

SEDLEY PLACE

Design & Access Statement
Front Yard Refurbishment
7 Parsifal Road

April 2023
CDD3161_DAS_S2(R0_MN)

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Our Ref: CDD3161_DAS_01(R0_MN)

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1.0 Introduction

- 1.1 This Design and Access Statement is made in respect of a planning application for the refurbishment of the front garden and off street parking area at No. 7 Parsifal Road, London NW6 1UG.
- 1.2 This application follows the receipt of a letter to the applicant from Camden, dated 9th March 2023, (see appendix A) requesting information about alleged unauthorised works at the site address concerning a plywood cover over the garage door opening, and removal of the front garden brickwork piers.

2.0 Description of the Existing Land and Surroundings

- 2.1 No 7 Parsifal Road is a semi-detached residential house in West Hampstead, Camden.
- 2.2 The house is owned and occupied by the Daniel family who acquired it in March 2018 and have resided at this address for the last 5 years.



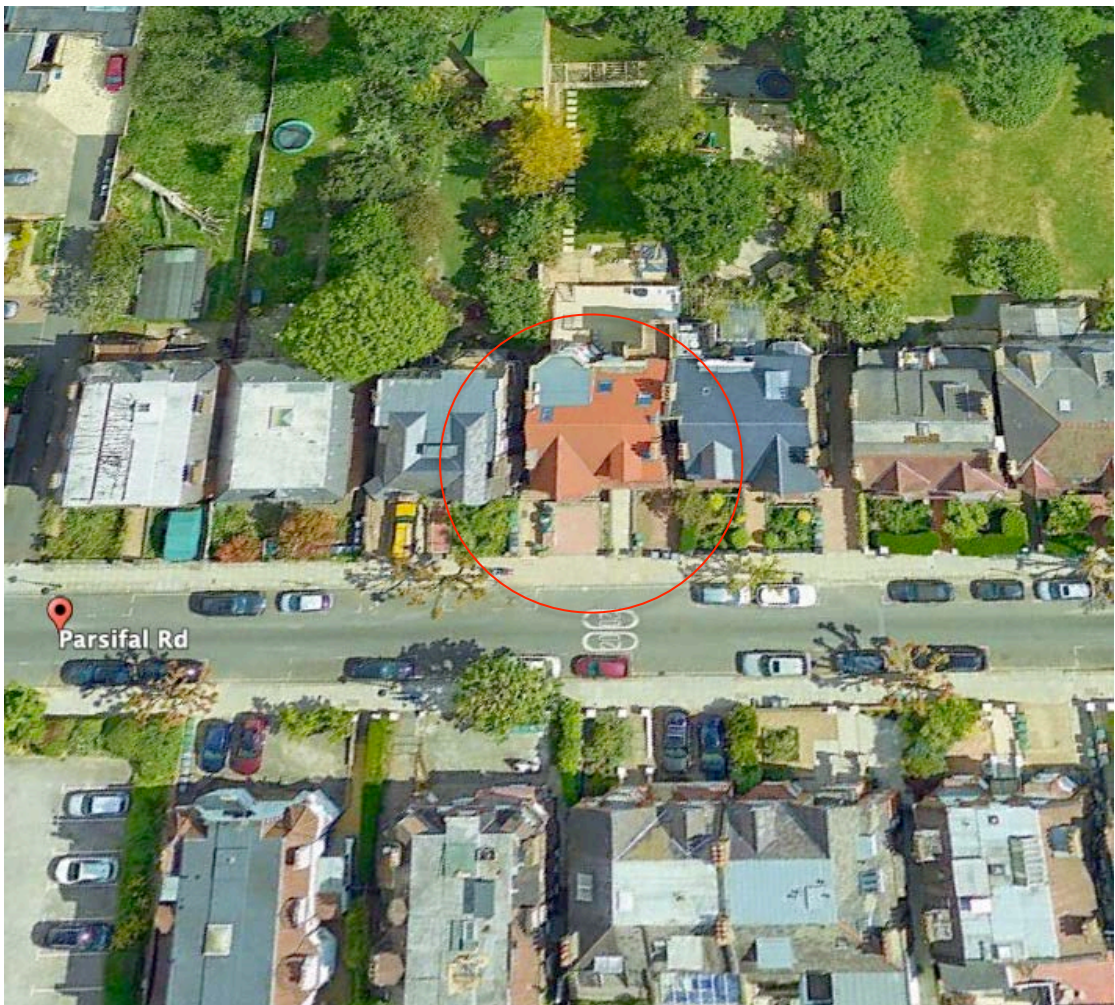
Plan to show site location.

Source: Ordnance Survey

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- 2.3 The house was refurbished internally in 2018-2020. It is a red-multi coloured brick, early 1900's house with a wide frontage and central front door. The house was designed with a lower ground floor garage and has a ramp down from street level on the right-hand side of the garden path. On the left-hand side of the garden path, is a further parking space on level ground that was presumably once garden.
- 2.4 At some point in 2021 the 2 gate piers were destroyed by separate accidents. The aerial photo below shows the situation in November 2020 and shadows indicate that the 2 gate piers were still standing at that time. The left-hand corner pier was lost at the time of this photo was probably missing in 2018 when the house was acquired, but the applicant does not know when the left-hand corner pier was lost.
- 2.5 The parking spaces are laid out in red interlocking blocks and there are blue/black brick borders to the path and the parking areas. On the left-hand side a further path ramps down to a gate serving the rear garden. There is a row of wheelie bins stored here.



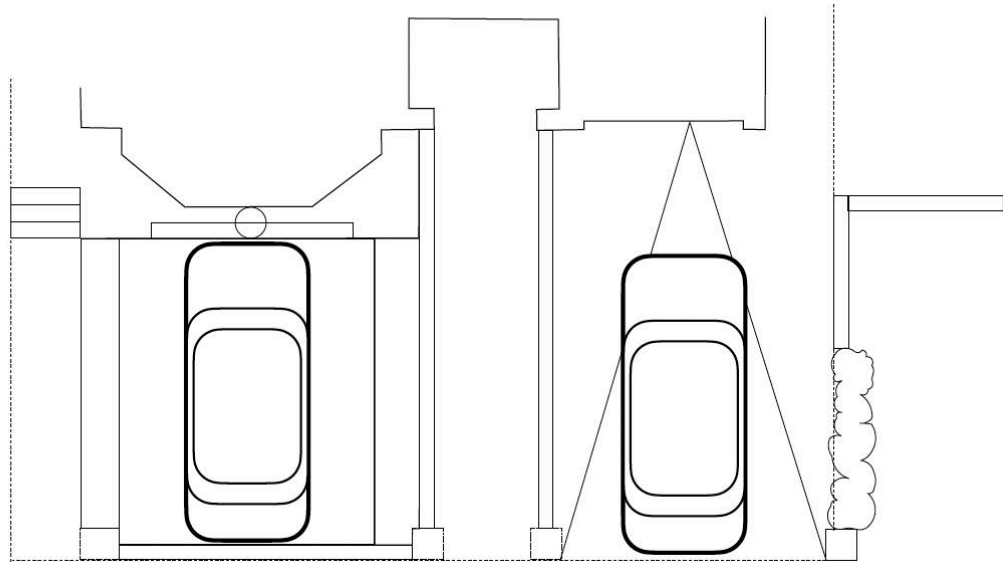
Aerial Photo of Parsifal Road in Nov 2022

Source: Google Earth

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- 2.6 There is a drop in level of approximately 1.5 to 1.7m between the highway and the front facade of no 7. There is a retaining wall on the LHS, which has failed in the past and has been buttressed, but the brickwork is visibly failing. The change in level is unprotected and, on the RHS of the garden path is separated by a shallow wall of approximately 150mm in height.



Plan as existing

Source: Sedley Place

- 2.7 One of the governing limitations of the proposals is the narrow space between the current buttressed retaining wall and the bay window, which is only 450mm. The buttressed brickwork is in poor condition as is evident from the photo below.



Photo of existing buttress brickwork in front of bay window

Source Sedley Place Design Ltd

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3.0 Planning Context

- 3.1 The internal alterations carried out by the applicant in 2018-2020 did not required planning consent, but there was scaffold at the front of the house for redecoration, and a succession of deliveries and skips placed in the front yard over a 24-month period.
- 3.2 The front of No 7 has been used for parking since before 1999. The aerial photo below shows the layout of the front garden in 2013 (with a large delivery vehicle parked in the LH parking space).



Aerial Photo of Parsifal Road

Source: Google Earth Pro

- 3.3 During the recent refurbishment works a delivery truck knocked over one of the front gate piers. Soon after, a car using the front of the drive to turn, knocked over the gate pier.
- 3.4 The houses downhill on the street mostly have a front garden, with front walls and hedgerow, with on-street parking. At No 9 and No 10 a degree of off-street parking starts to predominate, and this continues for the uphill houses. At No 7 and No 10 some gardens have been adapted to provide a mixture of parking with smaller front gardens, with space for either one car or two, and some have access to an original garage door (like No 7), and some have all 3 features some such as No 6 which is opposite the applicant's house.

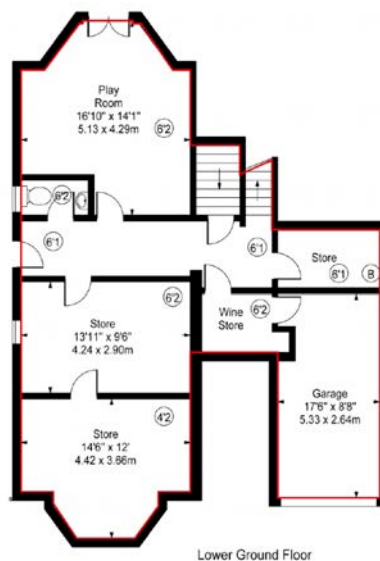
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Photo of No 6 Parsifal Road, which has parking for 2 cars as well as a small garden and a bin enclosure and a similar pre-existing garage as No 7. Source Google Earth

- 3.5 The plan of the existing house has a space dedicated as a garage on the lower ground floor. This is too small for modern cars and can not be widened due to the limits of the existing structure. The garage at No 7 is 2640mm wide internally with a doorway of approximately 2240mm. Most cars are between 1800 -1950mm wide, so it would not be possible to comfortably get a car through such a narrow the doorway and open a car door within. It is for this reason that all the originally designed garages in the street are no longer used for their intended purpose.



Plan of LG floor of No 7

Source: Applicant

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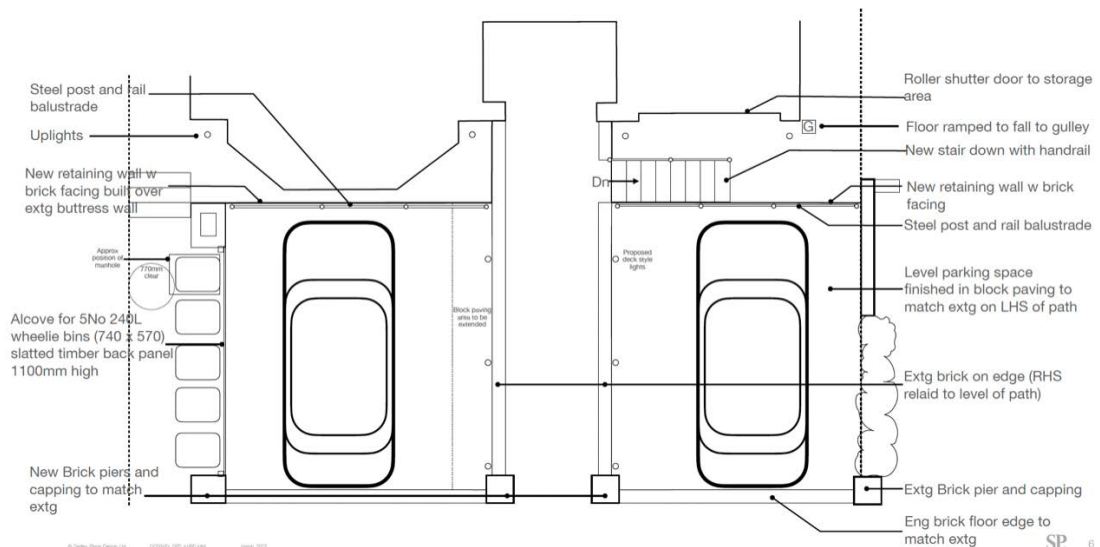
4.0 Design Proposal

4.1 The proposed design includes the following elements:

- New level parking space on the RHS of the garden path to replace the ramp.
- New block paving to both hard standings (garden path and engineering brick edges retained).
- New retaining facing walls added on both sides to make the ground structurally safe.
- Retaining and safety post and rail steel balustrades on both sides to guard both drops.
- New flight of steps down to lower yard with matching balustrade and handrail.
- Replacement of plywood cover over garage door with roller shutter, finished in white.
- New bin store shelter based on the same design as used at No 6.
- 4 No up lights to lower yard to illuminate 4 No new trees in pots.
- 6 No low profile pavement lights to help define safe routes in the dark.
- Rebuilding of 3 No brick piers using reclaimed stock bricks and white painted stone capping.

4.2 Future proposals may include changing the garage door to a window but that does not form part of this application.

4.3



4.4 The material for the tegular paving shall be Marshalls 'Driveset' Tegular 60 in Burnt Ochre as per the reference image below:



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- Reference image for tegular paving and existing engineering capping Source: Marshalls and SPL
- 4.5 Engineering bricks on edge will be used as a 225mm edging to the tegular paving and where existing blue/black engineering bricks exist these will be retained as capping and edgings.
- 4.6 The retaining walls to the parking areas will be faced with a reclaimed London red stock slip. This will be as close as possible to the facing colour of the house. Slips rather than bricks will be used because of the limited working space between the bay window and the retaining wall once the concrete blocks have been installed.



Photo of brick slip panel to be used for facing concrete block Source

Reclaimed Brick-Tile

- 4.7 On either side of the garden path, where the brick edge is not flush with the garden path, a walk through will be formed by dropping the bricks to path level for a width of 700mm.
- 4.8 The new railings and handrails will be simple but traditional park style design, with an overall height of 900mm and comprising a 70mm diameter posts and 3 solid steel rails. The posts and rails will finished in black gloss paint. The specification shall be the Irwell PU 3 Rail system as per the image below.



Photo of the Irwell PU 3 Rail system

Source 'Broxap'

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- 4.9 The white roller shutter garage door shall be a slatted system fitted with railed, inside the brick reveals and factory finished white for durability.



Photo of generic steel roller shutter garage door and slatted pressure treated fencing

Source: Internet

- 4.10 The bin enclosure shall be formed using 75mm oak posts set against brick piers and finished with 70mm stained timber slats, laid horizontally with 20mm spaces between. On the top edge will be a 38mm softwood capping stained to match, to protect the fence from the weather.
- 4.11 The walk on low profile pavement lights will be both functional and aesthetic. They help to guide the way in darkness and help those unfamiliar with the house to identify hazards.

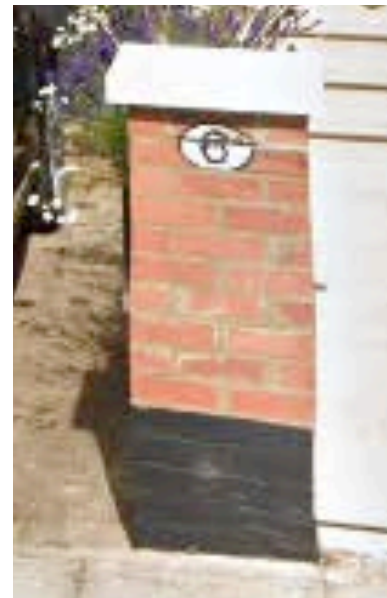


Photo of walk on pavement light and brick pier style with white stone capping to be Painted number 7.

Source SPL

- 4.12 The 3No brick piers will be re-constructed from reclaimed stock bricks. Each will be 450 x 450mm in girth (2 bricks in each dimension in plan). The height shall be 120mm to match the others in the street. The house number (7) will be painted onto the face of the LH pier in black oval on a white background.

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5.0 Accessibility and Sustainability

- 5.1 The proposals make accessibility easier by creating a level hardstanding for passengers alighting from vehicles and eliminating the existing steep ramp.
- 5.2 Trip hazards are being removed and the new guarding will make the changes in level safer for householders, as well as unfamiliar members of the public and delivery persons.
- 5.3 The lighting proposed will be low energy LED light with an 8-year lamp life in an IP64 casing.

6.0 Conclusions

- 6.1 The proposals will restore missing original features to the front elevation of No 7 Parsifal Road, as required by the Planning Authority, and will bring about an aesthetic and sympathetic upgrade to the house and the street scene in general.
- 6.2 The creation of an accessible parking space and a safer arrangement for negotiating the changes in level that prevail on the site, will result in a net gain for the property.

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Appendix A

