park Village East and the Parkway Tunnels.
- 4.2.7 In determining whether the proposed amended works constitute substantial harm to heritage assets, an important consideration is whether the adverse impact seriously affects a key element of their special architectural or historic interest rather than the scale of the proposed development. Whilst a physical or visual connection between a heritage asset and its setting may exist, this should not be considered essential or determinative in this instance.
- 4.2.8 Whilst it is acknowledged that the railway cutting does form a minor role in the setting of the listed buildings on Parkway (see sections 2.2 and 2.3), it clearly does not contribute to the significance of the listed buildings themselves.
- 4.2.9 Considering each of the surrounding heritage assets in turn:
- This terrace's heritage is listed in two parts, with 119-123 forming one listing and 125 the other. This grade II listed early 19th century stock brick terrace is built over 5 storeys. Number 125 is wider, at 3 bays wider than the other houses within the terrace. The ground floors have a rusticated stucco finish. There is a prominent stucco cornice between second and third floor levels as well as on top of the

parapet. This terrace's heritage significance is primarily derived from its historic value in terms of its age and its aesthetic value as a well-considered example of early 19th century architecture. It also has strong group value as a generally cohesive terrace in a good state of original repair. The close proximity to the railway cutting is certainly relevant in determining its urban setting. However, it is not considered that this has a strong impact on its significance. It is not considered that the proposal will have any impact, adverse or otherwise, on the heritage values or significance of these heritage assets. The proposed berm wall will not be visible from the asset due to its position, substantially lower than the current retaining wall.

- It is not considered that any of these values, or overall significance of either building would be affected by the proposed berm wall. This is primarily due to the proposal not being visible from these properties on account of it being substantially lower in height in comparison to the existing retaining wall. Furthermore, these buildings do not have a clear view of the railway cutting due to their location and screening modern infill development.
- No. 2 Park Village East, considered by the applicant's heritage consultant as a non-designated heritage asset. This red brick Victorian stable building was built as a riding school. Its age and high architectural quality merit it as a non-designated heritage asset. The proposed berm wall would not be visible from this property and therefore it is not considered to have any effect on its heritage significance.

4.2.10 The amended proposal illustrates that the changes will lead to less than substantial harm to the significance of a designated heritage asset, this harm should also be weighed against the public benefits of the proposal. Public benefits from the HS2 scheme that delivers economic, social, or environmental objectives and be of a nature or scale to be of benefit to the public at large and not just be a private benefit are considered significant as capacity and connectivity will improve and the sustainable, climate resilient transport infrastructure will help to cut carbon emissions from the transport sector.

4.2.11 In light of this assessment and the comprehensive pre-application discussions that were undertaken with officers at LB Camden, it is considered that the amended proposal does not cause harm to the historical significance or setting of the PVE villas or Parkway Tunnels.

That the development ought to, and could reasonably, be carried out elsewhere within the development's permitted limits

- 4.2.16 The proposal serves the functional requirements detailed in **Section 4.1** It therefore cannot be positioned in an alternative location within the HS2's permitted Limits of Deviation.

5 Pre-submission Consultation

5.1 Pre-submission consultation with the Local Planning Authority, statutory consultees and other relevant stakeholders is summarised in **Table 5** below.

Table 4: Pre-submission Consultation with LPA and Statutory Consultees

| Consultee Name | Consultation Date | Method of Consultation / Attended by | Summary of Consultation Outcome |
|--------------------------|---------------------------------|--------------------------------------|---|
| London Borough of Camden | 14 th June 2022 | Pre-application meeting | An overview of proposed design changes in PVE amendment applications. |
| | 20 th September 2022 | Pre-application meeting | An overview of proposed PVE design changes within Pack 2. |
| | 21 st February 2023 | Pre-application meeting | Page turn review of draft submission documents and drawings. |

6 Construction Programme

6.1 A high-level programme for the works subject to this submission and how they fit into the overall programme for other works in the area is contained in **Table 6** below. The programme for works on site may vary from the indicative dates shown.

Table 5: Proposed Programme and Sequence of Works

| Anticipated Start on Site Date (quarter/year) | Activity | Estimated Completion of Works (quarter/year) |
|---|--|--|
| April 2023 | Installation of below ground foundations (known as piling works) | August 2023 |
| April 2023 | Installation of above ground foundations (namely, a slab) | October 2024 |
| September 2025 | Structural remediation works to stretcher beam structure | December 2025 |
| May 2027 | Addition of concrete elements of berm | July 2027 |

7 Other Consents

- 7.1 Other main consents likely to be required for the works are summarised in **Table 7** below. Consent requirements may alter during design development and further consents not identified may be required.

Table 6: Other Consent Requirements

| Consent | Works Requiring Consent | To be submitted / approved |
|--|--|--|
| HS2 Act, Schedule 33, Part 1 | Ground anchors (installed from railway cutting, extending under Park Village East highway) | Approved by LBC on 14 April 2021 |
| Any other relevant Schedule 17 Plans and Specifications submissions for adjacent or associated works | Plans and Specifications – Park Village East Berm Wall and Euston Scissor Cut | Approved by LBC on 21 March 2021 (LPA ref: 2021/0126/HS2) |
| | Plans and Specifications – Euston Cavern Shaft and Headhouse | Approved by LBC on 18 October 2022 (LPA ref: 2022/3352/HS2). |
| | Plans and Specifications - Euston Portal Shaft and Headhouse | To be submitted. |
| | Plans and Specifications – Berm Wall Pack 1 | Approved on 07 Sept 2023, (LPA ref: 2023/1268/HS2). |