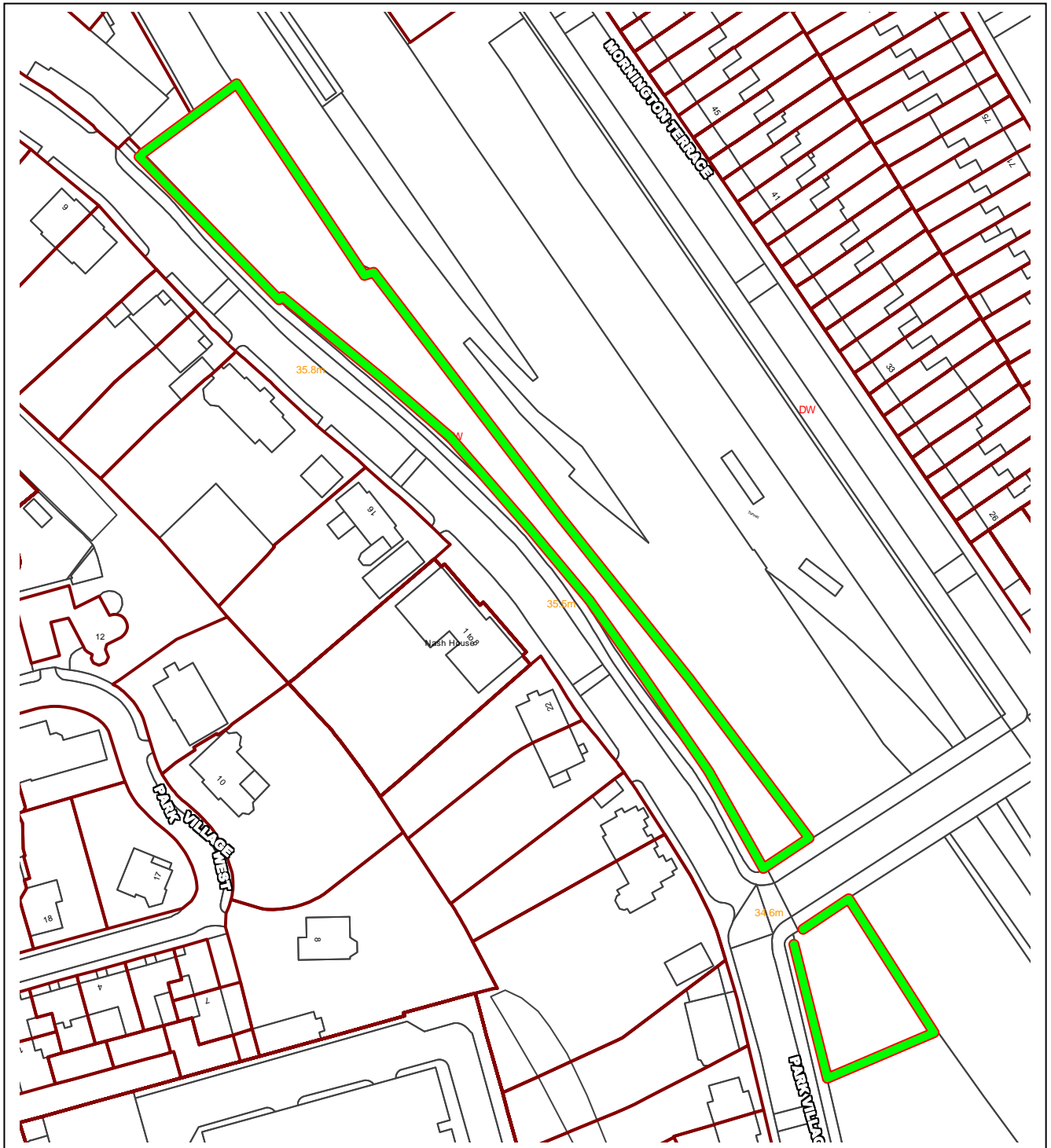


Railway cutting adjacent to the existing Park Village East retaining wall - 2023/1268/HS2



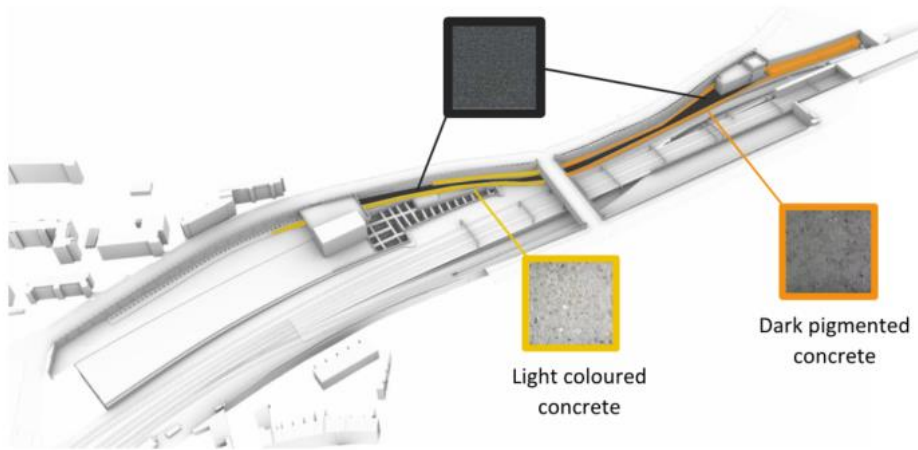
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Figure 1: Aerial view of Euston and surrounding area. Approximate site location demarcated in red (Map data Google 2019)

Approved

Asphalt to top



Proposed

Top surface
changed to light
concrete

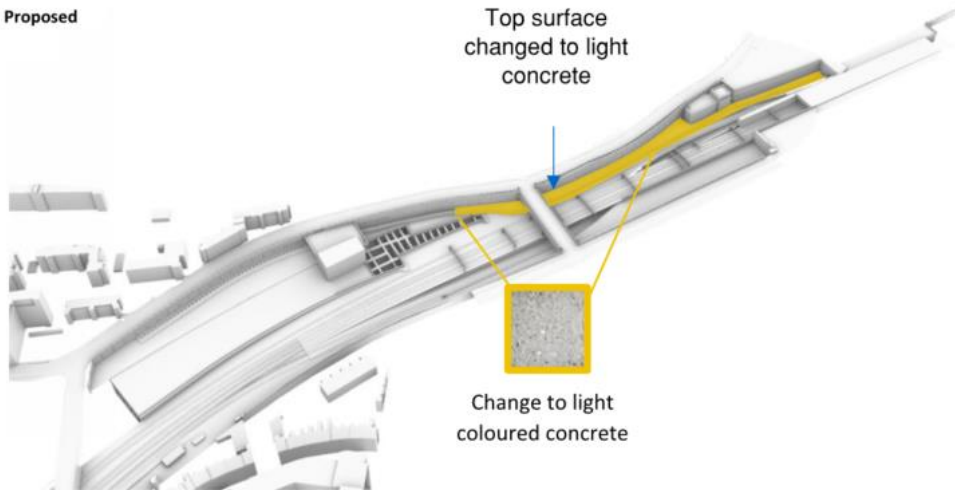


Figure 2: Comparison of materiality between approved and amendment schemes.

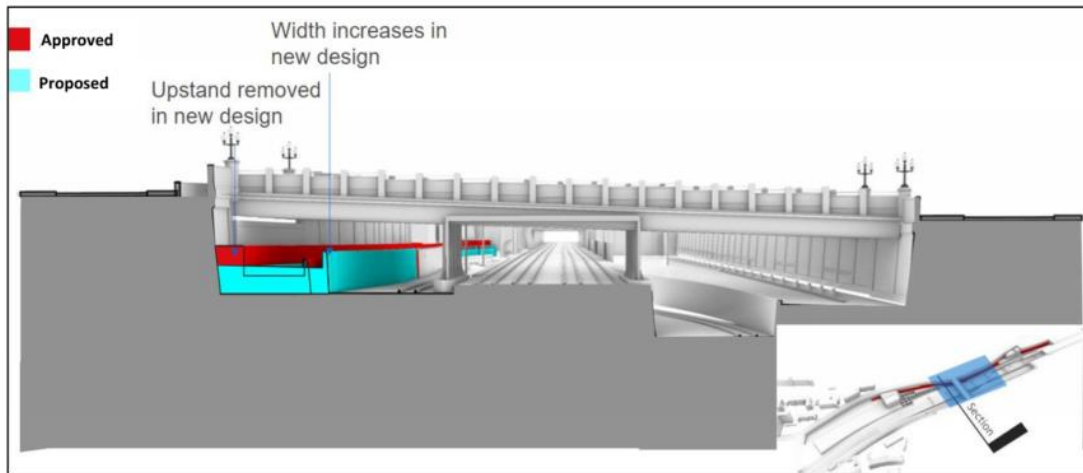


Figure 3: Comparison between approved and amendment schemes showing removal of upstand element

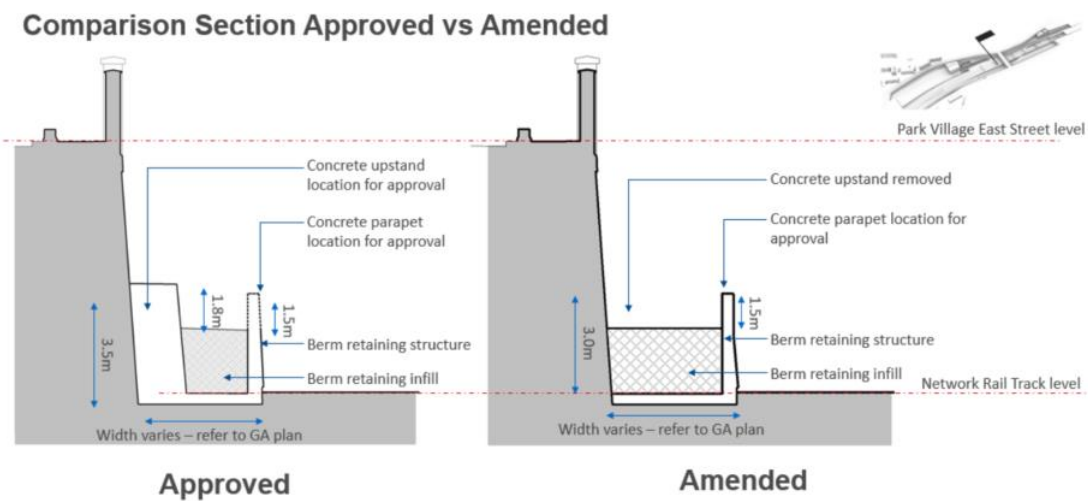


Figure 4: Comparison between approved and amendment schemes showing removal of upstand element south of Mornington Street Bridge to Euston Cavern Shaft



Figure 4: Photo taken at street level from Mornington Terrace facing cutting and proposal site (not visible from street level).

Delegated Report		Analysis sheet		Expiry Date:	19/05/2023
(Members Briefing)		N/A / attached		Consultation Expiry Date:	07/06/2023
Officer			Application Number(s)		
Brendan Versluys			2023/1268/HS2		
Application Address			Drawing Numbers		
Railway cutting adjacent to the existing Park Village East retaining wall extending northwards from south of Mornington Street Bridge to northern extent of Euston Cavern Headhouse.			Please refer to draft decision notice		
PO 3/4	Area Team Signature	C&UD	Authorised Officer Signature		
Proposal(s)					
Amendment to scheme approved as part of Schedule 17 consent for the Park Village East Berm Wall (LPA ref: 2021/0126/HS2, dated 17/03/2021), comprising a retaining structure located in the railway cutting. CHANGES INCLUDE; to change the proposed replacement parapet wall with lower pre-cast L-shaped reinforced concrete wall panels; removal of the proposed upstand element, apart from adjacent to the Euston Cavern Shaft; change in colour to non-pigmented concrete.					
Recommendation(s):		Grant consent			
Application Type:		Schedule 17 Application			

Conditions or Reasons for Refusal:	Refer to Draft Decision Notice					
Informatives:						
Consultations						
Adjoining Occupiers:	No. notified	00	No. of responses	6	No. of objections	6
Summary of consultation responses:	<p>Site notices were displayed on 31/03/2023 (consultation expiry date 24/04/2023) and a notice was placed in the local press on 6/04/2023 (consultation expiry date 30/04/2023).</p> <p>Letters of objection have been received from 6 local residents, summarised as follows:</p> <ul style="list-style-type: none"> • The drawings are not accurate/incomplete. Without accurate drawings it is not possible to get a proper understanding of the 'geo-technical properties of the existing wall'. • The written statement contains misleading material. • The proposed amendment is solely due to the cost saving which is inappropriate. • The proposed non-pigmented concrete will look worse and inappropriate in the heritage setting context. • Due to the berm intended to serve as a road for light goods vehicles, and the proposes amendment to substantially widen the berm, it is even more important that the amended berm be the right colour to fit in with the railroad architecture than it was in the initial application, not less important. • Amendments to approved plans for what the applicant might term 'small changes' are the thin edge of the wedge for continued requests for changes, which run the risk of, in the aggregate, being 'large changes' over the course of construction, which we now understand could be decades before completion. • The proposed amendments would affect the appearance of the berm and would be out of character with the existing railway architecture. • The new low level concrete structures will provide a convenient platform for graffiti painters to access the climbing frame of wailing beams that now covers the wall. <p><u>Officer's response:</u></p> <ul style="list-style-type: none"> • HS2 has acknowledged that due to the length of the PVE berm wall there are some minor changes to the profile to the upper parts of the wall, however the submitted drawings provide a typical cross section of the proposed works and existing elevations at a particular point of the wall based on the 3D scan model and are therefore considered to be accurate. <p>Further, a 3D scan of the wall produced a point cloud survey from which a shared model of the wall was created, and the submission drawings are developed from this data. This is considered the most accurate form of survey data currently available, together with increased understanding of the existing masonry and soil characteristics, following additional monitoring, it is considered that the drawings provide an accurate representation of the existing wall and proposed works.</p>					

- In summary and as discussed in more detail below, HS2 were not satisfied that the addition of pigmentation to the concrete could provide a uniform tone across multiple L- wall units, nor would it weather uniformly. Previous visual examples indicate that the tone can be highly variable giving a “bleached” look to the surface. The proposed amended proposal using a uniform mix of aggregate and cement, will minimise the variability in tone of the walls. The concrete finish will concur with the materiality of the berm, will naturally weather, and darken as it picks up atmospheric pollution, consequently the appearance will change over time and generally accord with the other assets within the cutting. HS2 is committed to reducing emissions and energy use. All proposals are examined to improve sustainability, hence the provision of the new amended proposal which significantly reduces the need for materials and consequently CO2 emissions are substantially less – These are the overriding reason for the amendment being sought. In terms of cost savings, the pigmented concrete is 3 x the cost of standard concrete (future maintenance costs will also be higher), but more importantly HS2 have provided information to show that they will be saving 16,000 tonnes of CO2 emissions, a 65% decrease from the approved scheme.
- Conservation and heritage effects are assessed under sections 3.11 – 3.26 of this report.
- Maintenance of the berm wall is not a material consideration for Schedule 17 application. Notwithstanding, HS2 have advised that they will pro-actively prevent instances of graffiti wherever we can and will assess whether a site needs more deterrent if it becomes a hotspot. Where graffiti has occurred, their policy will be to discourage repeat occurrences through swift intervention, typically by overpainting. HS2’s response will be proportionate the visual and social impact of the graffiti on a case-by-case basis, striving to achieve as close to the original aesthetic of the structure as is practicable using colour match sensors or similar. HS2 will aim to overpaint graffiti as soon as reasonably practicable based on accessibility and operational railway constraints.

**Regent's Park
Conservation Area
Advisory Committee
(RPCAAC)**

Regent's Park Conservation Area Advisory Committee (RPCAAC) have objected on the application. Their response is summarised as follows:

- Concern that the proposed berm wall amendments, as outlined in the Written Statement, are expressed as assertions of structural adequacy rather than full analysis of the way in which the structure has been found to be different from the 2021 position, and the analysis on which the current structural modification is proposed. A fully documented analysis should be provided by the applicant to ensure the structural adequacy of the current proposal. This is of major concern to the RPCAAC in terms of the stability and security of the adjacent Listed buildings and the character and appearance of the conservation area.
- Object to the change of colour proposed for the concrete of the proposed concrete wall panels. It was agreed by Camden in 2021 that the darker colour was appropriate to the historic character of the area: the basis of this judgment has not changed. Rather, as the Master Planning for the Over Site Development on behalf of LendLease has progressed, it is clear that the Cutting will become more visible than it is today. For example, plans for residential development within the existing cutting parallel to Mornington Terrace, with an open space, mean that the new work including that proposed in this application will be more visible as the setting of the Listed Buildings in Park Village East. The new work should be as consistent with the existing historic cutting as possible both in the present and for the future.
- The RPCAAC has not been asked by the applicant to engage in pre-application discussion of these proposals

Officer's response:

- HS2 have advised the 'assertions' of structural adequacy are based upon significant analysis undertaken by our engineers via geo-technical assessments and modelling of the existing wall, which has informed the design of the proposed amendment. The structural adequacy of the proposed amendment is also subject to EMRs and U&As as well as design review panels and assurance undertaken by independent engineers. As per the approved Schedule 17 scheme, the stability and security of the adjacent Listed Buildings is at the forefront of any design development of the mitigation works, whose main objective is to enhance its ability to accommodate ground movements and ensure the safety of the SCS work force for the duration of the HS2 works. Moreover, during construction, the wall will be continuously monitored for any trace of movement and appropriate mitigation will be provided if required.
HS2 have further advised they currently have prisms and tilts on the PVE wall and adjacent properties, studs on the adjacent path in Park Village East, load cells on the ground anchors, patch scanning of adjacent properties in regularly undertaken, boreholes to monitor water levels combined with data analysis of tree root systems as well as ongoing track analysis to check for movement. These are assessments are primarily automated, checked daily and reports are provided weekly.
- Conservation and heritage effects are assessed under sections 3.11

	– 3.26 of this report.
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Site Description

The application site is located at the inner edge of the railway cutting, to the west of the West Coast Main Line (WCML) cutting, just to the north of Euston Station. It directly adjoins the existing Park Village East Retaining Wall.

The site is elongated in form, and is situated parallel to Park Village East highway, as shown in Figure. It is approximately 253 metres in length (as a straight line distance) and approximately 0.23 hectares in area.

Park Village East adjoins Granby Terrace in the south, and Gloucester Gate in the north, providing a link through the residential area to the west of Regents Park.

The site lies parallel to Park Village East which comprises predominantly Georgian housing, including the Grade II* Nash Villas.

North-east of the site is the railway tunnel railings and piers which were added c.1900-6 to match the older work when the New Line was built. Further east is the Grade II listed Parkway Tunnel and Cutting to the Old Line. To the east of the cutting, is a row of Grade II listed Georgian terraced dwellings on Mornington Terrace.

To the west of the site lies the Regent's Park Conservation Area. To the east of the site, outside the railway cutting, is the Camden Town Conservation Area.

Relevant History

The planning history for the site can be summarised as follows:

2021/0126/HS2 - Application for approval under Schedule 17 of High Speed Rail (London - West Midlands) Act 2017 of installation of Wall Berm and Upstand Support Structure to structurally support the existing Park Village East retaining wall. The berm will be located in the railway cutting, adjacent to the existing Park Village East retaining wall and extending from Euston Scissor Box (open section) to Parkway Tunnel; and the installation of Euston Scissor Box (open section) - structurally supporting the portal between the Euston Tunnels and Scissor Cut, located within the railway cutting bound by Mornington Street and Granby terrace Bridge and forming part of the portal for the new HS2 tunnels.
Consent granted 17/03/2021

2019/6302/HS2: Submission under Schedule 17 of High Speed Rail (London - West Midlands) Act 2017 for plans and specifications for the excavation of a railway cutting involving erection of retaining walls with concrete parapets on top between Hampstead Road Bridge and Granby Terrace Bridge adjacent to the existing West Coast Main Line (to the east). **Refused 26/03/2020. APP/HS2/6 - Appeal Allowed 27/07/2020**

2019/4700/HS2: Lorry routes to and from the Euston Approaches and Adelaide Road worksites associated with works for HS2.

Main works activities include: Construction of the Park Village East retaining wall, portal and high-speed dive unders including the installation of ground anchors; Removal of excavated material from the station approach, tunnel portal and headhouse works; Construction of the decks over the high-speed dive under and railway south of Mornington Street Bridge; Construction of the west and east side retaining wall around Hampstead Road Bridge; Extension of Hampstead Road Bridge as well as associated utilities and highway works; Support the movement of plant and material down into the Euston approach railway cutting; Support the removal of excavated material generated in the railway cutting; Construction of Adelaide Road vent shaft and single storey headhouse building; and all other activities for the purposes and in connection with the scheduled and ancillary works. Incorporating lorry routes detailed in 'List of Roads for Approval' document.

Refused 16/09/2019 in conjunction with 2019/6302/HS2. Appeal for non determination Allowed 25/08/2020

Relevant policies

The HS2 Act

- High Speed Rail (London - West Midlands) Act 2017, in particular Schedule 17 paragraph 6

Statutory Guidance

- High Speed Rail (London - West Midlands) Act 2017 - Schedule 17 Statutory Guidance

Environmental Minimum Requirements and related documents

- High Speed Rail (London - West Midlands) Environmental Minimum Requirements (the EMRs) General Principles February 2017
- High Speed Rail (London-West Midlands) Environmental Minimum Requirements Annex 1: Code of Construction Practice High Speed Rail (London-West Midlands) Environmental Minimum Requirements Annex 1: Code of Construction Practice
- High Speed Rail (London-West Midlands) Environmental Minimum Requirements Annex 2: Planning Memorandum
- High Speed Rail (London-West Midlands) Environmental Minimum Requirements Annex 3: Heritage Memorandum
- High Speed Rail (London-West Midlands) Environmental Minimum Requirements Annex 4: Environmental Memorandum
- HS2 Context Report October 2017
- London - West Midlands Environmental Statement 2013
- Supplementary Environmental Statement 4 and Additional Provision 5 (Supplementary Environmental Information) 2015
- HS2 Phase One information papers: environment (series E)
- Local Environmental Management Plan London Borough of Camden (LEMP) December 2017
- Camden Local Traffic Management Plan
- Local Traffic Management Plan (S3 Main Works - Early Works Packages)
- The Dales Local Traffic Management Plan
- High Speed Two Phase One: Route-wide Traffic Management Plan

Assessment

1. Background

Legislation and policy context

- 1.1 Phase One of High Speed 2 (HS2) is the first phase of a new high speed railway network proposed by the Government to connect major cities in Britain.
- 1.2 On 23rd February 2017, Royal Assent was granted, namely the High Speed (London-West Midlands) Act 2017 (“the HS2 Act”), for Phase One of HS2. The HS2 Act provides powers for the construction and operation of Phase One of HS2.
- 1.3 High Speed Two (HS2) Limited is the company responsible for developing and promoting the UK’s new high speed rail network. It is funded by grant-in-aid from the government.
- 1.4 Section 20 of the HS2 Act grants deemed planning permission for HS2 Phase One and associated works (“the Works”) between London and the West Midlands, but some of the detailed design and construction are subject to further approval. Schedule 17 of the HS2 Act puts in place a process for the approval of certain matters relating to the design and construction of the railway which requires that the nominated undertaker (the organisation on whom the powers to carry out the works are conferred, in this case, HS2 Ltd.) must seek approval of these matters from the relevant planning authority. As deemed planning permission has been granted by the Act, requests for approval under Schedule 17 are not planning applications.
- 1.5 Schedule 17 sets out the approvals required to be obtained by HS2 Ltd. These approvals are:
- Plans and specifications of certain works;
 - Matters ancillary to development (“construction arrangements”);
 - Road transport (lorry routes);
 - Bringing into use; and
 - Site restoration schemes
- 1.6 This application seeks approval of “plans and specifications”.
- 1.7 The Council can only consider the application within the constraints of the HS2 Act, rather than planning policies set out in the Development Plan. The grounds for determination under the HS2 Act which the Council can base its decision to approve the application and attach reasonable conditions, or to refuse the application, are set out under Schedule 17 of the Act. In relation to this application, the Council may only refuse to approve plans or specifications on the grounds that:
- (a) the design or external appearance of the building works ought to be modified –*
- (i) to preserve the local environment or local amenity;*
- (ii) to prevent or reduce prejudicial effects on road safety or on the free flow of traffic in the local area; or*
- (iii) to preserve a site of archaeological or historic interest or nature conservation value,*
- and is reasonably capable of being so modified, or*
- (b) the development ought to, and could reasonably, be carried out elsewhere within the development’s permitted limits. [Schedule 17, Part 1, (2) (5)]*
- 1.8 Any representations received from the public or third parties will be considered by the Council but within the context of the HS2 Act.

1.9 It is important to note that the HS2 Act states that all applications must be determined within eight weeks of submission (unless the Council and the Nominated Undertaker agree an extension of time for determination), or the application is deemed to have been refused.

Additional environmental and community protection measures

- 1.10 The HS2 Phase One Environmental Statement (ES) was produced to accompany the HS2 Act. The ES includes the likely significant environmental impacts along the route along with the measures to manage and reduce these impacts. In order to ensure that the environmental effects of the project do not significantly exceed those assessed in the ES, Environmental Minimum Requirements (EMRs) (a group of documents setting out measures to be adopted to reduce adverse environmental impacts), sit alongside the statutory environmental controls included in the HS2 Act. Throughout the construction and operation of Phase One of the project, HS2 Ltd. and its contractors will be required to comply with both the EMRs and those statutory environmental controls. HS2 Ltd. is also required, in addition to the EMR's, to use reasonable endeavours to adopt measures that will further reduce adverse environmental impacts.
- 1.11 The Code of Construction Practice (CoCP) is Annex 1 of the EMRs. It sets out specific details and working practices in relation to site preparation (including site investigation and remediation, where appropriate), demolition, material delivery, excavated material disposal, waste removal and all related engineering and construction activities. The CoCP sets out the measures that the nominated undertaker and contractors are required to implement in order to limit disturbance from construction activities, as far as reasonably practicable, including traffic and transport.
- 1.12 Local Environmental Management Plans (LEMPs) have been prepared for each local authority area which set out site specific control measures to be adopted by HS2 Ltd.'s Contractors.
- 1.13 In considering plans and specifications applications, Camden as a qualifying authority should have due regard to the system of controls available under the HS2 Act and shall not therefore seek to duplicate controls that the EMRs already contain.
- 1.14 There would be specific and significant impacts from HS2 Ltd. and its construction on Camden, Camden Council, petitioners and affected parties, such as Camden Cutting Group. The Council has therefore sought to secure additional assurances on key measures such as amenity controls and community working groups that will help protect the lives and livelihoods of its residents and businesses. Assurance is the term used to describe any other commitments. These are unilateral commitments given directly to petitioners or affected parties, which do not have the status of legally binding contracts enforceable by the courts, but are made binding on the project and ultimately enforced through contempt of Parliament proceedings.

2. Proposal

- 2.1 The request for approval of plans and specifications has been made under paragraphs 2 and 3 of Schedule 17 of the HS2 Act.
- 2.2 The works submitted for approval and their ground for approval under the HS2 Act, include proposed amendments to the approved Park Village East (PVE) Berm Wall Support Structure (approved under 2021/0126/HS2) within the railway cutting at the approach to Euston Station.
- 2.3 Specifically, the proposal seeks to amend the central section of this approved scheme (Euston Cavern Headhouse to Mornington Street bridge).
- 2.4 The key amendments to the central section of the berm wall include:

- Replacement of parapet wall with lower pre-cast L-shaped reinforced concrete wall panels;
- Removal of the significant upstand element, apart from to the rear of the Euston Cavern Shaft where it will be concealed from view;
- Change in colour of berm support structure from 12% pigmented to non-pigmented concrete.

2.5 To connect with small drainage weep holes in the existing PVE berm wall, the proposed amendment scheme also incorporates corresponding weep holes.

3. Assessment

3.0 The main considerations in relation to this proposal are:

- Impact on local environment / local amenity;
- Impact on road safety / the free flow of traffic;
- Impact on archaeological, historic or nature conservation value.

3.1 Normally, when determining a planning application, regard must be had to the development plan and to other material considerations and the determination must be made in accordance with the development plan unless material considerations indicate otherwise (see section 70(2) of the Town and Country Planning Act 1990 and section 38(6) of the Planning and Compulsory Purchase Act 2004). However, the current application is not made under the TCPA, but under Schedule 17 of the HS2 Act and therefore the statutory duties imposed by the 1990 and 2004 Acts do not apply. Nevertheless, the policies of the development plan set out the Council's general approach to dealing with matters of design, conservation, archaeology, amenity and transport and are a helpful reference point in terms of making an assessment of an application of this nature.

3.2 The Council notes that the application is made under paragraphs 2 and 3 of Schedule 17 of the HS2 Act (as noted at paragraph 3.1.1 of the Written Statement submitted in support of the application). The applicant has set out a table which shows the works submitted and their grounds for approval under the HS2 Act. Works of this sought have been clarified by the Planning Appeal reference APP/HS2/6 where the Inspector considered whether the proposed parapet walls (for the erection of retaining and parapet walls between Hampstead Road Bridge and Granby Terrace Bridge) can lawfully be considered as part of the earthworks, in terms of Schedule 17 of the Act.

Local environment or amenity

3.3 A number of objections comment on the visual amenity impacts of the proposed changes to the wall. An assessment of the visual appearance of the amended section of wall in respect to the impact on the local area are made in sections 3.12 – 3.26 below.

3.4 Given the nature of the proposed amendments only resulting in limited change to the previously consented scheme in relation to the height and depth of the PVE berm wall, and the context of the berm wall being set within the existing railway cutting, separated a generous distance from adjacent residential properties, any amenity impacts relative to the approved scheme, would be negligible. Overall, the impact on the local environment / local amenity is considered to be acceptable.

3.5 To conclude this section, given the nature and siting of the proposed works, the development is not considered to have a detrimental impact on neighbouring residential amenity in terms of loss of light, privacy, outlook, overlooking or a sense of enclosure.

3.6 It is noted that there are a number of regulatory controls to mitigate impacts to residential amenity.

These include those under the EMRs, CoCPs, LTMPs, LEMPs, and the assurances specific to Camden alongside the other statutory environmental controls included in the HS2 Act and the assurance that HS2 Ltd. Therefore, there are no outstanding additional issues with regards to the local environment or amenity, which would warrant grounds for refusal on this matter.

Impact on road safety / the free flow of traffic

- 3.7 TfL were consulted and raise no objection.
- 3.8 The proposed amendment to the berm wall design will enable a significant 80% decrease in 'muck-away' large good vehicles (LGVs) and a 41% decrease in concrete LGVs during the construction phase. Therefore, the amended design would lessen impacts on the transport network.
- 3.9 The amended design of the berm will enable construction and operational vehicles to travel off-street, and so the proposal will not result in prejudicial effects on road safety or to the free flow of traffic in the local area.
- 3.10 Following completion of the berm wall, the proposed berm will only be used, whilst in operation for maintenance vehicles.
- 3.11 Overall, when assessed in the context of the approved scheme, impacts of the proposed amendments on the road safety and traffic would be acceptable.

Impact on archaeological, historic or nature conservation value.

- 3.12 The site is not located within an area of archaeological interest, and as such, the proposed development is not considered to result in harm to the archaeological interest of the site. However, with regard to cultural heritage (including archaeological or historic interest), it is noted that control measures are outlined within the Code of Construction Practice (CoCP) (Section 8), E8: Archaeology. This is in addition to the HS2 Phase One Heritage Memorandum within the EMRs.
- 3.13 While the site itself is not located within a Conservation Area, it is adjacent to the Regent's Park and Camden Town Conservation areas and several listed buildings.
- 3.14 It is noted there are functional design requirements for the proposed amendments to the section of berm wall, which are understood to have arisen during the detailed design phase of the berm wall, after the scheme was approved under the original consent (2021/0126/HS2).
- 3.15 The works are to run adjacent to the existing non-designated Park Village East retaining wall and will have a direct physical impact on this asset and the parapet wall above. The principle of these impacts were accepted through approval of the original PVE Berm Wall scheme (2021/0126/HS2).
- 3.16 The Environmental Statement (as amended) does not identify any impacts of the works on the setting for any of the nearby and adjacent heritage assets. In particular, the Environmental Statement does not identify the works as having a physical impact on the adjacent Grade II Parkway Tunnel and Cutting. Given the nature of the proposed amendments resulting in limited change to the footprint of the central section of the PVE berm wall, the proposed amendments are not considered to result in any change to the findings of the Environmental Statement with regard to any physical impacts on adjacent listed buildings.
- 3.17 The construction of the amended berm wall, in terms of its attachment to the existing wall, is proposed to be the same as the approved scheme and therefore no new impact will result in the

construction of the new wall.

- 3.18 With regard to the insertion of weep holes within the proposed amended section of the locally listed wall, the introduction of the weep holes will have a negligible impact on the significance of this asset, as they will not impact the ability for the original railway character to be appreciated or for the ability to appreciate the wall as part of understanding the development of the railway.
- 3.19 HS2 propose to undertake formal recording of the listed asset, to mitigate the intervention of the weepholes and new berm wall to ensure understanding and record of the structure prior to the works.
- 3.20 A number of objections relate to the proposed change in colour of berm support structure from 12% pigmented to non-pigmented concrete.
- 3.21 As discussed within the previous Schedule 17 application for the wall, the colour of the berm wall was of considerable discussion especially after a number of objections were received from neighbouring properties and amenity groups. HS2 have advised that previous visual examples indicate that the tone can be highly variable giving a “bleached” look to the surface. The proposed amended berm wall concrete finish, using a uniform mix of aggregate and cement, will minimise the variability in tone of the walls. The concrete finish will concur with the materiality of the berm, will naturally weather, and darken as it picks up atmospheric pollution, consequently the appearance will change over time and generally accord with the other assets within the cutting.
- 3.22 The proposed amendment to the colour of the wall has been discussed at length with Planning Officers, Urban Designers and Conservation Officers internally and also with HS2 during pre application discussions. Whilst accepted within the previous Officers Report that the element of works which are included within this amendment were visible from Mornington Terrace, Planning Officers have undertaken a number of site visits, and do not consider that the views from the public realm, down onto the tracks to be significant. The wall along Mornington Terrace is considered to screen the views to the track level and therefore, the impact would not be significant.
- 3.23 With regard to adverse effects on the setting of adjacent listed buildings, notably the Grade II listed Nash Villas on Park Village East and Park Village West, while the rail cutting may form part of the setting of the listed buildings, it has a nil or negligible contribution to their significance, which is derived from their picturesque setting and their historic value as a group. With regard to the Grade II listed Parkway Tunnel and Cutting, the proposed amended PVE berm wall would not be viewed as overly dominant when compared to the significant Grade II Listed Portal structure, even when considered creatively from a longer view, due to the mix of finishes, colours and infrastructure that occupies this view.
- 3.24 Within the previous application, verified views were taken from the second Storey windows on Mornington Terrace. Whilst it can be argued that a collection of private views constitute a public view, the view is only possible from a small number of upper floor windows and therefore, in this instance, the views from private residences, are not considered to be a material planning consideration in this consideration., In relation to the wider views, due to the visual obstructions of the parapet walls atop the railway cutting, the adjacent listed buildings and the PVE berm wall would not be visible together as viewed from surrounding streets.
- 3.25 The official listing of the Grade II* Nash Villas in Park Village East relate to the architectural features of these properties, which are not affected by the proposed works. All the intrinsic significance of these properties would also be unaffected by the proposals as the amendments relate to the central section of the PVE berm wall located at the base of the railway cutting. The setting of the Grade II* listed buildings and the adjacent Conservation Areas are located approximately 18m above the railway cutting and the works do not alter the setting of its adjoining and immediate surroundings. Furthermore, the Grade II* Nash Villas and the Conservation Areas are set back behind the Park Village East parapet wall and the adjoining roads. Therefore, the proposals would have negligible impact on the proposed listed buildings and the Conservation

Areas.

3.26 Overall, the public benefits of the proposal which would see a 41% decrease in concrete LGVs and a saving of 16,000 tonnes of CO2 emissions, a 65% decrease from the approved scheme would outweigh any less than significant harm to the designated harm assets.

Recommendation: Approval of plans and specifications pursuant to paragraphs 2 and 3 of Schedule 17 of the HS2 Act.

The decision to refer an application to Planning Committee lies with the Director of Regeneration and Planning. Following the Members Briefing panel on Monday 4th September 2023, nominated members will advise whether they consider this application should be reported to the Planning Committee. For further information, please go to www.camden.gov.uk and search for 'Members Briefing'.

Application ref: 2023/1268/HS2
Contact: Brendan Versluys
Tel: 020 7974 1196
Email: Brendan.Versluys@camden.gov.uk
Date: 29 August 2023

Development Management
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SCS Railways Joint Venture (SCS)
Black Arrow House
2 Chandos Road
London
NW10 6NF
United Kingdom

DRAFT

Dear Sir/Madam

DECISION

High Speed Rail (London - West Midlands) Act 2017

Schedule 17 - Conditions of Deemed Planning Permission Approval

Address:

**Park Village East
NW1 2DU**

Proposal:

Amendment to scheme approved as part of Schedule 17 consent for the Park Village East Berm Wall (LPA ref: 2021/0126/HS2, dated 17/03/2021), comprising a retaining structure located in the railway cutting. CHANGES INCLUDE; to change the proposed replacement parapet wall with lower pre-cast L-shaped reinforced concrete wall panels; removal of the proposed upstand element, apart from adjacent to the Euston Cavern Shaft; change in colour to non-pigmented concrete.

Drawing Nos: 1MC03-SCJ_SDH-AR-DEL-SS01_SL03-040094, rev P01.1; 1MC03-SCJ_SDH-AR-DEL-SS01_SL03-040034, rev P01.1; 1MC03-SCJ_SDH-AR-DEL-SS01_SL03-040032, rev P08; 1MC03-SCJ_SDH-AR-DDE-SS01_SL03-040092, rev P02; 1MC03-SCJ_SDH-AR-DGA-SS01_SL03-040012, rev P08; 1MC03-SCJ_SDH-AR-DLO-SS01_SL03-040021, rev P02; 1MC03-SCJ_SDH-IN-APP-SS01_SL03-000020, rev C01; 1MC03-SCJ_SDH-IN-STA-SS01_SL03-000005, rev C01; 1MC03-SCJ_SDH-IN-TEM-SS01_SL03-000005, rev C01

The Council as the Local Planning Authority & Qualifying Authority within the meaning of the above Act has **granted** permission subject to the following condition(s) and informative(s) listed below:

Condition(s) and Reason(s):

- 1 The development hereby permitted shall be carried out in accordance with the following approved plans- 1MC03-SCJ_SDH-AR-DEL-SS01_SL03-040094, rev P01.1; 1MC03-SCJ_SDH-AR-DEL-SS01_SL03-040034, rev P01.1; 1MC03-SCJ_SDH-AR-DEL-SS01_SL03-040032, rev P08; 1MC03-SCJ_SDH-AR-DDE-SS01_SL03-040092, rev P02; 1MC03-SCJ_SDH-AR-DGA-SS01_SL03-040012, rev P08; 1MC03-SCJ_SDH-AR-DLO-SS01_SL03-040021, rev P02; 1MC03-SCJ_SDH-IN-APP-SS01_SL03-000020, rev C01; 1MC03-SCJ_SDH-IN-STA-SS01_SL03-000005, rev C01; 1MC03-SCJ_SDH-IN-TEM-SS01_SL03-000005, rev C01

Reason: For the avoidance of doubt and in the interest of proper planning.

Informative(s):

- 1 In accordance with assurances given by the Secretary of State, the applicant is reminded that HS2 Phase 1 Environmental Minimum Requirements must at all times be fully complied within undertaking the works.
- 2 In accordance with the Phase 1 Code of Construction Practice, the applicant must adhere to the control measures set out in the HS2 Phase 1 Route-wide Traffic Management Plan and the Camden Local Traffic Management Plan. The measures contained in the Local Traffic Management Plan must be kept under review during the execution of the works, in consultation with TfL, London Borough of Camden and other relevant stakeholders.

Yours faithfully

Chief Planning Officer

DECISION