



49 Fitzjohn's Avenue, London NW3 6PG

Parking Plan - Reasons for Replacement

Introduction

1. Occupation of residential units in 49 Fitzjohn's Avenue is increasing and the use of the car parking will soon mean that all spaces are occupied and in active use. The location of spaces as shown on the approved parking plan will lead to difficulties with manoeuvring which could be resolved by altering the location of spaces.

Existing Situation

2. The approved parking plan is attached at Annex 1 with numbers added to each of the parking spaces for ease of reference.

3. For the deemed accessible space (Space 1) to be of practical use for a mobility-impaired driver of a vehicle parking on the site then the vehicle would always need to be driven in forward and then reversed out of the south gate. This manoeuvre would involve crossing the footway with very poor inter-visibility between driver and pedestrians and, if travelling towards the south, two lanes of moving traffic as well. Alternatively, for Space 1 to be of practical use for a mobility-impaired front passenger of the vehicle then the vehicle would always first need to be reversed in before being driven out forwards. If approaching from the north, this manoeuvre would involve crossing two lanes of moving traffic as well as the footway in reverse gear. Indeed, whether or not Space 1 is being used as a standard parking space or as an accessible space, one or other of the set of manoeuvres described above will always be required in order to use the space.

4. It can be seen therefore that the location of Space 1 requires the driver to either reverse in or reverse out onto Fitzjohn's Avenue. Both manoeuvres are undesirable and clearly contrary to the original intention of the planning condition which was to act in the interests of highway safety.

5. At present only one space (marked as Space 3) is in regular use and therefore the occupant when driving can enter by one gate and leave by the other without hindrance. However, if in future Space 1 is occupied then use of both Space 2 and Space 3 is constrained which does not readily lend itself to entering and leaving the site in forward gear. This is acknowledged by the implied swept paths of vehicles using Spaces 2 and 3 shown on the approved parking plan albeit this is undesirable and contrary to the original intention of the planning condition which was to act in the interests of highway safety.

6. In practice there is potential for the driver of a vehicle parked in Space 2 (assuming it has entered from the north gate in a forward direction) to reverse towards the bin store and thereby turn the vehicle so that it can leave in a forward direction. However, such a manoeuvre is more constrained for a vehicle parked in Space 3. The driver of a vehicle parked in Space 3 is therefore even less likely to attempt to do so and will be more inclined to reverse out onto Fitzjohn's Avenue.



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7. Lastly, it may be noted an EV charging point has been installed alongside the gate operating system close to the north gate however none of the currently allocated spaces are especially convenient for its use.

Proposed Solution

8. The proposed layout of spaces shown on the Parking Plan (included at Annex 2) seeks primarily to remove the obstruction created by Space 1 being located immediately inside the south gate. It can be seen that, in practice, there is adequate width beyond this point for a vehicle to be parked alongside the south wall without obstructing access to the rear of the property. It is proposed that this location should accommodate one vehicle (Space A).

9. Space 2 would become Space B and remain in largely its existing location however the clear area created by removing Space 1 affords the opportunity for the user of this space to enter the south gate in a forward direction and then reverse into Space B. The vehicle is thereby in a position to be driven forwards when leaving, via the south gate.

10. Space 3 would be moved to a more central position (Space C) thereby creating a clear manoeuvring area for the user of Space A to turn around within the site, either before parking or prior to leaving. Moreover, Space C would no longer be restricted by the presence of a vehicle inside the south gate and therefore could enter and leave in a forward direction by using one gate to enter and the other to leave. It is also advantageous for this space to be the deemed accessible space since the route in can be varied to suit if it is either the driver or the front passenger that is mobility-impaired. No reversing will be required in either case.

11. The disposition of these spaces means that an additional space can be provided which is also most convenient for the EV charging station (Space D). Space D can be accessed in the same manner as Space B using the north gate with the turning manoeuvre taking place within the site.

12. It may be noted that the number of parking spaces in front of the building line of 49 Fitzjohn's Avenue will remain at three, thereby having no material impact on the appearance of the premises or character of the immediate area compared to the effect of the three spaces shown on the approved parking plan.

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Summary

13. The existing layout, when fully used, will necessitate vehicles using Space 1 to either be reversed onto or off Fitzjohn's Avenue. This situation cannot be avoided. It is also likely that users of Spaces 2 and 3 will also include reversing onto Fitzjohn's Avenue as part of their parking manoeuvre. This is undesirable and clearly contrary to the original intention of the planning condition which was to act in the interests of highway safety. Moreover, the absolute constraint applies to the parking space which is deemed to be accessible. It is not clear why such a layout was approved when the operational issues which result are clearly detrimental to highway safety including to pedestrians passing by the site.

14. The proposed layout rationalises the car parking layout to remove the unnecessary existing constraints on the ability of vehicle drivers to both enter and leave the site in a forward direction. Moreover the proposed layout enables the provision of a space which can be deemed accessible and importantly can be approached from either the north or south gate so maximising the functionality of the space for either a mobility-impaired driver or a mobility-impaired front passenger. The benefits which will accrue by reducing potential disruption to traffic flow as well as the dangers for pedestrians and other road users associated with reversing into or out of the site are significant compared to the largely immaterial impact of an additional parking space adding to road traffic.

15. The creation of an additional parking space which is located alongside an EV charging station further maximises the value of amending the existing layout while the number of parking spaces in front of the building line of 49 Fitzjohn's Avenue will remain at three, thereby having no material impact on the appearance of the premises or character of the immediate area compared to the approved parking plan.

16. Lastly, it should be noted the development will continue to be car-free from the perspective that residents are not entitled to apply for a residents' parking permit under the terms of the Development Agreement.

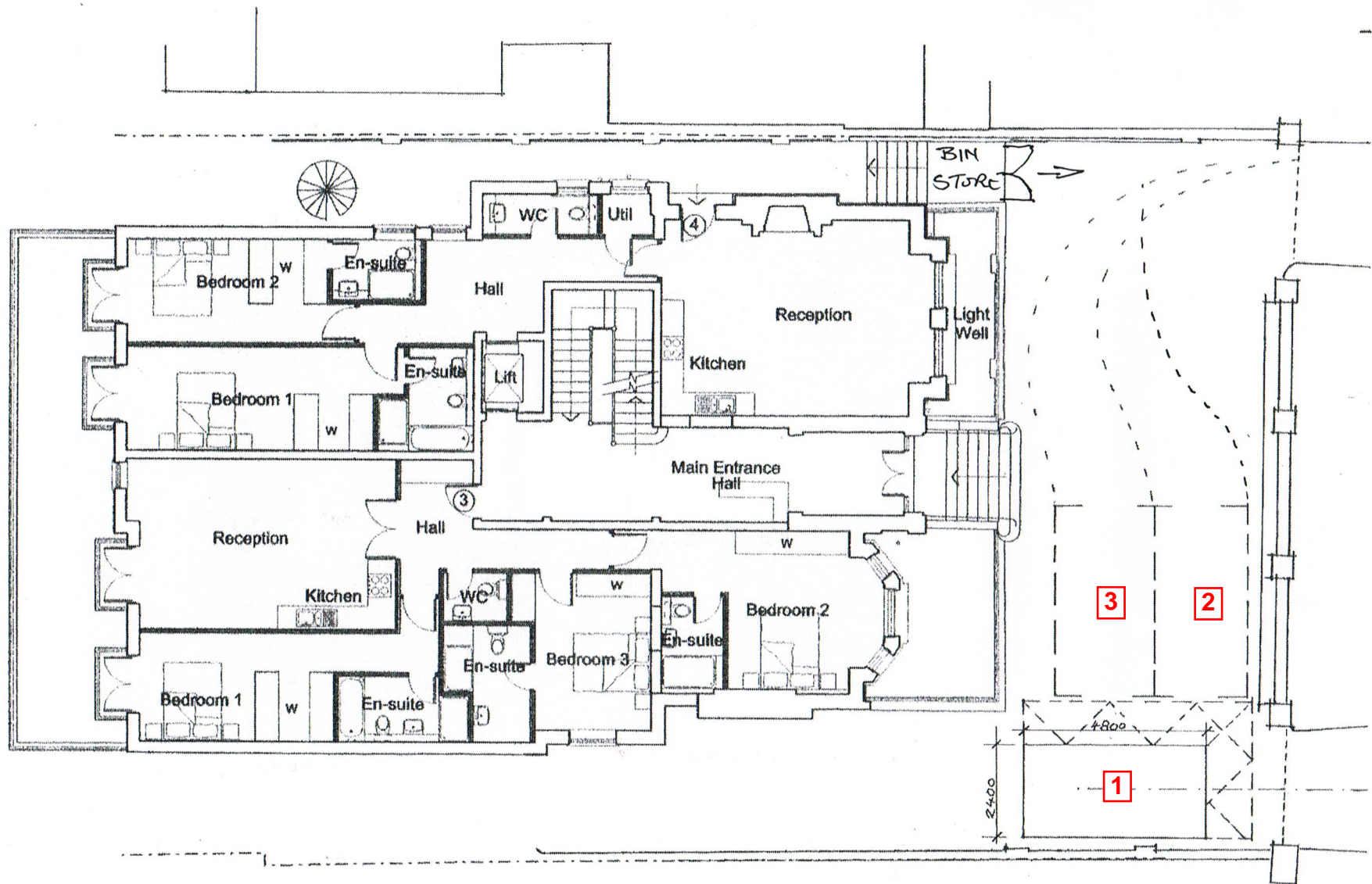
Attachments

Annex 1 – Approved Parking Plan (marked up for ease of reference)

Annex 2 – Parking Plan (Drg No 3023001/A/4)

ANNEX A

**Approved Parking
Plan – marked up for
ease of reference**



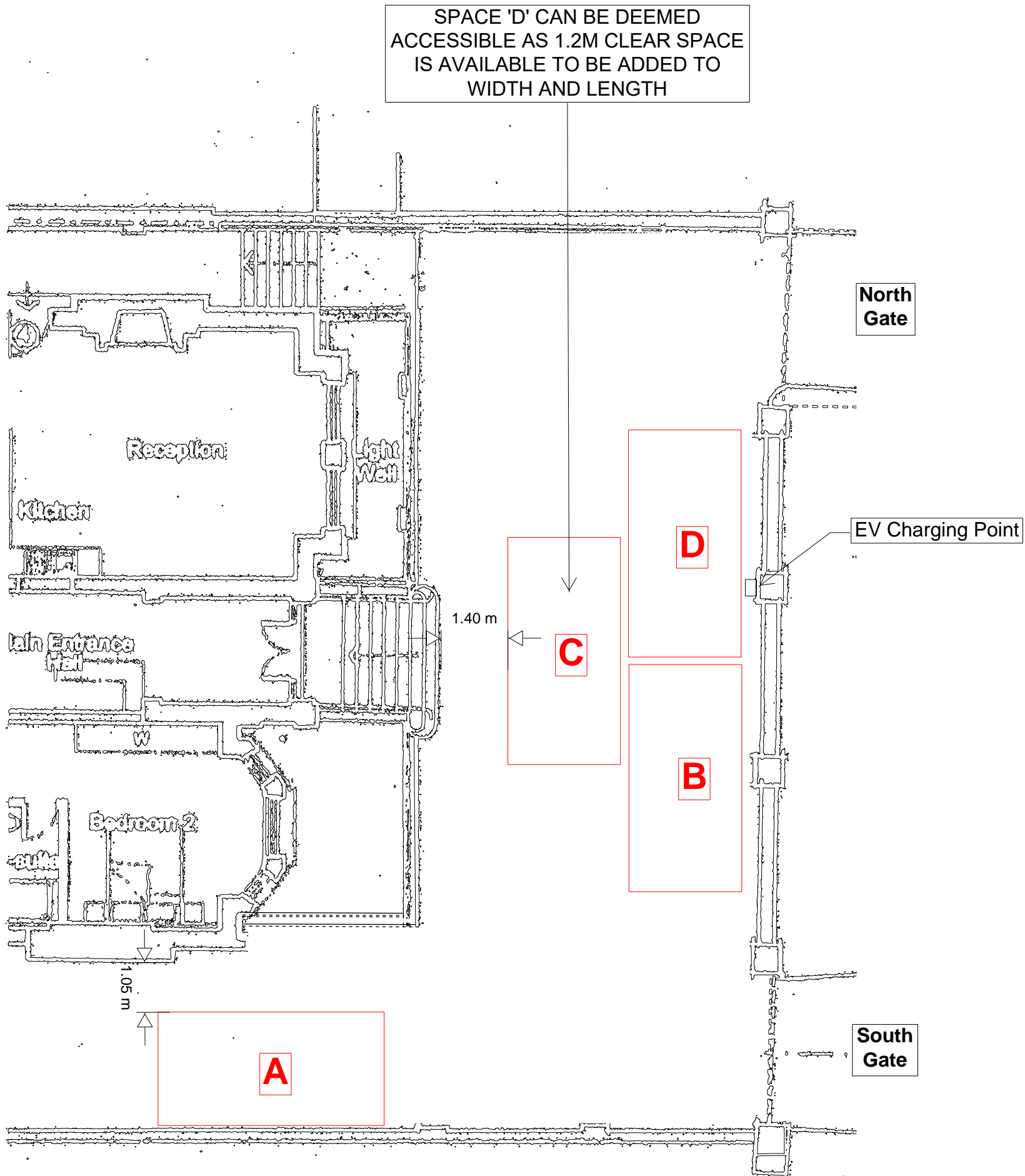
Approved Parking Plan



ANNEX B

Parking Plan -

Drg No 3023001/A/4



49 FITZJOHN'S AVENUE PARKING PLAN

Notes:
1. ALL CAR PARKING SPACES = 4.8m LONG x 2.4m WIDE.

Title: 49 FITZJOHN'S AVE, NW3 PROPOSED 4-SPACE CAR PARKING LAYOUT	Scale: 1:1000
Client: CATAL GROUP	Date: 09/03/2023
	Prepared: CLEWLOW C'LT'G
	Checked: SPC
	Dwg No: 3023001/A/4