

1MCo3 Main Works - Contract Lot S1

Schedule 17 Plans and Specifications - Written Statement - Amendment to Berm Wall Pack 1 (South of Mornington Street Bridge to northern extent of Euston Cavern Headhouse) - Park Village East Wall S1

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High Speed Rail (London – West Midlands) Act 2017

HS2 Ltd

London Borough of Camden

**Amendment to Berm Wall Pack 1 (South of Mornington
Street Bridge to northern extent of Euston Cavern
Headhouse) - Park Village East Wall S1**

**Schedule 17 Plans and Specifications Written
Statement for Information**

[SCS/ HS2 ref. - TBC]

Document Reference: 1MC03-SCJ_SDH-IN-STA-SS01_SL03-000005

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1 Introduction

1.1 Background Information

Table 1: Park Village East Berm Wall Schedule 17 Address Details and Description of Works

Site	Details
Scheme	High Speed Two
Applicant	High Speed Two (HS2) Limited
Applicant Address	<i>c/o Agent:</i> SCS Railways Joint Venture (SCS) Black Arrow House 2 Chandos Road NW10 6NF
Site Address	Park Village East, London Borough of Camden. NW1 2DU The works are located from: X528866 (Easting), Y183280 (Northing) to X528717 (Easting), Y183505 (Northing).
Description	Submission under Schedule 17 of the High Speed Rail (London-West Midlands) Act for approval of amendments to the approved Park Village East Berm Wall (LBC Application Ref: 2021/0126/HS2). The Park Village East Berm is a berm retaining structure located in the railway cutting, adjacent to the existing PVE retaining wall and extending northwards from south of Mornington Street Bridge to northern extent of Euston Cavern Headhouse.

1.2 Terms of Reference

1.2.1 This Written Statement is compiled in accordance with the High Speed Two (HS2) Phase 1 Planning Memorandum and Planning Forum Notes (PFNs) as required by the planning regime established under Schedule 17 of the High-Speed Rail (London – West Midlands) Act 2017.

1.2.2 The submission documents that have been prepared to support the Plans and Specifications submission under Schedule 17 of the Act have been prepared in accordance with the PFN1, PFN2 and PFN3. The engagement undertaken to inform the preparation of this Plans and Specification submission has been in compliance with PFN4 and PFN5.

1.2.3 This statement provides the London Borough of Camden with information to assist with the determination of the Plans and Specifications submission under Schedule 17, in relation to the above description of works.

1.2.4 The information in this Written Statement is provided for information to assist in determining the request for approval. It is not for approval.

1.3 Introduction to High Speed 2

1.3.1 HS2 is a new high-speed railway network that will connect major cities in Britain. It will bring significant benefits for inter-urban rail travellers through increased capacity and improved connectivity between London, the Midlands and the North. It will release capacity on the existing rail network and so provide opportunities to improve existing commuter, regional passenger and freight services.

1.3.2 Phase One of HS2 will provide a dedicated high-speed rail service between London, Birmingham and the West Midlands. It will extend for approximately 230km (143 miles). Just north of Lichfield, high speed trains will join the West Coast Main Line for journeys to and from Manchester, the North West and Scotland.

1.3.3 For further information on HS2 and the route through the London Borough of Camden please refer to the Planning Context Report for the London Borough of Camden, deposited with the Council by HS2 Ltd.

1.4 High Speed Rail (London – West Midlands) Act 2017

1.4.1 The High-Speed Rail (London – West Midlands) Act 2017 ('the Act') provides powers for the construction and operation of Phase 1 of High Speed Two. HS2 Ltd is the nominated undertaker in relation to the works subject to this Plans and Specifications submission.

1.4.2 Section 20 to the Act grants deemed planning permission for the works authorised by it, subject to the conditions set out in Schedule 17. Schedule 17 includes conditions requiring the following matters to be approved or agreed by the relevant LPA.

- Construction arrangements (including large goods vehicle routes);
- Plans and specifications;
- Bringing into use requests; and
- Site restoration schemes.

- 1.4.3 This is therefore a different planning regime to that which usually applies in England (i.e. the Town and Country Planning Act) and is different in terms of the nature of submissions and the issues that the LPAs can have regard to, in determining requests for approval.
- 1.4.4 Schedule 17 of the Act sets out the grounds on which the LPA may impose conditions on approvals or refuse requests for approval.
- 1.4.5 This Written Statement includes information supporting the Plans and Specifications submission in relation to the matters outlined in **Table 2** below.

Table 2 Schedule 17 Plans and Specifications Submission Details

Site	Details
Plans and Specifications (permanent works)	<ul style="list-style-type: none">• Building works – PVE Berm Wall (Paragraph 2)• Earthworks – PVE Berm Wall Support Structure¹ (Paragraph 3)

- 1.4.6 With respect to the details identified as earthworks, paragraph 3 (9) defines “earthworks” as, “*terracing, cuttings, embankments or other earthworks.*” The Retaining Wall constitutes earthworks because it is not a wall in the usual sense of forming a boundary or enclosure; instead it forms part of the earthwork itself, being the external part of it and consequently an integral and necessary part of the cutting.
- 1.4.7 The works to which this application relates, and the cumulative impact of the works in conjunction with other HS2 development, have been assessed and are compliant with paragraph 1.1.3 (bullet point 2) of the HS2 Phase 1 Environmental Minimum Requirements General Principles².

1.5 High Speed Two: Environmental Minimum Requirements

- 1.5.1 The Environmental Statement (ES) (as amended) is an assessment of the likely significant environmental effects of the proposed HS2 railway and the proposals to avoid, reduce or remedy these likely significant environmental effects.
- 1.5.2 HS2 Ltd as the nominated undertaker is contractually bound to comply with the controls set out in the Environmental Minimum Requirements (EMRs). These

¹ The Park Village East Berm Wall is a structural support for the existing retaining wall. Further detail on the purpose and design of the berm is outlined in paragraph 4.2.2.
² https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/618074/General_principles.pdf

controls along with the powers contained in the High-Speed Rail (London – West Midlands) Act and the Undertakings and Assurances will ensure that impacts which have been assessed in the ES will not be exceeded.

1.5.3 The EMRs comprise the following suite of documents:

- Code of Construction Practice (CoCP);
- Planning Memorandum;
- Heritage Memorandum;
- Environmental Memorandum; and
- Undertakings and Assurances.

1.6 High Speed Two: Code of Construction Practice

1.6.1 HS2 Ltd as the nominated undertaker is contractually bound to comply with the controls set out in the Environmental Minimum Requirements. The Environmental Minimum Requirements include the High Speed Two Code of Construction Practice (CoCP).

1.7 Structure of Written Statement

1.7.1 This Written Statement is structured as follows:

- A description of the location and main characteristics of the works area is provided in **Section 2**;
- **Section 3** describes the main works being undertaken in the area, as set out in Schedule 1 of the Act, and those that are the subject of this Schedule 17 Plans and Specifications submission;
- The design criteria and rationale for the works which are the subject of this Schedule 17 Plans and Specifications submission are described in **Section 4**;
- **Section 5** summarises the pre-submission consultations that were undertaken, including a list of the consultees, dates, attendees at meetings and a brief summary of the outcome of these discussions;
- A high-level programme for the works and how they fit into the wider programme for other works in the area, as set out in Schedule 1 of the Act, is provided in **Section 6**; and
- **Section 7** identifies any other main consents, or known forthcoming consents associated with the works.

2 Site Location and Characteristics

2.1 Site Location

- 2.1.1 The application site (known hereafter as the 'site') is elongated in form, and is situated parallel to Park Village East highway, as shown in **Figure 1**. It is approximately 253 metres in length (as a straight line distance) and approximately 0.23 hectares in area.
- 2.1.2 Park Village East adjoins Granby Terrace in the south, and Gloucester Gate in the north, providing a link through the residential area to the west of Regents Park.
- 2.1.3 The site lies to the west of the West Coast Main Line (WCML) cutting, just to the north of Euston Station. It directly adjoins the existing Park Village East Retaining Wall. There is no direct view into the railway cutting site from the public realm.



Figure 1 Aerial view of Euston and surrounding area. Approximate site location demarcated in red (Map data Google 2019)

2.2 Adjacent Land Uses

- 2.2.1 The site lies parallel to Park Village East which comprises predominantly Georgian housing, including the Grade II* Nash Villas.
- 2.2.2 North-east of the site is the railway tunnel railings and piers which were added c 1900-6 to match the older work when the New Line was built. Further east is the is the Grade II listed Parkway Tunnel and Cutting to the Old Line. To the east of the cutting, is a row of Grade II listed Georgian terraced dwellings on Mornington Terrace.
- 2.2.3 Regents Park lies approximately 300m to the west of the site and is one of London's largest and most significant areas of open space.

2.3 Environmental Characteristics

- 2.3.1 Regent's Park Site of Metropolitan Importance (SMI) lies approximately 300m to the west of the application site. The park contains mature parkland trees, a small enclosed woodland, an ornamental lake and a grassland area managed specifically for wildlife.
- 2.3.2 As the site is separated from Regent's Park SMI by the intervening streets, it is not anticipated that construction works will have any impacts on the SMI.
- 2.3.3 In addition, there are several designated heritage assets in the area, as summarised in **Table 3** and **Figure 2**.

Table 3 Designated heritage assets in proximity to the application site

	Listed Asset	Type	Distance to PVE Berm Support Structure
1	Nash Villas along Park Village East	Grade II*	Adjacent to and above site at road level, along Park Village East highway
2	Nash Villas along Park Village West	Grade II*	~80m to west of site
3	Parkway Tunnel and Cutting	Grade II	~100m to north of northern extent of site
4	Pair of stone piers with lamp standards to east end of Mornington Street railway bridge	Grade II	~60m to east of site
5	Pair of stone piers with lamp standards to west end of Mornington Street railway bridge	Grade II	Above south part of site

	Listed Asset	Type	Distance to PVE Berm Support Structure
6	Regents Park Barracks, Block K (The Officers' Mess)	Grade II	~70m to west of site
7	York and Albany Public House	Grade II	~50m to north of site
8	Parkway: Nos. 119 - 123 and 125	Grade II	~100m to north of site
9	Regents Park Conservation Area	-	Adjacent to west
10	Camden Town Conservation Area	-	To east of cutting

2.3.4 As included in **Table 3** and **Figure 2**, Regents Park Conservation Area lies to the west. It covers the eastern part of John Nash's Regent's Park masterplan development, as created in the early 19th century - this comprises the Nash villas on Park Village East and Park Village West, alongside Regents Park.

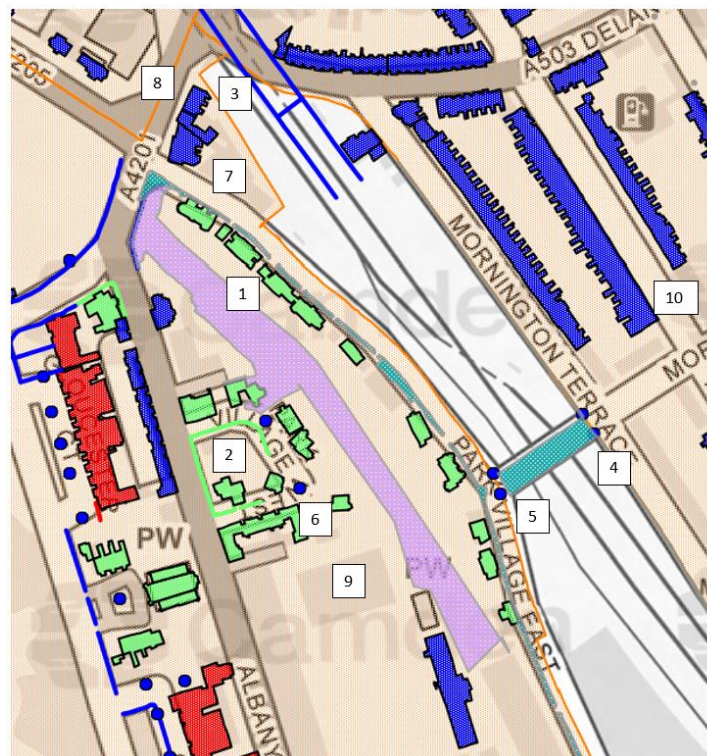


Figure 2 Designated heritage assets in proximity to Park Village East Berm

2.3.5 There are also several non-designated heritage assets near the site which contribute to the special character of the area. These comprise the locally listed structures associated with the expansion of the London to Midland Railway at the beginning of the 20th century. These include Mornington Street Bridge; the parapet wall at street level which runs south from 1 Park Village East to Granby Terrace (adjacent to the

west of the rail cutting); and the parapet wall at street level along Mornington Terrace and Clarkson Row (adjacent to the east of the rail cutting).

2.3.6 The rail cutting itself in this area (north of Granby Terrace) is a significant non-designated asset as it remains largely as it was altered in circa 1905, with elements of the 1870s work evident in some areas. The cutting retains an element of its original character but has been altered constantly as it responds to the technological advancement of the railway, through the provision of gantries, Power Supply Points, Auto Transformer Stations and the recent provision of the undercut of the West Coast mainline.

2.3.7 These retaining walls are included as 'street features or other structures' on Camden's Local List (adopted on 21 January 2015), which details non-designated heritage assets within the Borough.

2.4 Surrounding Highway Network

2.4.1 The site is adjacent to Park Village East highway, a local road that connects to Parkway (A4201) to the north. Further to south, Park Village East forms onward connections to Hampstead Road (A400).

2.4.2 Park Village East also connects to Mornington Street Bridge, which crosses the railway cutting to Mornington Terrace. Mornington Terrace itself connects with Delancey Street (A503) to the north. It also forms onward connections to Hampstead Road (A400).

3 Description of the Works

3.1 Introduction

- 3.1.1 This Written Statement supports the Schedule 17 submission for the approval of plans and specifications for amendments to the approved Park Village East Berm Wall Support Structure in the vicinity of Euston Station.
- 3.1.2 The Plans and Specifications submitted for approval are listed in the proforma accompanying the application. A summary of the rationale for the amendment application and proposed works are provided in **Section 3.2 – 3.3**, and a comparison with the originally approved scheme in **Section 3.4**.
- 3.1.3 Information on adjoining HS2 works is supplied in **Section 3.5**. This information is not for approval under Schedule 17.
- 3.1.4 **Sections 3.6 to 3.11** provide information on other aspects of the works to assist in understanding the context of the works being submitted for approval. The text contained within these sections is not for approval under Schedule 17.

3.2 Why has the proposal been amended?

- 3.2.1 The section of the PVE Berm Wall approved (LBC Ref: 2021/016/HS2) has been amended for the following reasons:
- There has been a diminution of the mass and bulk of the proposed berm structure because of a better understanding of the geo-technical attributes of the existing wall, which has led to a significant reduction in the requirement for composite materials and subsequently LGV movements and CO₂ emissions as part of the manufacturing process;
 - Further investigation of the cutting has established that concrete walls inserted as part of the West Coat Mainline Works (circa 2000) similar to the amended scheme are already in situ in the location between the Cavern Headhouse and Mornington Street bridge;
 - Concerns were raised as to whether there would be a consistent colour in the pigmentation process given the length of the proposed wall, as it is not possible to produce an absolute identical colouring given that this process relies on temperature, air humidity and the consistent colour and texture of the aggregates used;

- Views of the Berm Wall in public areas are inconspicuous, and therefore we do not consider that the lighter coloured wall would harm the setting of the Park Village East Listed properties or the Listed Parkway Tunnel and Cutting;
- The cutting retaining wall located along Mornington Terrace blocks views from street level and therefore no resultant harm would be considered to occur to the neighbouring conservation area to the east of the cutting; and
- It is considered that the Berm Wall would not harm the setting of the Grade II* listed buildings as it would be read in the immediate context of the railway structures and not within the setting of the listed buildings at street level above.

3.3 Works for Approval

- 3.3.1 This is one of three Schedule 17 applications to amend the approved PVE Berm Wall scheme (LPA application ref: 2021/0126/HS2).
- 3.3.2 The Park Village East Berm Wall Support Structure is not a scheduled work under Schedule 1 of the HS2 Act. It will instead be carried out under the ancillary powers of Section 2 of the Act for the purposes of facilitating scheduled works under Schedule 1 of the Act. The proposed works are within limits.
- 3.3.3 Additionally, a detailed assessment of the works has been undertaken to confirm that any new works remain comparable to the ES (as amended). It has been considered overall that there are no new or different significant effects when compared to those reported in the ES (as amended).
- 3.3.4 The works submitted for approval are summarised in Table 4 below.

Table 4 The works submitted for approval and their grounds for approval under the HS2 Act

Proposed works	HS2 Act Grounds for Refusal
Park Village East Berm Wall Support Structure	<p>As 'Building works' under Sch.17, Part 1, Paragraph 2:</p> <p>(a) the design or external appearance of the building works ought to be modified—</p> <ul style="list-style-type: none"> (i) to preserve the local environment or local amenity, (ii) to prevent or reduce prejudicial effects on road safety or on the free flow of traffic in the local area, or (iii) to preserve a site of archaeological or historic interest or nature conservation value, <p>and is reasonably capable of being so modified, or</p> <p>(b) the development ought to, and could reasonably, be carried out elsewhere within the development's permitted limits.</p>
Park Village East Berm Wall Support Structure	<p>As 'Earthworks' under Sch.17, Part 1, Paragraph 3:</p> <p>That the design or external appearance of the works ought to, and could reasonably, be modified –</p> <ul style="list-style-type: none"> (a) To preserve the local environment or local amenity, (b) To prevent or reduce prejudicial effects on road safety or on the free flow of traffic in the local area or, (c) To prevent a site of archaeological or historic interest or conservation value. <p>If the development does not form part of a scheduled work, that the development ought to, and could reasonably, be carried out elsewhere within the development's permitted limits.</p>

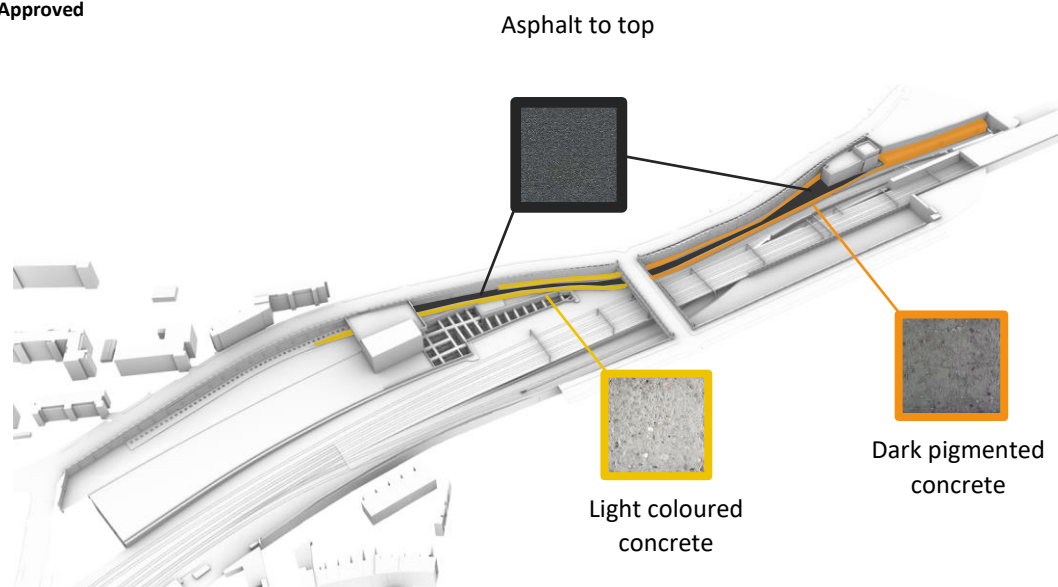
3.4 Comparison to approved scheme

- 3.4.1 The Park Village East Berm & Upstand was granted approval, under Schedule 17, on 17 March 2021 (LPA ref: 2021/0126/HS2) by the London Borough of Camden.
- 3.4.2 The current submission seeks to amend the central section of this approved scheme (Cavern Headhouse to Mornington Street bridge). Amendments for the north and south sections of the approved scheme will be dealt with in two subsequent Schedule 17 consent applications.
- 3.4.3 The key amendments in the current submission (**Figures 3 - 5**) include:
- Replacement of parapet wall with lower pre-cast L-shaped reinforced concrete wall panels;

- Removal of the significant upstand element, apart from to the rear of the Euston Cavern Shaft where it will be concealed from view;
- Change in colour of berm support structure from 12% pigmented to non-pigmented concrete.

To connect with small drainage weep holes in the existing PVE wall, the proposed amendment scheme also incorporates corresponding weep holes.

Approved



Proposed

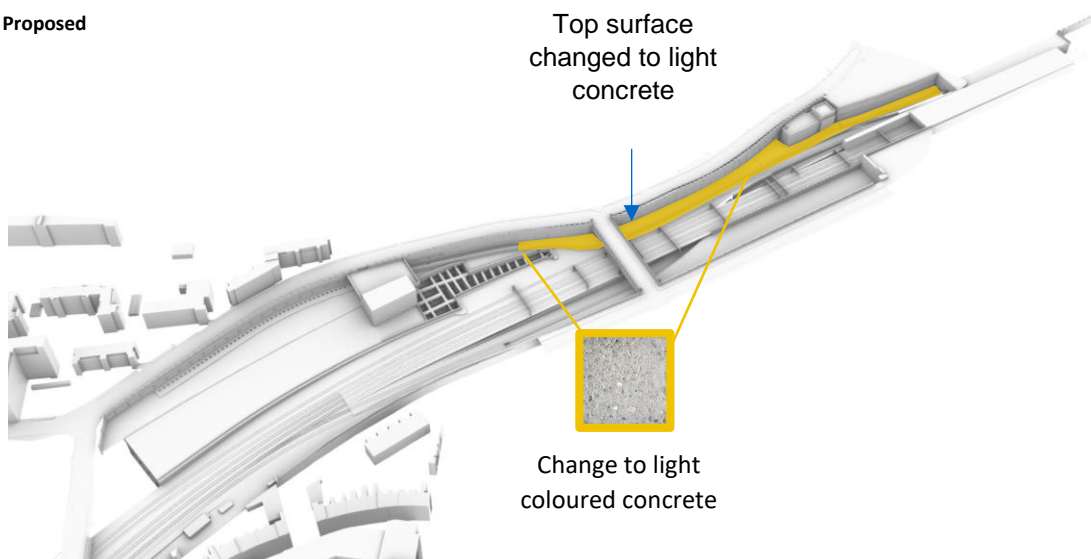


Figure 3 Comparison of materiality between approved and amendment schemes.

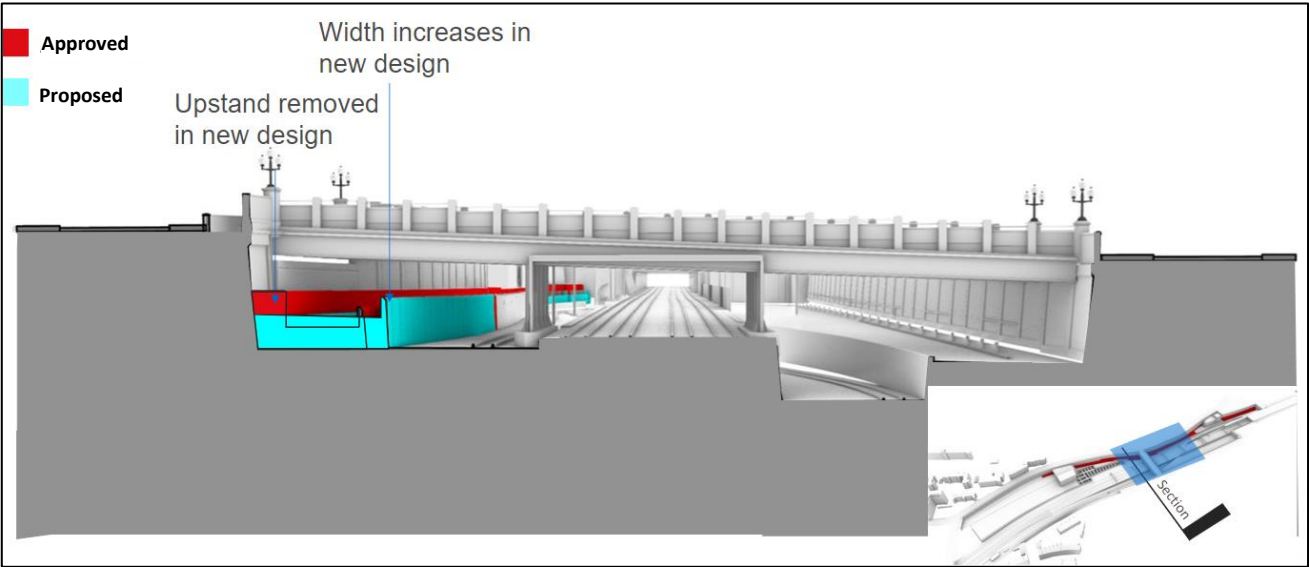


Figure 4 Comparison between approved and amendment schemes showing removal of upstand element

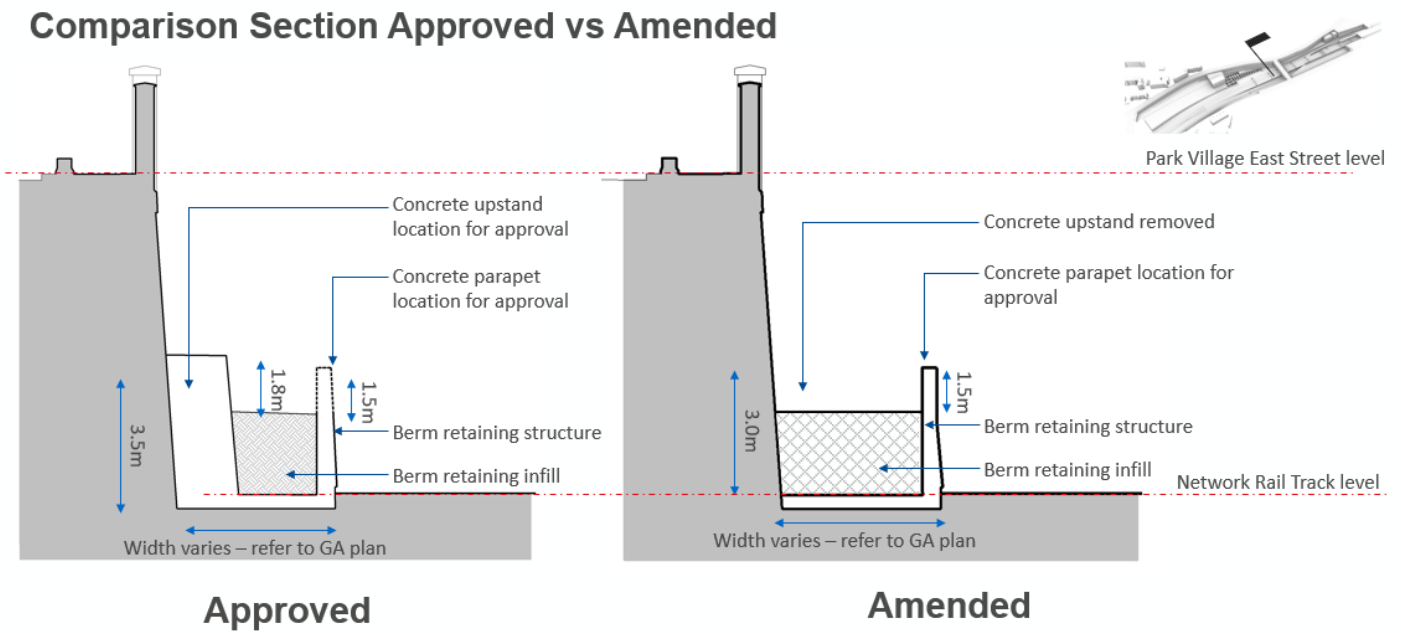


Figure 5 Comparison between approved and amendment schemes showing removal of upstand element south of Mornington Street Bridge to Euston Cavern Shaft

3.5 Other works

3.5.1 There are a range of HS2 works in the area surrounding the application boundary for this Schedule 17 application. **Figure 6** provides an overview of neighbouring HS2 works. This section will focus on those works immediately adjacent to the proposed scheme.

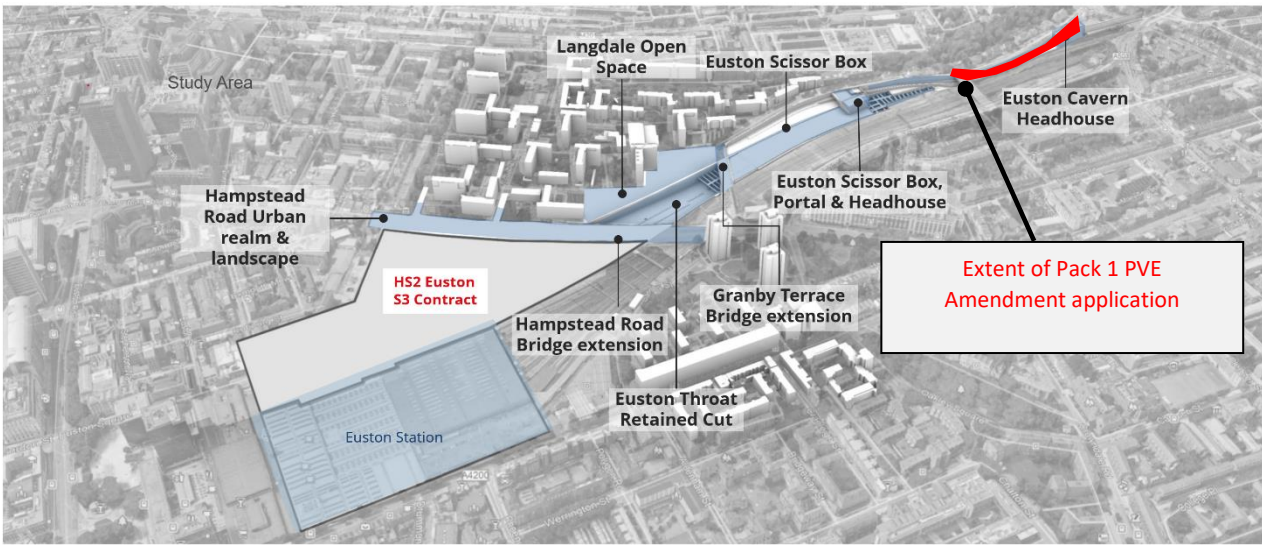


Figure 6 Indicative visualisation of neighbouring HS2 works to the site

- 3.5.2 Euston Cavern Headhouse adjoins the north part of the proposed scheme – it comprises an escape and ventilation shaft (ascending from HS2 tunnels to Park Village East Street level) enclosed in a Headhouse building. The Schedule 17 application for this scheme has been submitted for determination by the London Borough of Camden.
- 3.5.3 The Euston Scissor Box sits to the south of the proposed scheme, and comprises an area of land which will be excavated for the HS2 Euston Tunnels. The section adjacent to the proposed scheme is not enclosed by a roof, so to allow warm air to escape from the HS2 railway below. It is supported by retaining walls and props. The Euston Scissor Box was approved as part of the approved scheme for the Park Village East Berm Support Structure (LPA ref: 2021/0126/HS2).
- 3.5.4 The Euston Portal & Headhouse lies to the south of the proposed scheme. The design for these elements is still in development. The Headhouse is accessed from Park Village East highway and houses mechanical and electrical plant, a separate auto-transformer station that provides power to the railway, and space for maintenance vehicle parking.

3.6 Construction Method

3.6.1 This section summarises the general construction methodology and the main temporary works arrangements. The arrangements described may alter, are for information and background only and do not form part of this request for approval. The works subject to this request for approval of Plans and Specifications will be undertaken in accordance with the HS2 Code of Construction Practice and the Class Approval issued by the Secretary of State (March 2017).

Access to construction site

3.6.2 The construction works will fall within the Euston Approaches Worksite. The eastern boundary of the Worksite is demarcated by Network Rail tracks to the east.

3.6.3 The access to the PVE Berm Wall Support Structure will be predominantly via the corner of Park Village East and Granby Terrace Bridge including a ramp down from Granby Terrace. This will vary as the works progress, with access based on the construction methodology, outlined below. The Euston Approaches Worksite will benefit from four access / egress points including one located at the northern end of Park Village East, one on Granby Terrace at the junction of Stanhope Street / Park Village East, and two on Hampstead Road. These access points are set out in **Figure 7**.

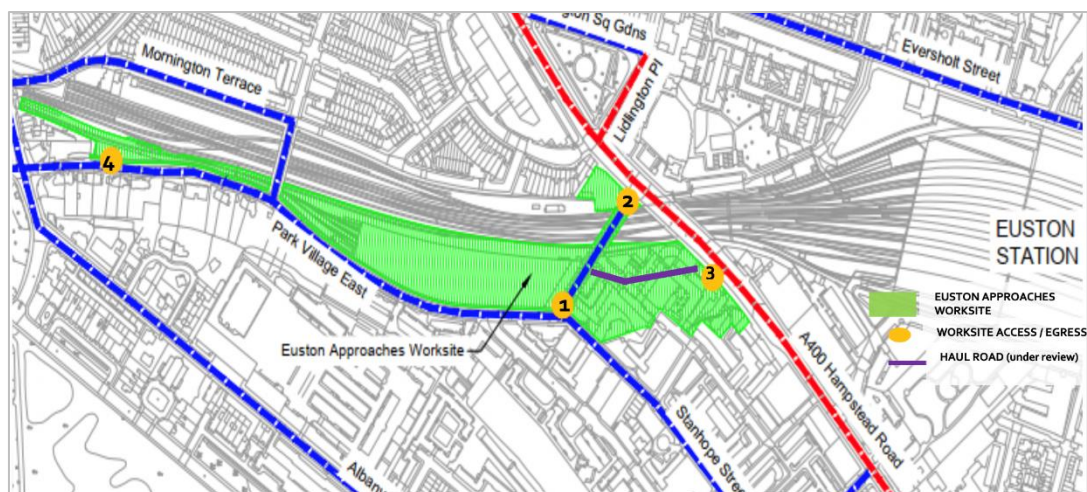


Figure 7 Access and egress points from Worksite.

Construction Method

3.6.4 This will take place as follows:

- **Berm sections**
 - Construct blinding slab (layer under base slab and L-units);
 - Temporarily fix pre-cast wall units to blinding slab with steel angles and bolts;
 - Foundation grouting;
 - Install hanger bars into the PVE wall foundation;
 - Install ground beam along PVE wall;
 - Install base slab (all zones) *At this stage, L-units become permanent (Oct. 2023); and
 - Install mass fill concrete berm & drainage (all zones)
- **Piled slab berm section** (around Euston Cavern Shaft Headhouse)
 - Install working platform piling mat;
 - Construct guide walls;
 - Drill and install piles;
 - Excavate around piles and break down piles; and
 - Construct pile caps and concrete base slab.

3.7 Historic Environment

Background

- 3.7.1 The HS2 Heritage Memorandum (part of the HS2 Environmental Minimum Requirements) explains that a route-wide generic written scheme of investigation: Historic Environment Research and Delivery Strategy (GWSI: HERDS) has been prepared in consultation with Historic England (HE) and all local planning authorities along the route. It sets out the research framework and general principles for design, evaluation, investigation, recording, analysis, reporting and archive deposition to be adopted for the design development and construction.
- 3.7.2 The arrangements for the management of archaeology during construction are not a matter for approval under Schedule 17. However, the preservation of a site of archaeological or historic interest is a key ground for refusal for the determination of plans and specification for the purposes of paragraph 3 of Schedule 17. Under the Schedule 17 Statutory Guidance (February 2017) grounds

for refusal also include the preservation of the setting of designated heritage assets.

3.7.3 Archaeological potential within the footprint of the now demolished cargo shed area and Euston Throat was examined through a desk-based assessment and an assessment of evidence noted during the recording and demolition of the structures. This concluded that any further archaeological investigation and monitoring was not required. It was determined that the expansion and excavation of the railway cutting and subsequently railway engineering works would have removed any previous Georgian structures and associated garden soils which may have contained any in situ archaeological features. The demolished cargo shed itself has been subject of a historic building record (non-designated heritage asset)³. This has included detailed recording of Park Village East parapet wall and planter. Consultation with GLAAS and Historic England on these aspects was undertaken on 19 Jan 2017 and Feb 2017.

3.7.4 The HS2 Heritage Memorandum also sets out how the historic environment (including heritage assets and their setting) will be addressed during design. The HS2 Environmental Memorandum sets out the approach to landscape and visual mitigation which takes account of the historic environment.

Heritage assets adjoining and neighbouring the site

3.7.5 The works are to run adjacent to the existing non-designated Park Village East retaining wall and will have a direct physical impact on this asset and the parapet wall above. The principle of these impacts were accepted through approval of the original PVE Berm Wall scheme (LPA ref: 2021/0126/HS2).

3.7.6 The Environmental Statement (as amended) does not identify any impacts of the works on the setting for any of the assets detailed in section 2. The works will not have a physical impact on the Grade II Parkway Tunnel and Cutting. The information set out in **Figure 2** (sourced from the Camden Local List) identifies that the extent of the listing located north of the PVE Berm Support Structure. The extent of this listing was confirmed with Historic England during a pre-application meeting on 9th March 2020 and formal recording of the listed asset will be undertaken by the EWC.

3.7.7 The current proposal includes the insertion of weep holes through the existing locally listed wall which was not included in the original approved scheme. The

³ Project Plan for Historic Building Recording of DB Schenker Shed, Euston Document No. 1D037-EDV-EV-REP-020-000002 and Historic Building Record of DB Cargo Shed: Interim Report. Document No. 1EW02-CSJ-EV-REP-S001-000011

significance of the wall is based on the fact that it retains its original rail character and is important in understanding the development of one of the first inter-city railways in the works as conceived by Robert Stephenson. The introduction of the weep holes will have a negligible impact on the significance of this asset, as they will not impact the ability for the original railway character to be appreciated or for the ability to appreciate the wall as part of understanding the development of the railway.

- 3.7.8 The construction of the amended berm wall, in terms of its attachment to the existing wall, is proposed to be the same as the approved scheme and therefore no new impact will result in the construction of the new wall.
- 3.7.9 Formal recording of the listed asset will be undertaken to mitigate the intervention of the weepholes and new berm wall to ensure understanding and record of the structure prior to the works.
- 3.7.10 In addition to above, a level 2 historic building recording of the non-designated Railway Cutting Euston to Parkway was carried out by the EWC4 and the Grade II listed Parkway Tunnel and cutting was recorded in accordance with a Heritage Agreement Method Statement that was approved by Camden in December 2020 (LBC ref: 2020/4629/HS2)⁵.

3.8 Environmental Management during Construction

- 3.8.1 The Environmental Memorandum, which forms part of the High-Speed Rail (London -West Midlands) Environmental Minimum Requirements, sets out the arrangements for the management of environmental issues during construction and the Code of Construction Practice (CoCP) sets out specific details and working practices that apply. The CoCP is supported by Local Environmental Management Plans (LEMPs) which include specific measures by topic, relevant to each relevant local authority area⁶.
- 3.8.2 Environmental management arrangements during construction do not form part of this request for approval of Plans and Specifications under Schedule 17.

⁴ Historic Building Recording of Railway Cutting Euston to Parkway - 1EW02-CSJ-EV-REP-S003-000128

⁵ Historic Building Recording of Parkway Tunnel and Cutting - 1EW02-CSJ-EV-REP-S003-000127

⁶ The LEMP relevant to the works subject to this Schedule 17 submission is P15 Local Environmental Management Plan - London Borough of Camden and can be found here: <https://www.gov.uk/government/publications/local-environmental-management-plans-for-hs2-phase-one>

4 Design Criteria and Rationale

4.1 Introduction

- 4.1.1 This section provides an overview of the rationale for amendments to the Berm Wall.

4.2 Design Requirements

- 4.2.1 The design approach for the proposal has been developed through working with key stakeholders including the London Borough of Camden. Details of engagement are provided in **Section 5**.

Functional Design Requirements

- 4.2.2 The proposal forms the central section of the approved PVE Berm Wall (LPA application ref: 2021/0126/HS2). There are therefore no changes to the location of the Berm Wall, only minor changes to its design and external appearance.
- 4.2.3 As the same rationale to the approved scheme, the existing Park Village East (PVE) wall is understood to have undergone historical episodes of movement and has been subject to a range of previous mitigation works. The historical mitigation works are understood to have stabilised the wall and prevented further movements. This is supported by recent monitoring of the wall. However, the wall is not considered to meet modern structural standards and could be at risk of structural and/or geotechnical failure when subjected to ground movements resulting from HS2 construction. It is therefore proposed to undertake a series of mitigation works to enhance its ability to accommodate ground movements and ensure the safety of the SCS work force for the duration of the HS2 works. This remains the purpose of the Berm Wall scheme.
- 4.2.4 In terms of minor changes to design and external appearance, in comparison to the approved scheme, the proposal omits the upstand and, at some points along the Berm's length, reduces the height of the berm wall.
- 4.2.5 The omission of the upstand has arisen through detailed design development of the scheme. Along with an improved understanding of the masonry and soil properties, allowing a more refined design to be pursued, the Observational Method has been adopted by the design team. As such, rather than providing a "fit and forget" style solution, the design implemented initially is based on probable behaviour and continuously monitored during the works.

- 4.2.6 A high-quality concrete finish to the outer face of the berm support structure has been chosen as a functional and appropriate response to the project sustainability requirements and context.
- 4.2.7 The height and width of the berm itself has been chosen to meet the structural and geotechnical requirements of the PVE wall mitigation strategy. For comparison of the various changes in height and width between the approved and proposed scheme, the Elevation on drawing no. 1MC03-SCJ_SDH-AR-DEL-SS01_SL03-040032 and drawing no. 1MC03-SCJ_SDH-AR-DGA-SS01_SL03-040012 respectively can be compared within the approved and current submission packs.
- 4.2.8 Additionally, the amended scheme still incorporates a barrier between the top of the berm and the adjacent WCML to act as a vehicle restraint and contain any risk of construction vehicle impact on the operational railway.

4.3 Design Criteria

- 4.3.1 Officers previously noted that views of the lighter Berm Wall would be extremely limited in public views as such, and therefore do not consider that the lighter coloured wall would harm the setting of the Park Village East Listed properties.
- 4.3.2 Moreover, the cutting retaining wall located along Mornington Terrace blocks views from street level and therefore no resultant harm would be considered to occur to the neighbouring conservation area to the east of the cutting (see **Figure 8**).



Figure 8 Photo taken at street level from Mornington Terrace facing cutting and proposal site (not visible from street level).

4.3.3 Officers also considered that the Berm Wall would not harm the setting of the Grade II* listed buildings as it would be read in the immediate context of the railway structures and not within the setting of the listed buildings at street level above.

4.3.4 Considering these comments, we will set out our response in particular, the use of pigmentation for the 'L' wall units for the proposed Berm Wall.

4.3.5 In essence, there are two issues that need to be considered when assessing the need for pigmentation of the 'L' shaped walls of the proposed Berm Wall between the approved Cavern Headhouse scheme and Mornington Street bridge.

4.3.6 Firstly, whether the viewpoints which were used to support the previous application were robust in planning terms.

4.3.7 For clarity, these viewpoints were:

- View A1 from Mornington Terrace represented views from second storey windows on Mornington Terrace providing an orthogonal view of the proposed Berm Wall; and
- View B1 from Mornington Terrace represented views from second storey windows on Mornington Terrace at the junction with Mornington Street bridge provides a view from the south-east.

4.3.8 In the Members Briefing Note (para 3.10), it was accepted that the only views of the proposal would be from the upper levels of Mornington Terrace properties. It is generally accepted in planning terms, the potential impacts of a proposed development on private views are not considered a planning matter. Private individuals do not have a right to a view and even if a new development significantly changes a view from a private property, this is not normally a legal ground on which planning approval can be refused. Consideration of visual impacts of a proposed development is usually restricted to potential impacts on public visual amenity or in heritage context.

4.3.9 Secondly in terms of the heritage context, the Members Briefing Note (para 3.11), suggests that:

'...officers considered that to the north of Mornington Street Bridge, the concrete should be tinted a darker colour to match the existing brickwork of the listed Parkway tunnels, north of Mornington Street Bridge as it would be visible from Mornington Terrace and therefore the alterations should respond sensitively to the existing context...'

- 4.3.10 Given that the listed structures of the Parkway Tunnels are located further east than the proposed Berm Wall, (if it is at all visible from the public areas of Mornington Terrace or surrounding environs), it will only at best provide a minor backdrop to the listed Parkway Tunnels. The impact of the proposal on the setting and significance of the Parkway Tunnels can therefore only be considered in the 'less than substantial category' as there would only be the merest trace of harm if it is at all possible to view the proposed Berm Wall from public areas, which would need to be balanced against the significant public benefits of the HS2 scheme.
- 4.3.11 In terms of the orthogonal view of the important Grade II* listed buildings on Park Village East from Mornington Terrace, which are central to the character and appearance of the Conservation Area, the proposed Berm Wall, as already stated in the Members Briefing Note (para 3.10), cannot be seen from the public area in Mornington Terrace. Given the height of the existing cutting boundary wall and the view presented in the Members Briefing Note, the impact of the proposal on the amenity, setting and significance of these buildings can only, therefore, be negligible.
- 4.3.12 In addition, the design of the berm will enable construction and operational vehicles to travel off-street, and so the proposal will not result in prejudicial effects on road safety or to the free flow of traffic in the local area.
- 4.3.13 In summary as the proposed 'L' shaped units would not temper the prominence of the Listed buildings or conservation areas, they do not need to be pigmented.

5 Pre-submission Consultation

5.1 Pre-submission consultation with the Local Planning Authority, statutory consultees and other relevant stakeholders is summarised in **Table 5** below.

Table 5: Pre-submission Consultation with LPA and Statutory Consultees

Consultee Name	Consultation Date	Method of Consultation / Attended by	Summary of Consultation Outcome
London Borough of Camden	14 th June 2022	Pre-application meeting	An overview of proposed design changes in PVE amendment applications.
	9 th August 2022	Pre-application meeting	Discussion on detail of proposed design changes and rationale within PVE Pack 1.
	21 st February 2023	Pre-application meeting	Page turn review of draft submission documents and drawings.

6 Construction Programme

6.1 A high-level programme for the works subject to this submission and how they fit into the overall programme for other works in the area is contained in **Table 6** below. The programme for works on site may vary from the indicative dates shown.

Table 6: Proposed Programme and Sequence of Works

Anticipated Start on Site Date (quarter/year)	Activity	Estimated Completion of Works (quarter/year)
Second quarter 2020	Site mobilisation	Second quarter 2020
Third quarter 2022	First stage works for PVE Berm Wall Support Structure	First quarter 2023
First quarter 2023	Second stage works for PVE Berm Wall Support Structure	Third quarter 2023
Second quarter 2023	Third stage works for PVE Berm Wall Support Structure	First quarter 2024
First quarter 2024	Fourth stage works for PVE Berm Wall and Support Structure	First quarter 2024

7 Other Consents

7.1 Other main consents likely to be required for the works are summarised in **Table 7** below. Consent requirements may alter during design development and further consents not identified may be required.

Table 7: Other Consent Requirements

Consent	Works Requiring Consent	To be submitted / approved
HS2 Act, Schedule 33, Part 1	Ground anchors (installed from railway cutting, extending under Park Village East highway)	Approved by LBC on 14 April 2021
Any other relevant Schedule 17 Plans and Specifications submissions for adjacent or associated works	Plans and Specifications – Park Village East Berm Wall and Euston Scissor Cut	Approved by LBC on 21 March 2021 (LPA ref: 2021/0126/HS2)
	Plans and Specifications – Euston Cavern Shaft and Headhouse	Approved by LBC on 18 October 2022 (LPA ref: 2022/3352/HS2).
	Plans and Specifications - Euston Portal Shaft and Headhouse	To be submitted.