

HIGHGATE NEWTON COMMUNITY PARTNERS PARKING MANAGEMENT PLAN



SYSTRA

HIGHGATE NEWTON COMMUNITY PARTNERS

PARKING MANAGEMENT PLAN

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TABLE OF CONTENTS

1.	INTRODUCTION	5
1.1	GENERAL	5
1.2	REPORT STRUCTURE	6
2.	BASELINE CONDITIONS	7
2.1	GENERAL	7
2.2	SITE LOCATION	7
2.3	CYCLE FACILITIES	8
2.4	LOCAL HIGHWAY NETWORK	8
2.5	PARKING	8
3.	CONSENTED DEVELOPMENT	12
3.1	GENERAL	12
3.2	ACCESS	12
3.3	CAR PARKING	13
3.4	CYCLE PARKING	13
3.5	SERVICING & REFUSE COLLECTION	14
4.	PARKING MANAGEMENT	15
4.1	MANAGEMENT OBJECTIVES	15
4.2	PARKING PROVISION & ALLOCATION	15
4.3	ACCESS MANAGEMENT	15
4.4	SERVICING MANAGEMENT	16
5.	SUMMARY & CONCLUSION	17
5.2	CONCLUSION	17

LIST OF FIGURES

Figure 1. Site Location Plan 7
Figure 2. CPZ CA-U 9
Figure 3. Bertram Street 9
Figure 4. Bertram Street Disabled Parking 10
Figure 5. Dedicated Disabled Parking, Croftdown Road 10
Figure 6. Cycle Store Location 14

LIST OF TABLES

Table 1. Cycle Parking Provision 13

1. INTRODUCTION

1.1 General

1.1.1 SYSTRA Ltd (SYSTRA) has been commissioned by the London Borough of Camden (the Client) to provide transport and highways consultancy in support of the development proposals at Highgate Newton Community Partners, at 25 Bertram Street, London, N19 5DQ (the Site). The Local Planning Authority and Local Highway Authority is the London Borough of Camden (LBC).

1.1.2 Planning permission was granted (ref: 2016/6088/P) in July 2017 for the redevelopment of the existing Highgate Newtown Community Partners (HNCP) and Fresh Youth Academy (FYA) and the change of use of the People’s Mission Gospel Hall to provide replacement community facilities and 31 residential units with associated public open space, landscaping, cycle storage, plant and disabled parking.

1.1.3 Planning permission was subsequently granted in March 2019 (ref: 2018/5774/P) for a revised scheme whereby the principles of development remained the re-provision of community facilities and new residential units, but incorporating standalone residential and commercial buildings and an increase in residential units to 41 from 31 (the Consented Development). The Consented Development includes the provision of cycle parking facilities and refuse storage.

1.1.4 Planning Condition 35 of the permission states that:

Prior to occupation of any part of the development, a parking management plan shall be submitted to and approved by the local planning authority. The proposals will be maintained in accordance with this plan thereafter.

Reason: In order to protect the pedestrian environment and the amenities of the area generally and to ensure the continued free flow of traffic in the area in accordance with Camden Local Plan policies T1, T2 and T3.

1.1.5 Planning permission was subsequently granted in August 2023 (ref: 2023/2662/P) to secure minor modifications to the Consented Development, whereby the principles of development remained the re-provision of community facilities and new residential units, but would facilitate a tenure change to 100% affordable housing along with a change to layout of selected units to increase their capacity and size which would result in an overall reduction in unit numbers from 41 to 36. Access arrangements, car parking and cycle parking provision remain as per the Consented Development.

1.1.6 This Parking Management Plan has therefore been prepared to discharge Planning Condition 35. The Parking Management Plan (PMP):

- Summarises car and cycle parking provision for both land uses of the Consented Development;
- Demonstrates how parking demand associated with the Consented Development will be managed;
- Provide details of vehicular access arrangements; and
- Describe the management and enforcement strategies related to parking and vehicular activity to be implemented to ensure the Consented Development operates correctly and effectively at all times.

1.2 Report Structure

1.2.1 Following this introductory section, the remainder of this PMP is structured as follows:

- **Section 2: Baseline Conditions** – Describes the existing highways and transport conditions in the area surrounding the Site, with a focus on the local highway network and parking restrictions.
- **Section 3: Development Proposals & Parking Provision** – Summarises the Consented Development in terms of land uses, residential units, access and both car and cycle parking provision.
- **Section 4: Parking Management** – Sets out the strategy and management measures that will be implemented for parking activity associated with the Consented Development.
- **Section 5: Summary & Conclusion** – Summarises the key points arising from the work carried out to inform this PMP.

2. BASELINE CONDITIONS

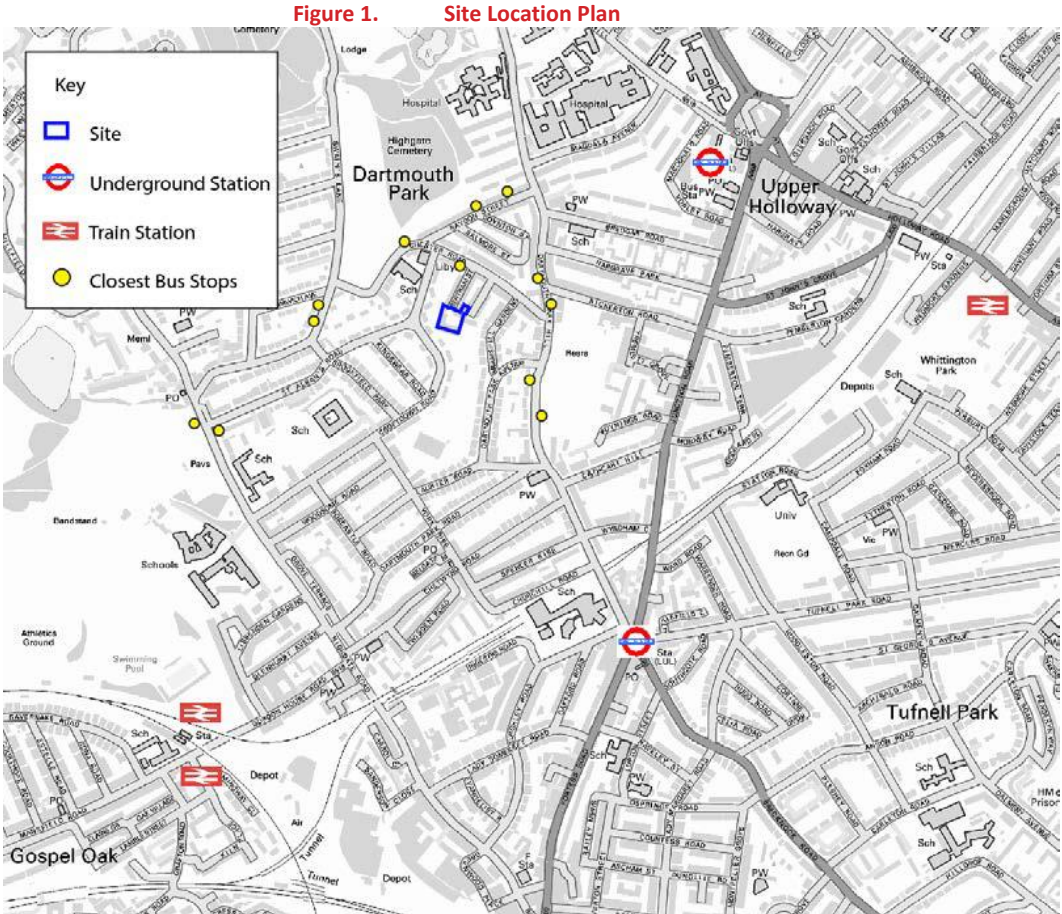
2.1 General

2.1.1 This section describes the existing transport and highways conditions at the Site and in the immediate surrounding area. Particular focus is placed on parking provision, the highway network and cycle infrastructure.

2.2 Site Location

2.2.1 The Site is located at the southern end of Bertram Street, a residential cul-de-sac in the London Borough of Camden. It is bound by Bertram Street to the north, properties fronting Croftdown Road to the west and south, and properties fronting Bramshill Gardens to the east.

2.2.1 A plan detailing the location of the Site in the context of the surrounding area is shown in **Figure 1**.



2.3 Cycle Facilities

2.3.1 The Site is located in close proximity to a number of segregated cycle routes, as well as quieter roads recommended for use by cyclists. To the west of the Site, Highway West Hill and Highgate Road are signed or marked for use by cyclists, and there are off-road routes through Hampstead Heath. Chester Road, Raydon Street and Dartmouth Street are labelled as quieter roads that are recommended for use by cyclists.

2.4 Local Highway Network

Bertram Street

2.4.1 Bertram Street is 7.0m wide cobbled residential cul-de-sac which runs north-south and connects the Site to Chester Road. Parking bays are marked on both sides of the carriageway. Footway build-outs are provided at the northern end of and half way down Bertram Street, which narrow the carriageway width to approximately 3.2m.

2.4.2 At present, there is no turning head provided on Bertram Street, meaning vehicles have to turn in locations where there are empty parking spaces, or travel in reverse gear along the entire length of the road. The latter practice is currently undertaken by refuse vehicles, which reverse back onto Chester Road. It is noted that traffic volumes on Bertram Street are low; however, such manoeuvres could be considered a highway safety hazard.

Chester Road

2.4.3 Chester Road is located to the north of the Site and is adopted by LBC as the Local Highway Authority. It runs east-west between Swan’s Lane and Dartmouth Park Hill. Residential parking and wide footways are provided both sides of the road. Vehicles are permitted to travel in a westbound direction only on the section of Chester Road between Dartmouth Park Hill and Raydon Street.

2.4.4 There are a series of footway build-outs and associated speed cushions provided on Chester Road, aimed to reduce vehicle speeds and provide informal crossing points for pedestrians.

Croftdown Road

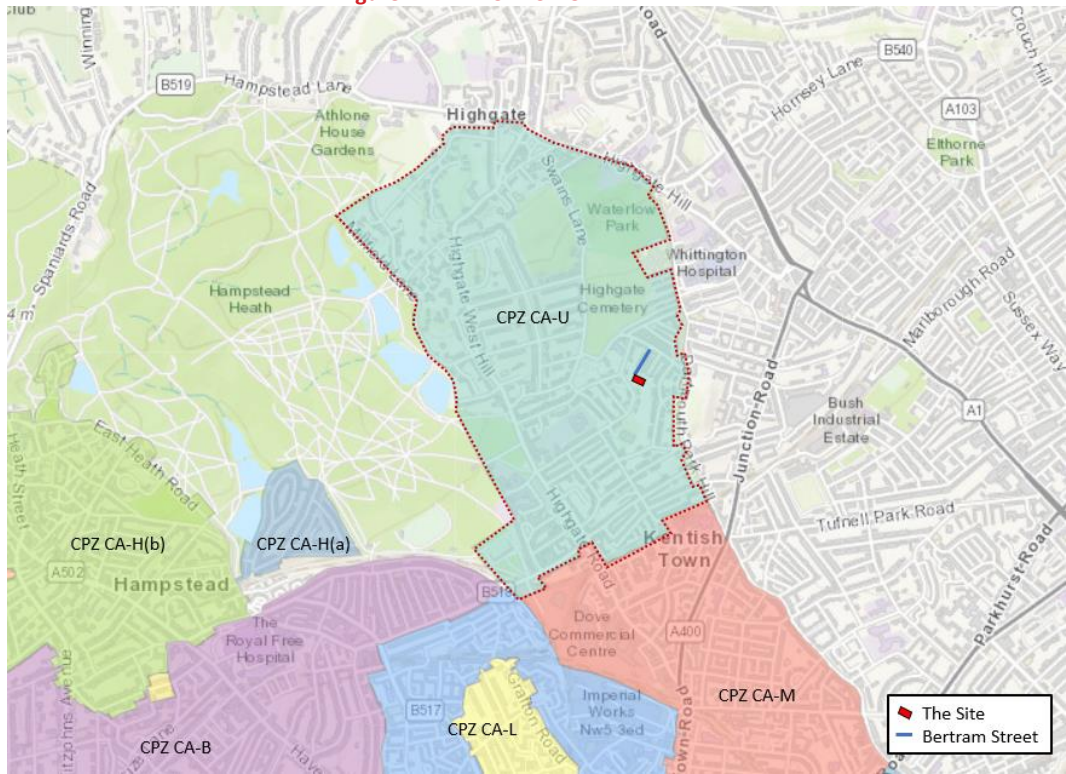
2.4.5 Situated to the south of the Site, Croftdown Road runs north-east to south-west between Chester Road and Highgate Road and facilitates two-way traffic for its duration. It is also adopted by LBC as the Local Highway Authority. Speed humps are installed at frequent intervals to reduce vehicle speeds.

2.4.6 No stopping and loading is permitted on Croftdown Road adjacent to the pedestrian route that links to the Site between 07:30 and 18:00, Monday to Saturday.

2.5 Parking

2.5.1 Streets in the vicinity of the Site (including Bertram Street and Croftdown Road) are located in Controlled Parking Zone (CPZ) CA-U, the spatial extent of which is shown in **Figure 2** overleaf. Parking restrictions are operational between the hours of 10:00 and 12:00, Monday to Friday.

Figure 2. CPZ CA-U



Adapted from LBC CPZ Map (2022)

- 2.5.2 The majority of the parking bays on Chester Road, Croftdown Road and at the northern end of Bertram Street (Figure 3) are available for use by permit holders only during CPZ operational hours, and for general public use without charge at other times.
- 2.5.3 The bays at the southern end of Bertram Street and a limited number on Chester Road are shared use (for permit holders or Pay & Display) during CPZ hours, with a maximum stay of 1.5 hours permitted. Outside of CPZ hours, bays can be used without charge or stay duration restriction.

Figure 3. Bertram Street



- 2.5.4 There are a number of on-street disabled parking bays in the area, including two on Bertram Street (one adjacent to the Site entrance and one approximately mid-way down

its length, shown in **Figure 4**), and two on Croftdown Road, close to the pedestrian footpath. Whilst located within a short walk distance of the Site, the bays on Croftdown Road are designated to a specific permit holder, as shown in **Figure 5**.

Figure 4. Bertram Street Disabled Parking



Figure 5. Dedicated Disabled Parking, Croftdown Road



Car Club

2.5.5

Car clubs are short-term car rental services that allow members access to locally parked cars and pay by the minute, hour or day. The closest car club spaces to the Site are located on:

- Bickerton Road (Zipcar), located approximately 350m to the east of the Site;
- Dartmouth Park Hill / Highgate Hill (Zipcar), located approximately 950m to the north of the Site;
- Darmouth Park Road (Zipcar), located approximately 600m to the south of the Site;
- Ospringe Road (Enterprise), located approximately 1.35km to the southeast of the Site;
- Pemberton Gardens (Zipcar), located approximately 600m to the east of the Site;
- Sherlock Road (Enterprise), located approximately 1.75km to the south of the Site;
- St Alban's Road (Zipcar), located approximately 600m to the west of the Site; and

- St John's Grove (Zipcar), located approximately 1km to the east of the Site.

3. CONSENTED DEVELOPMENT

3.1 General

- 3.1.1 This section provides an overview of the Consented Development, including land uses and unit numbers, parking provision and access details.
- 3.1.2 The Consented Development to which the planning condition is associated incorporates the demolition of the building previously located on the Site and construction of a new building to provide a new home for the community centre uses previously located on-site through the Highgate Newton Community Partners (1,892 sqm), and three buildings accommodating residential units.
- 3.1.3 This will be supported by associated public open space, landscaping, cycle parking, plant and refuse storage.
- 3.1.4 Planning permission was subsequently granted in August 2023 to secure minor modifications to the Consented Development, whereby the principles of development remained the re-provision of community facilities and new residential units, but would facilitate a tenure change to 100% affordable housing along with a change to layout of selected units to increase their capacity and size which would result in an overall reduction in unit numbers from 41 to 36.
- 3.1.5 A total of 36 units will be provided across three buildings, comprising 1x studio, 19x one-bed, 8x two-bed, 3x three-bed, 3x four-bed and 2x six-bed.
- 3.1.6 Access arrangements, car parking and cycle parking provision remain as per the Consented Development.
- 3.1.7 The planning applications allows for the community use to be operational between the hours of:
 - 08:30 and 22:00, Monday to Thursday;
 - 08:30 and 23:30, Friday and Saturday; and
 - 09:30 and 21:00, Sunday.
- 3.1.8 HNCP has confirmed that the community uses, incorporating the ground floor, first floor and hall, are expected to accommodate a peak capacity of 300 people. LBC’s Youth Service will occupy other floors of the building.

3.2 Access

- 3.2.1 The Site’s internal courtyard has been designed as primarily a pedestrian space to deter the use of this space by vehicles. To ensure that the courtyard operates as intended, it is intended to restrict access to permitted vehicles only through the installation of a key-operated telescopic rising bollard.
- 3.2.2 Any unscheduled access in the week will be managed by HNCP and FYA staff during their operating hours. Deliveries will be undertaken from a new loading bay to be provided on Croftdown Road (see **Section 5**).
- 3.2.3 This arrangement will be reviewed by LBC (as freeholder) at regular intervals and modified if required.

3.3 Car Parking

3.3.1 The residential element of the development is proposed to be car-free, in line with Camden policy. Given the Site’s PTAL level is towards the upper end of 3, it is expected that that residents will be able to travel sustainably, without the need to travel by car. By not providing car parking spaces for the residents, and restricting residents from applying for on-street parking permits, the impact of the new residential units on the surrounding highway network will be minimised.

3.3.2 As part of the planning application for the Consented Development, it was agreed that if any residential units were to be adapted for disabled residents, allocated disabled car parking spaces could be provided on-street, similar to those located on Croftdown Road. This is in line with CPG 7, which states that:

‘Where a resident in need of a reserved disabled parking space moves into a development with no off-street spaces, the Council will consider a request for a designated disabled space on-street in the same way whether the development is formally car-free or not’.

3.3.3 It is noted that any disabled users travelling by car to the community centre use will be able to be dropped off or picked up on Croftdown Road or Bertram Street, within n accessible walk distance of the community centre uses.

3.4 Cycle Parking

3.4.1 An overview of cycle parking to be provided as part of the Consented Development is provided in [Table 1](#).

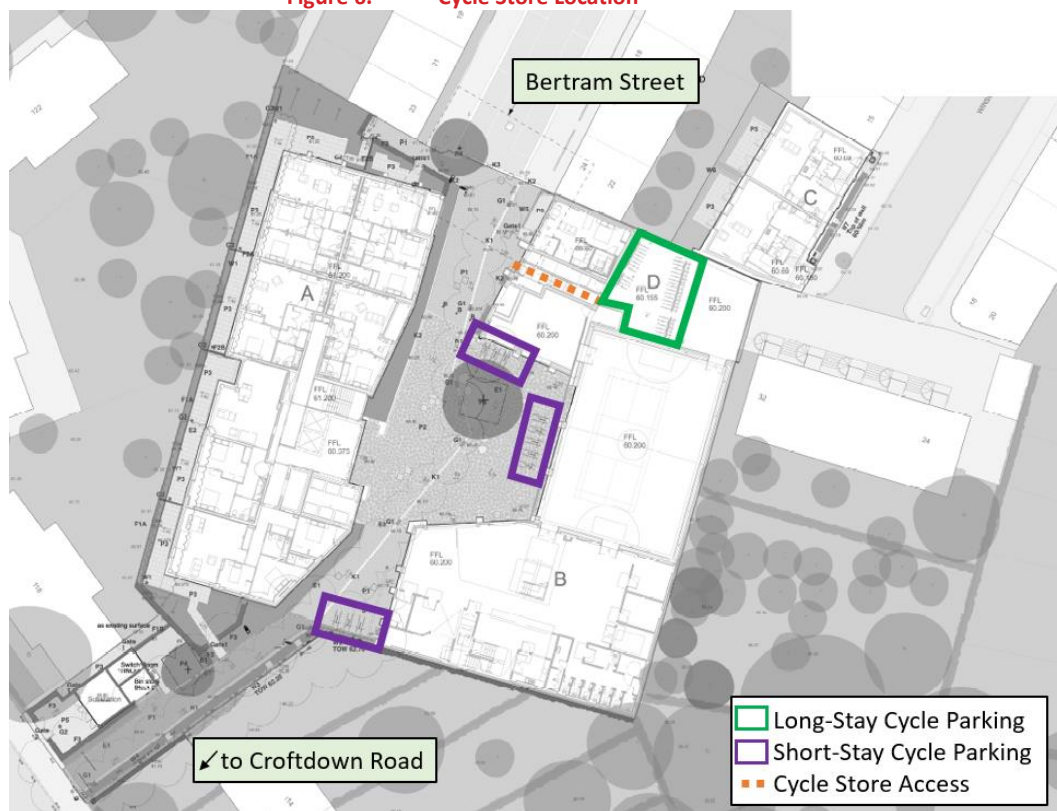
Table 1. Cycle Parking Provision

UNIT TYPE	SHORT-STAY	LONG-STAY
Residential	2	58
Community Centre	20	0
Total	22	58

3.4.2 Residential cycle parking accords with minimum standards stipulated by the London Plan adopted at the time of the planning application, requiring one long-stay space per studio and one-bed unit, and two long-stay spaces for all other dwellings, alongside one short-stay space per 40 units.

3.4.3 Residential long-stay cycle parking will be located within a consolidated sheltered and secure cycle store located at ground floor adjacent to Block D, as highlighted in [Figure 6](#). The 22 short-stay cycle parking spaces will be provided in the form of eleven Sheffield stands in two locations within the internal courtyard.

Figure 6. Cycle Store Location



3.5 Servicing & Refuse Collection

3.5.1 The majority of delivery and servicing activity associated with the Consented Development will be undertaken on-street, from a new loading bay to be provided on Croftdown Road. It is currently proposed that the following servicing and delivery trips will be undertaken from this loading bay:

- Residential deliveries;
- Community use deliveries; and
- HNCP minibus drop-off.

3.5.2 It is proposed for a loading bay to be provided on Croftdown Road, in front of the crossover provided to enable construction works for the Consented Development. Single yellow line restrictions are currently in place in this location.

3.5.3 Refuse collection activity will be undertaken from within the courtyard, with vehicles accessing via Bertram Street. Refuse vehicles will enter and exit the Site in forward gear. Emergency vehicle access will also be provided to the courtyard.

3.5.4 Servicing activity associated with the two freehold houses located in Block C will be undertaken on-street from Winscombe Street.

3.5.5 Compared to the previous arrangement at the Site, whereby all servicing and refuse collection vehicles were required to reverse the length of Bertram Street on leaving the Site, the proposed arrangements that include the possibility for vehicles to turn on-site represents an improvement to road safety along both Bertram Street and Chester Road.

4. PARKING MANAGEMENT

4.1 Management Objectives

- 4.1.1 Without a strong PMP, a number of issues relating to parking can arise, including parking in inappropriate locations such as within the courtyard, on footways and grass verges, compromised access for emergency vehicles and severance to pedestrian and cyclist movements.
- 4.1.2 The aim of this PMP is therefore to direct the safe operation of parking activity that does not result in significant negative impacts on the local highway network or pedestrian and cyclist safety. This will be achieved through the following key objectives:
- Ensure that the parking is used efficiently, in accordance with the detailed allocation and within capacity; and
 - Provide access for all appropriate users, whilst preventing unauthorised vehicle access through a series of management measures.

4.2 Parking Provision & Allocation

- 4.2.1 As previously detailed, the residential element of the Consented Development is car-free, with residents restricted from applying for or owning on-street parking permits. It is expected that that residents will be able to travel sustainably via public transport, on foot or by cycle, without the need to travel by car.
- 4.2.2 By not providing on-site car parking spaces for residents and restricting residents from being eligible for on-street parking permits, given the Site and surrounding streets lie within a CPZ, the impact of the new residential units on the surrounding highway network will be minimised.
- 4.2.3 The community elements of the Site will be car-free, with disabled visitors / staff able to park on-street including on Croftdown Road and Bertram Street, with parking opportunities located within a short step-free walk distance of the main access to the community centre uses.
- 4.2.4 In Camden, blue badge holders are able to park in blue badge bays, resident permit bays, shared use bays and paid-for parking bays without charge. Additionally, holders can park for up to three hours on single and double yellow lines where there is no loading ban, when displaying the arrival time on the blue badge clock. Disabled motorists can therefore make use of a range of parking opportunities in the vicinity of the Site.
- 4.2.5 Cycle parking will be provided through a mix of two-tier Josta and Sheffield stands. Residential cycle parking is provided at a ratio of one long-stay space per studio and one-bed unit, and two long-stay spaces for all other dwellings. A total of 22 short-stay cycle parking spaces will be provided in the form of eleven Sheffield stands in two locations within the internal courtyard, for use by community centre and residential visitors.

4.3 Access Management

- 4.3.1 As detailed in **Section 3**, vehicular access to the Site’s internal courtyard will be limited to minimise the extent of vehicular activity accommodated and ensure the space operates as intended as primarily a pedestrian area.

4.3.2 Rising bollards will be installed at the Bertram Street entrance to prevent unauthorised vehicular access.

4.4 Servicing Management

4.4.1 Deliveries will be scheduled to occur outside of peak community centre periods as far as possible. All deliveries to the community uses will be controlled by a delivery booking system to ensure that deliveries are equally distributed across the week and across delivery hours. The majority of deliveries are anticipated to be made by smaller vehicles. Servicing vehicles will not be permitted to park or stop on-site.

4.4.2 The booking system will ensure that no more than one delivery takes place at a time. Unless there is capacity to accommodate within the loading bay on Croftdown Road, unplanned deliveries or those arriving outside of their designated time-slot will not be accepted, and will be advised to return at a pre-arranged time. Suppliers will be informed of the booking system prior to the commencement of any contract and will be given details of a contact with whom deliveries should be scheduled.

4.4.3 A standalone Servicing Management Plan has been prepared for the Consented Development and sets out the ways in which the efficiency, safety and reliability of deliveries and servicing activity at the Consented Development will be maintained.

5. SUMMARY & CONCLUSION

- 5.1.1 SYSTRA has been commissioned by the London Borough of Camden (the Client) to provide transport and highways consultancy in support of the development proposals at Highgate Newton Community Centre, at 25 Bertram Street, London, N19 5DQ. The Local Planning Authority and Local Highway Authority is the London Borough of Camden.
- 5.1.2 Planning permission has been granted for the redevelopment of the existing Highgate Newtown Community Partners and Fresh Youth Academy and the change of use of the People’s Mission Gospel Hall to provide replacement community facilities and construction of 41 residential units with associated public open space, landscaping, cycle parking, plant and refuse storage.
- 5.1.3 Planning permission was subsequently granted in August 2023 to secure minor modifications to the Consented Development, whereby the principles of development remained the reprovision of community facilities and new residential units, but would facilitate a tenure change to 100% affordable housing along with a change to layout of selected units to increase their capacity and size which would result in an overall reduction in unit numbers from 41 to 36.
- 5.1.4 Access arrangements, car parking and cycle parking provision remain as per the Consented Development.
- 5.1.5 The measures set out in this PMP are intended to inform LBC of the ways in which the parking demand will be managed following occupation of the Consented Development. No parking will be provided on-site, with residents also restricted from applying for resident parking permits.
- 5.1.6 The Applicant will liaise with LBC, as the Local Planning Authority, should circumstances arise under which amendments will be required to this PMP. The PMP will be complied with unless otherwise agreed in writing by LBC.

5.2 Conclusion

- 5.2.1 It is considered that the approval requirements of Planning Condition 35 have been fully satisfied within this PMP and the Condition should therefore be discharged.

SYSTRA